

LOCATION MAP
Scale: 1" = 100'
WATERSHED: LOWER SCHUYLKILL RIVER
FLOOD MANAGEMENT DISTRICT: A
FEMA FLOODPLAIN: ZONE X

REVISIONS		
ISSUE	DATE	DESCRIPTION
1	01/09/23	PWD PCSM/NPDES SUBMISSION
2	02/14/23	PWD PCSM RESUBMISSION
3	05/24/23	PWD PCSM RESUBMISSION



REVIEWED BY:
PROJECT COORDINATOR:
REALS:



KELLY MAIELLO ARCHITECTS
1420 Walnut Street, 15th Floor
Philadelphia, PA 19102
www.kmarchitects.com

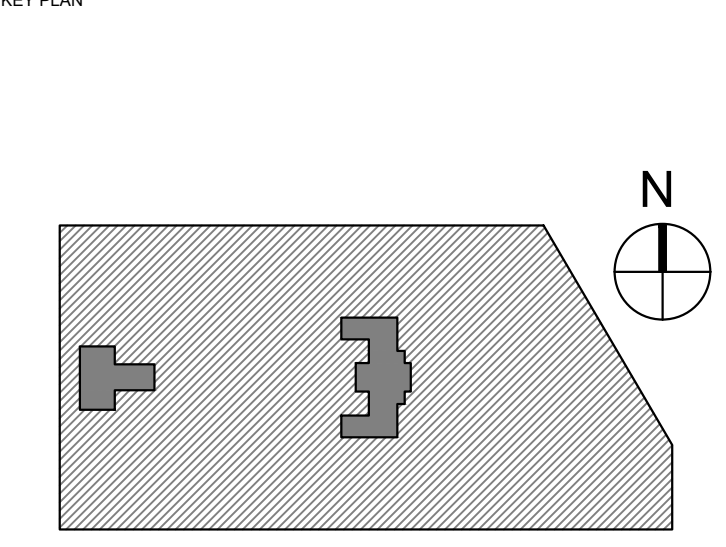
LANDSCAPE ARCHITECT:
Salt Design Studio
4100 Main Street Suite 200
Philadelphia PA 19127
www.saltdesignstudio.com

M.E.P./F.P./SITE CIVIL ENGINEERS:
Pennoni Associates
1900 Market Street Suite 300
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www.pennoni.com

LEED CONSULTANT:
Verde Architecture Consulting
1635 Market Street Suite 1600
Philadelphia PA 19103

CITY OF PHILADELPHIA
DEPARTMENT OF PUBLIC PROPERTY
1515 ARCH STREET
11TH FLOOR, ONE PARKWAY BUILDING
PHILADELPHIA, PENNSYLVANIA

PROJECT TITLE:
KINGSESSING RECREATION CENTER



DRAWING TITLE:
EXISTING CONDITIONS PLAN

PROJECT NO. KMLX21003	DRAWING NO.
DATE 08/28/23	C-021-R.2
SCALE 1" = 30'	
DRAWN BY SDL	
CHECKED BY MJM	

GENERAL INFORMATION:
SITE:
4801 KINGSESSING AVE
PHILADELPHIA, PA 19143
OPA#183249500

CLIENT:
CITY OF PHILADELPHIA
PARKS AND RECREATION
1515 ARCH STREET, 10TH FLOOR
PHILADELPHIA, PA 19102

ENGINEER:
PENNONI ASSOCIATES
1900 MARKET STREET, SUITE 300
PHILADELPHIA, PA 19103

SITE INFORMATION:
MUNICIPAL ZONING INFORMATION:
THE SITE IS LOCATED IN THE FOLLOWING ZONES IN THE CITY OF PHILADELPHIA:
ACTIVE PARKS AND OPEN SPACE (SP-POA)
ACCESSORY SIGN CONTROLS - SPECIAL CONTROLS FOR COBBS CREEK, ROOSEVELT BOULEVARD, AND DEPARTMENT OF PARKS AND RECREATION LAND

FOR COMPLETE ZONING INFORMATION PLEASE REFER TO THE ZONING CODE OF THE CITY OF PHILADELPHIA AS CURRENTLY AMENDED

FLOOD ZONE INFORMATION:
BY GRAPHIC PLOTTING ONLY. BASED UPON THE FLOOD INSURANCE RATE COMMUNITY MAP NO. 420750184H WHICH BEARS AN EFFECTIVE DATE OF NOVEMBER 18, 2015, THE SITE IS LOCATED IN THE FOLLOWING AREAS:
ZONE X AREA OUTSIDE THE 0.2% ANNUAL CHANCE FLOODPLAIN

REFERENCES:
1. TOPOGRAPHIC INFORMATION TAKEN FROM A PLAN BY AMERICAN ENGINEERS GROUP, LLC ENTITLED "KINGSESSING RECREATION CENTER TOPOGRAPHIC SURVEY", SHEET NO. 1, DATED 09/12/2021.
2. BOUNDARY INFORMATION FROM GIS. BOUNDARY SURVEY NOT PERFORMED

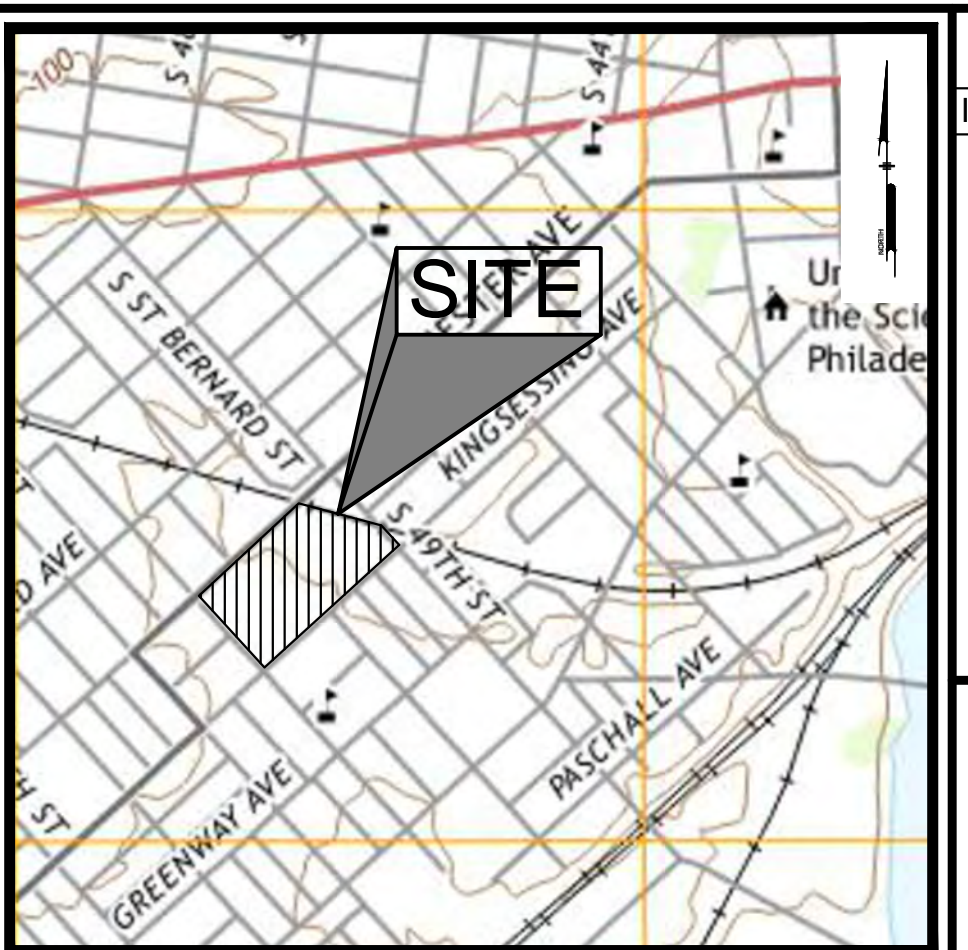
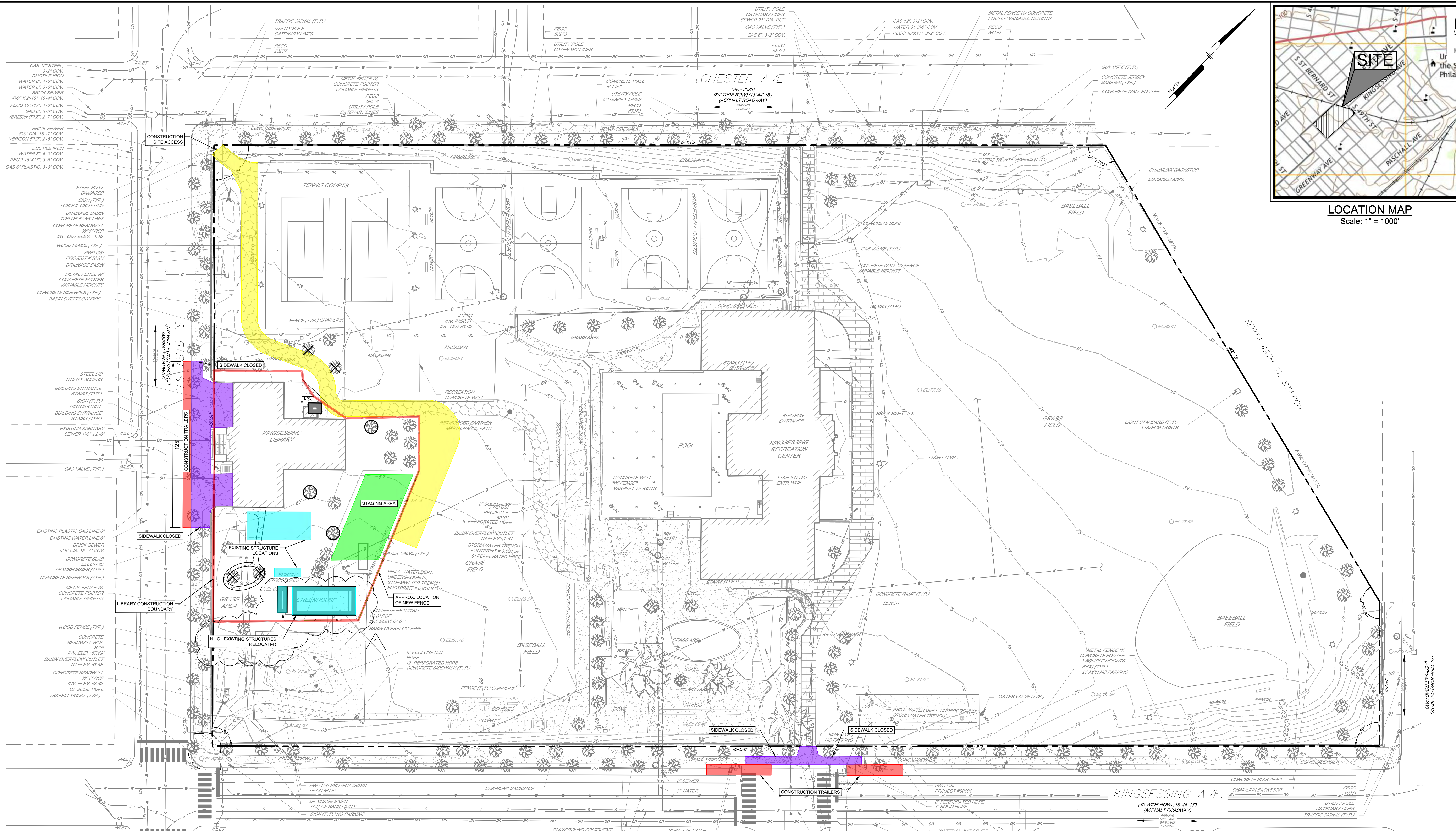
SITE NOTES:
1. PWD TRACKING #FY22-KING-6800-01
2. UNLESS SPECIFICALLY STATED OR SHOWN HEREON TO THE CONTRARY, THIS SURVEY IS MADE SUBJECT TO AND DOES NOT LOCATE OR DELINEATE:
2.A. RIGHTS OR INTERESTS OF THE UNITED STATES OF AMERICA OR COMMONWEALTH OF PENNSYLVANIA OVER LANDS NOW OR FORMERLY FLOWED BY TIDE WATER BUT NO LONGER VISIBLE OR PHYSICALLY EVIDENT, OR LANDS CONTAINING ANY ANIMAL, MARINE, OR BOTANICAL SPECIES REGULATED BY OR UNDER THE JURISDICTION OF ANY FEDERAL, STATE, OR LOCAL AGENCY.
2.B. BUILDING SETBACK LINES, ZONING REGULATIONS OR LINES ESTABLISHED BY ANY FEDERAL, STATE OR LOCAL AGENCY WHICH MAY AFFECT THE BUILDING OR DEVELOPMENT POTENTIAL OF THE SUBJECT PROPERTY.
2.C. ANY SUBSURFACE OR SUBTERRANEAN CONDITION, EASEMENTS OR RIGHTS INCLUDING BUT NOT LIMITED TO MINERAL OR MINING RIGHTS, OR THE LOCATION OF OR RIGHTS TO ANY SUBSURFACE STRUCTURES, CONTAINERS OR FACILITIES OR ANY OTHER NATURAL, OR MANMADE SUBSURFACE CONDITION WHICH MAY OR MAY NOT AFFECT THE USE OR DEVELOPMENT POTENTIAL OF THE SUBJECT PROPERTY.
3. LOCATIONS OF ON AND OFF SITE UTILITIES AS SHOWN ARE APPROXIMATE AND MAY OR MAY NOT BE COMPLETE. THE NATURE AND EXACT LOCATION OF EXISTING UTILITIES SHOULD BE VERIFIED PRIOR TO INITIATING ANY ACTIVITY THAT MAY AFFECT THEIR USE OR LOCATION.
3.A. THE LOCATION OF THE EXISTING UNDERGROUND UTILITIES SHOWN ON THIS PLAN HAVE BEEN TAKEN FROM EXISTING UTILITY RECORDS AVAILABLE AT THE TIME THESE PLANS WERE PREPARED AND FROM SURFACE OBSERVATION OF THE SITE.
3.B. COMPLETENESS OR ACCURACY OF LOCATION AND DEPTH OF UNDERGROUND UTILITIES AND STRUCTURES IS NOT GUARANTEED.
3.C. IN ACCORDANCE WITH PA ACT 267 OF 1974 AS AMENDED BY PA ACT 121 OF 2008 ENTITLED "UNDERGROUND UTILITY LINE PROTECTION LAW", THE CONTRACTOR SHALL NOTIFY ALL UTILITIES WITHIN THE WORK AREA VIA THE PENNSYLVANIA ONE CALL SYSTEM INC. (800-242-1778) A MINIMUM OF 3 WORKING DAYS BEFORE THE START OF EXCAVATION.
3.D. THE CONTRACTOR SHALL VERIFY LOCATIONS AND DEPTHS OF ALL UNDERGROUND UTILITIES AND STRUCTURES BEFORE THE START OF WORK.
4. ALL BOUNDARY DIMENSIONS SHOWN ARE IN CITY OF PHILADELPHIA DISTRICT STANDARD MEASURE (DS) UNLESS DESIGNATED (US) TO DENOTE U.S. SURVEY FOOT.
5. BOUNDARY DIMENSIONS SHOWN ARE IN CITY OF PHILADELPHIA DISTRICT STANDARD MEASURE (DS) UNLESS DESIGNATED (US) TO DENOTE U.S. SURVEY FOOT.

SURVEY NOTES FROM AMERICAN ENGINEERS:
1. TOPOGRAPHIC INFORMATION HEREON IS TAKEN FROM A PLAN PREPARED BY AMERICAN ENGINEERS GROUP, LLC ENTITLED "KINGSESSING RECREATION CENTER TOPOGRAPHIC SURVEY" FOR CITY OF PHILADELPHIA REBUILD SITE 4801 KINGSESSING AVE. DRAWING NUMBER 1 DATED 11/22/21.
2. PENNSYLVANIA STATE PLANE: SOUTH ZONE
HORIZONTAL DATUM: NORTH AMERICAN DATUM OF 1983 (NAD 83)
VERTICAL DATUM: CITY OF PHILADELPHIA DATUM
3. KINGSESSING RECREATION CENTER SITE CITY BENCHMARK CITY SURVEY BUREAU SET PK NAIL ELEVATIONS:
EAST CORNER CHESTER AVE. AND S. 48TH ST. - 87.57'
NORTH CORNER CHESTER AVE. AND S. 48TH ST. - 87.90'

EXISTING LEGEND

---	EXISTING PROPERTY LINE (APPROXIMATE)
---	EXISTING RIGHT OF WAY LINE (APPROXIMATE)
---	EXISTING BUILDING
---	EXISTING CURB
---	EXISTING SIDEWALK
---	EXISTING EDGE OF MACADAM/GRAVEL
---	EXISTING TRAFFIC MARKING
---	EXISTING FENCE
---	EXISTING TREE
---	EXISTING MAJOR CONTOUR
---	EXISTING MINOR CONTOUR
---	EXISTING SIGN
---	EXISTING BOLLARD
---	EXISTING UTILITY POLE
---	EXISTING LIGHT
---	EXISTING INLET
---	EXISTING SEWER
---	EXISTING STORM SEWER
---	EXISTING WATER LINE
---	EXISTING UNDERGROUND TELEPHONE LINE
---	EXISTING GAS LINE
---	EXISTING UNDERGROUND ELECTRIC
---	EXISTING OVERHEAD WIRES
---	EXISTING UTILITY STRUCTURES

PACKAGE 2 - IFB
NOT FOR CONSTRUCTION
08/28/23
NPDES PERMIT #PAC510302
PWD TRACKING #FY22-KING-6800-01



LOCATION MAP
Scale: 1" = 1000'

REVISIONS		
ISSUE	DATE	DESCRIPTION
1	01/09/23	PWD PCSMNPDES SUBMISSION
2	02/14/23	PWD PCSM SUBMISSION
3	05/24/23	PWD PCSM RESUBMISSION



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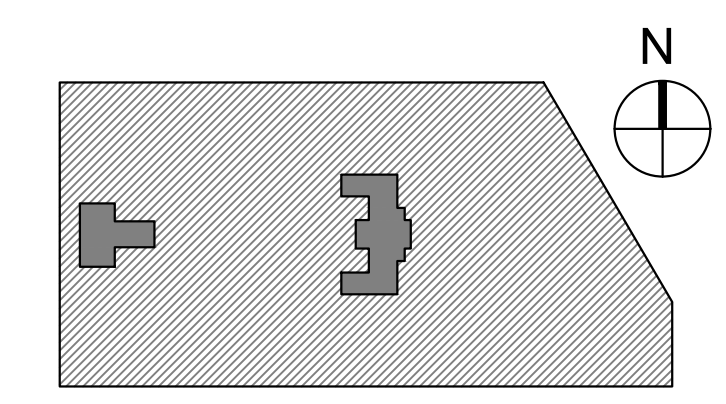
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CITY OF PHILADELPHIA
DEPARTMENT OF PUBLIC PROPERTY
1515 ARCH STREET
11TH FLOOR, ONE PARKWAY BUILDING
PHILADELPHIA PENNSYLVANIA

PROJECT TITLE
KINGSESSING RECREATION CENTER

KEY PLAN



DRAWING TITLE
Site Utilization Plan

PROJECT NO. KLMXLX21003	DRAWING NO.
DATE 08/28/23	C-050-R.2
SCALE 1"=30'	
DRAWN BY SDL	
CHECKED BY MJM	

GENERAL INFORMATION:

SITE:

4901 KINGSESSING AVE
PHILADELPHIA, PA 19145
OP#8783249500

CLIENT:

CITY OF PHILADELPHIA
PARKS AND RECREATION
1515 ARCH STREET, 10TH FLOOR
PHILADELPHIA, PA 19102

ENGINEER:

PENNONI ASSOCIATES
1900 MARKET STREET, SUITE 300
PHILADELPHIA, PA 19103

SITE INFORMATION:

MUNICIPAL ZONING INFORMATION:

THE SITE IS LOCATED IN THE FOLLOWING ZONES IN THE CITY OF PHILADELPHIA:

- ACTIVE PARKS AND OPEN SPACE (SP-POA)
- ACCESSORY SIGN CONTROLS - SPECIAL CONTROLS FOR COBBES CREEK, ROOSEVELT BOULEVARD, AND DEPARTMENT OF PARKS AND RECREATION LAND

FOR COMPLETE ZONING INFORMATION PLEASE REFER TO THE ZONING CODE OF THE CITY OF PHILADELPHIA AS CURRENTLY AMENDED

FLOOD ZONE INFORMATION:

BY GRAPHIC PLOTTING ONLY, BASED UPON THE FLOOD INSURANCE RATE COMMUNITY MAP NO. 4207571944 WHICH BEARS AN EFFECTIVE DATE OF NOVEMBER 18, 2015, THE SITE IS LOCATED IN THE FOLLOWING AREAS:

- ZONE X AREA OUTSIDE THE 0.2% ANNUAL CHANCE FLOODPLAIN

NO FIELD SURVEYING WAS PERFORMED TO DETERMINE THIS ZONE

REFERENCES:

- TOPOGRAPHIC INFORMATION TAKEN FROM A PLAN BY AMERICAN ENGINEERS GROUP, LLC ENTITLED "KINGSESSING RECREATION CENTER TOPOGRAPHIC SURVEY", SHEET NO. 1, DATED 09/12/2021
- BOUNDARY INFORMATION FROM GIS, BOUNDARY SURVEY NOT PERFORMED

SITE NOTES:

- UNLESS SPECIFICALLY STATED OR SHOWN HEREON TO THE CONTRARY, THIS SURVEY IS MADE SUBJECT TO AND DOES NOT LOCATE OR DELINEATE.
- RIGHTS OR INTERESTS OF THE UNITED STATES OF AMERICA OR COMMONWEALTH OF PENNSYLVANIA OVER LANDS NOW OR FORMERLY FLOWED BY TIDEWATER BUT NO LONGER VISIBLE OR PHYSICALLY EVIDENT, OR LANDS CONTAINING ANY ANIMAL, MARINE, OR BOTANICAL SPECIES REGULATED BY OR UNDER THE JURISDICTION OF ANY FEDERAL, STATE, OR LOCAL AGENCY.
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- COMPLETENESS OR ACCURACY OF LOCATION AND DEPTH OF UNDERGROUND UTILITIES AND STRUCTURES IS NOT GUARANTEED.
- IN ACCORDANCE WITH PA ACT 287 OF 1974 AS AMENDED BY PA ACT 121 OF 2008 ENTITLED "UNDERGROUND UTILITY LINE PROTECTION LAW", THE CONTRACTOR SHALL NOTIFY ALL UTILITIES WITHIN THE WORK AREA VIA THE PENNSYLVANIA ONE CALL SYSTEM INC. (800-242-1778) A MINIMUM OF 3 WORKING DAYS BEFORE THE START OF EXCAVATION.
- THE CONTRACTOR SHALL VERIFY LOCATIONS AND DEPTHS OF ALL UNDERGROUND UTILITIES AND STRUCTURES BEFORE THE START OF WORK.
- ALL BOUNDARY DIMENSIONS SHOWN ARE IN CITY OF PHILADELPHIA DISTRICT STANDARD MEASURE (DS) UNLESS DESIGNATED (US) TO DENOTE U.S. SURVEY FOOT.
- THE HORIZONTAL DATUM FOR THIS PROJECT REFERENCES THE PENNSYLVANIA STATE PLANE COORDINATE SYSTEM, SOUTH ZONE, NORTH AMERICAN DATUM OF 1983
- THE VERTICAL DATUM FOR THIS PROJECT REFERENCES THE CITY OF PHILADELPHIA VERTICAL DATUM.
- CONTRACTOR TO COORDINATE CONSTRUCTION TRANSITION WITH LIBRARY CONTRACTOR

SURVEY NOTES:

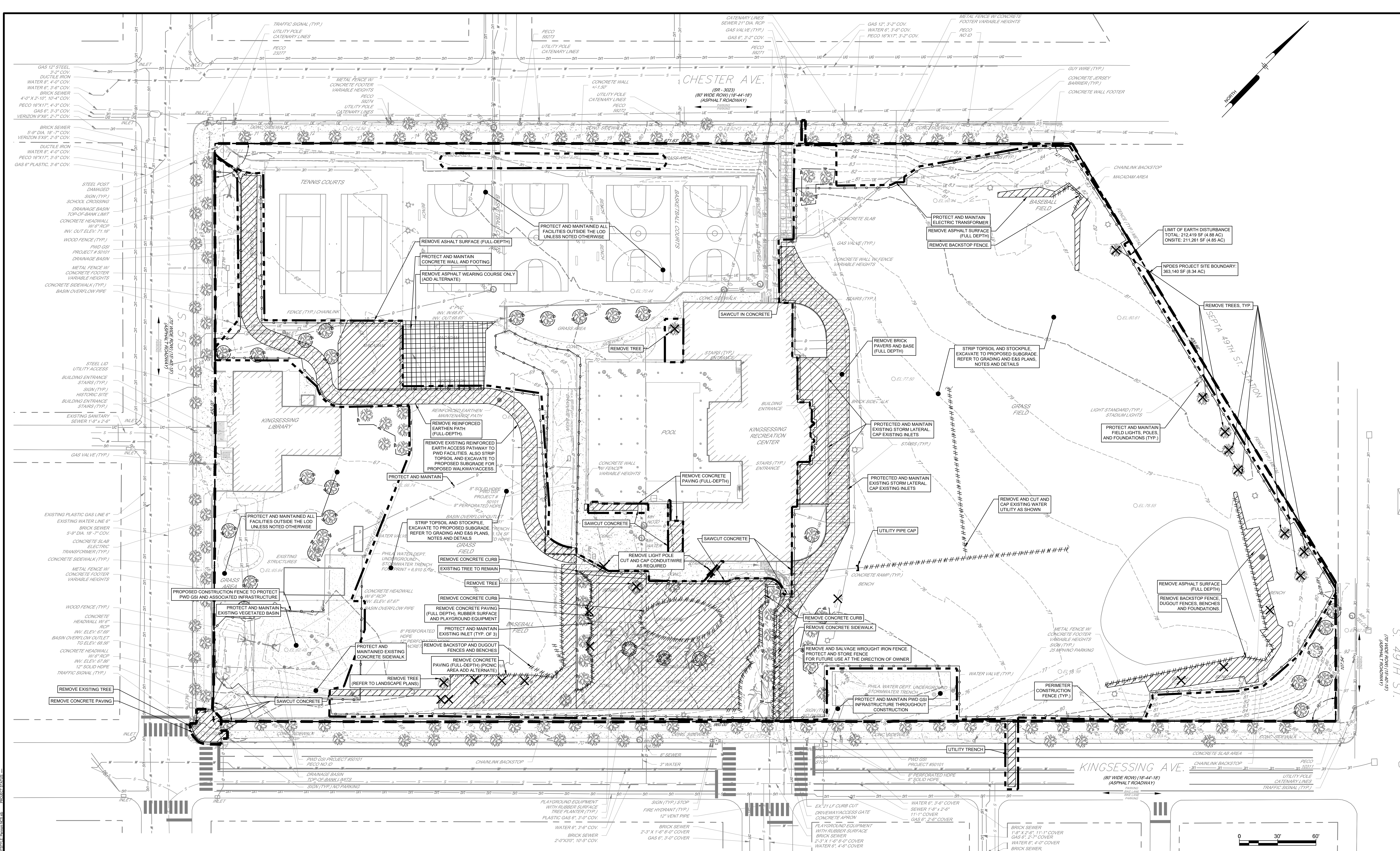
- PENNSYLVANIA STATE PLANE, SOUTH ZONE
HORIZONTAL DATUM: NAD 83
VERTICAL DATUM: CITY OF PHILADELPHIA DATUM
- KINGSESSING RECREATION CENTER SITE CITY BENCHMARK
CITY SURVEY BUREAU SET PK NAIL ELEVATIONS:
EAST CORNER CHESTER AVE. AND S. 48TH ST. - 87.57'
NORTH CORNER CHESTER AVE. AND S. 48TH ST. - 87.90'

EXISTING LEGEND

---	EXISTING PROPERTY LINE (APPROXIMATE)	---	CONSTRUCTION ACCESS
---	EXISTING RIGHT OF WAY LINE (APPROXIMATE)	---	STAGING AREA
---	EXISTING BUILDING	---	CONSTRUCTION TRAILERS
---	EXISTING CURB	---	SIDEWALK CLOSED
---	EXISTING SIDEWALK	---	STRUCTURE RELOCATION
---	EXISTING EDGE OF MACADAM/GRAVEL		
---	EXISTING TRAFFIC MARKING		
---	EXISTING FENCE		
---	EXISTING TREE		
---	EXISTING MAJOR CONTOUR		
---	EXISTING MINOR CONTOUR		
---	EXISTING SIGN		
---	EXISTING BOLLARD		
---	EXISTING UTILITY POLE		
---	EXISTING LIGHT		
---	EXISTING INLET		
---	EXISTING SEWER		
---	EXISTING STORM SEWER		
---	EXISTING WATER LINE		
---	EXISTING UNDERGROUND TELEPHONE LINE		
---	EXISTING GAS LINE		
---	EXISTING UNDERGROUND ELECTRIC		
---	EXISTING OVERHEAD WIRES		
---	EXISTING UTILITY STRUCTURES		

PACKAGE 2 - IFB
NOT FOR CONSTRUCTION
08/28/23
NPDES PERMIT #PAC510302
PWD TRACKING #FY22-KING-6800-01

NOTE: ALL DIMENSIONS AND CONDITIONS SHALL BE VERIFIED BY THE CONTRACTOR AT THE SITE BEFORE PROCEEDING WITH THE WORK.



GENERAL INFORMATION:

SITE:
401 KINGSESSING AVE
PHILADELPHIA, PA 19143
OP#W78324950

CLIENT:
CITY OF PHILADELPHIA
PARKS AND RECREATION
1515 ARCH STREET, 10TH FLOOR
PHILADELPHIA, PA 19102

ENGINEER:
PENNONI ASSOCIATES
1900 MARKET STREET, SUITE 300
PHILADELPHIA, PA 19103

REFERENCES:

1. TOPOGRAPHIC INFORMATION TAKEN FROM A PLAN BY AMERICAN ENGINEERS GROUP, LLC ENTITLED "KINGSESSING RECREATION CENTER TOPOGRAPHIC SURVEY", SHEET NO. 1, DATED 05/20/2011.
2. BOUNDARY INFORMATION FROM GIS. BOUNDARY SURVEY NOT PERFORMED.

DEMOLITION NOTES:

1. PURSUANT TO THE REQUIREMENTS OF PENNSYLVANIA ACT 287 OF 1974, AS AMENDED BY ACT 121 OF 2008, THE CONTRACTOR SHALL CONTACT THE PENNSYLVANIA ONE CALL SYSTEM AT 1-800-242-1176, AT LEAST THREE (3) WORKING DAYS PRIOR TO EXCAVATION. CONTRACTOR IS RESPONSIBLE FOR UTILIZING APPLICABLE EROSION CONTROL MEASURES PRIOR TO AND DURING DEMOLITION.
2. ALL SUPER STRUCTURES, AND MISCELLANEOUS ITEMS SHALL BE REMOVED IN THEIR ENTIRETY FROM THE SITE, UNLESS OTHERWISE NOTED. ALL OPEN EXCAVATION AS A RESULT OF DEMOLITION WORK (I.E. BASEMENTS, TRENCHES) SHALL BE FILLED TO SURROUNDING GRADE LEVEL. AFTER DEMOLITION IS COMPLETE, SAFETY DEVICES (I.E. BARRICADES, WARNING TAPE, CHAIN LINK FENCING, ETC.) SHALL BE USED DURING DEMOLITION PROCEDURES TO INSURE THE SAFETY OF THE SURROUNDING PUBLIC.
3. THE CONTRACTOR SHALL INSTALL AN 8" H CHAIN LINK SECURITY FENCE. POSTS SHALL BE DRIVEN OR SET ON PLATES. EXACT LOCATION SHALL BE COORDINATED WITH OWNER/ENGINEER AND DESIGNATED ON CONTRACTOR'S SITE SAFETY PLAN WHICH SHALL BE APPROVED BY PROJECT ENGINEER.
4. CONTRACTOR IS RESPONSIBLE FOR OBTAINING ALL APPROPRIATE PERMITS FROM GOVERNMENTAL AGENCIES PRIOR TO START OF DEMOLITION.
5. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO DETERMINE THE LOCATION OF ALL EXISTING UNDERGROUND UTILITIES AND TO TAKE WHATEVER STEPS ARE NECESSARY TO PROVIDE FOR THEIR PROTECTION. THE ENGINEER HAS INDICATED EXISTING UTILITIES ON THESE PLANS BASED ON INFORMATION PROVIDED BY OWNER AND VISUAL SITE OBSERVATIONS. HOWEVER, THIS INFORMATION IS SHOWN FOR THE CONTRACTOR'S CONVENIENCE ONLY. THE ENGINEER ASSUMES NO RESPONSIBILITY FOR THE LOCATIONS OF UTILITIES SHOWN OR NOT SHOWN. THE CONTRACTOR SHALL CONTACT THE UTILITY COMPANIES FOR EXACT LOCATION OF UTILITIES PRIOR TO STARTING CONSTRUCTION. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO REPAIR AND REPLACE ANY AND ALL DAMAGE MADE TO UTILITIES BY THE CONTRACTOR. AT CONTRACTOR'S EXPENSE.
6. CONTRACTOR TO NOTIFY AND COORDINATE WITH APPROPRIATE UTILITY COMPANY AND THE OWNER PRIOR TO SHUT-DOWN, CUTTING/CAPPING, REMOVAL OR RELOCATION OF ANY UTILITY.
7. ALL MATERIALS TO BE REMOVED SHALL BE DISPOSED OF AT AN APPROVED AND PERMITTED WASTE DISPOSAL SITE OR SHALL BE RECYCLED IN ACCORDANCE WITH PROJECT REQUIREMENTS.
8. ALL STREETLIGHTS, SIGNS, POSTS, VAULTS, VALVES, ETC. IN THE WALKWAYS AND STREETS ARE TO BE PROTECTED AND MAINTAINED IN OPERABLE CONDITION THROUGHOUT DEMOLITION AND CONSTRUCTION.
9. THE EXTENTS OF SHEETING, SHORING AND UNDERPINNING (IF ANY) ARE UNKNOWN. THE CONTRACTOR IS RESPONSIBLE FOR RETAINING A PENNSYLVANIA LICENSED ENGINEER TO PREPARE CONSTRUCTION DRAWINGS FOR ALL EXCAVATION BRACING, UNDERPINNING, SUPPORT, SHEETING, SHORING OR OTHER MEANS NECESSARY TO ALLOW FOR DEMOLITION. EXTENT OF SHEETING AND SHORING MAY REQUIRE A CONSTRUCTION ENTRANCE RAMP. THE CONTRACTOR IS RESPONSIBLE FOR OBTAINING ALL REQUIRED PERMITS AND APPROVALS FOR INSTALLING THE SHEETING AND SHORING PLACED WITHIN THE SUBJECT PROPERTY AND OTHER AREAS OUTSIDE THE PROPERTY BOUNDARY. THE CONTRACTOR SHALL COORDINATE THE SHEETING AND SHORING WITH CITY AND STATE AGENCIES AND THE ADJACENT PROPERTY OWNERS.
10. CONTRACTOR TO PROTECT AND SUPPORT EXISTING UTILITY STRUCTURES TO REMAIN DURING DEMOLITION AND CONSTRUCTION.

11. STREET LIGHT CONDUITS AND LUG WIRING LOCATED WITHIN THE PUBLIC RIGHT-OF-WAY WILL NOT BE MARKED BY PA ONE-CALL AND A SEPARATE CONTACT MUST BE MADE. THE CONTRACTOR SHALL CONTACT THE PHILADELPHIA STREETS DEPARTMENT AT 215-885-1220 (RICH MONTANEZ) NOT LESS THAN (3) DAYS OR MORE THAN 10 DAYS PRIOR TO THE START OF CONSTRUCTION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY WORK TO RELOCATE ANY/L STREET LIGHTING EQUIPMENT.
12. ALL UTILITY LINES TO BE ABANDONED SHALL BE SEALED WITH A MINIMUM OF 12-INCH DEEP MASONRY PLUG UNLESS OTHERWISE NOTED OR REQUIRED BY MORE STRINGENT LOCAL UTILITY REQUIREMENTS.
13. ALL MATERIALS SHALL BE NEW UNLESS USED OR SALVAGED MATERIALS ARE AUTHORIZED BY THE OWNER.
14. ALL FIRE HYDRANTS AND ASSOCIATED WATER MAINS AND VALVES SHALL BE PROTECTED AND REMAIN IN OPERATION AND ACCESSIBLE THROUGHOUT CONSTRUCTION.
15. PROTECT AND MAINTAIN NEWLY CONSTRUCTED EXISTING ADA CURB RAMPS THROUGHOUT CONSTRUCTION. IF THE NEWLY CONSTRUCTED EXISTING ADA CURB RAMPS ARE DAMAGED IN ANY WAY DURING CONSTRUCTION, THEN THE DEVELOPER/CONTRACTOR/OWNER MUST RECONSTRUCT ALL DAMAGED COMPONENTS ACCORDING TO THE ORIGINAL DESIGN OR ADA STANDARDS AND SUBMIT COMPLETED AS-BUILT PERMITS (OS-445) FORMS TO THE PHILADELPHIA STREETS DEPARTMENT ADA UNIT WITHIN 30 CALENDAR DAYS OF FINISHING CONSTRUCTION. FOR ALL DAMAGED/RECONSTRUCTED RAMPS.
16. BELOW GRADE OBSTRUCTIONS SHALL BE REMOVED FROM THE SITE AT DEPTHS OF UP TO 15 FEET BELOW EXISTING GRADES. THESE OBSTRUCTIONS WILL LIKELY BE PROBLEMATIC WITH INSTALLATION OF ANY BELOW GRADE COMPONENT AT THE SITE. MARKING THE LOCATION OF OBSTRUCTIONS AND ABANDONED UTILITIES, AND PRE-EXCAVATION OR PROBE DRILLING OF UTILITY CORRIDORS AND AREAS OF BELOW GRADE COMPONENTS MAY BE PRUDENT TO LOCATE OBSTRUCTIONS FOR PLANNING AND DESIGN PURPOSES.
17. ALL UTILITIES UNREARDED/DISTURBED DURING EXCAVATION ACTIVITIES ARE TO BE REMOVED IN THEIR ENTIRETY. OTHERWISE, UTILITIES 6" DIAMETER AND SMALLER CAN BE LEFT IN PLACE AFTER BEING CUT AND CAPPED. UTILITIES LARGER THAN 6" DIAMETER SHALL BE REMOVED OR GROUDED SOLID AFTER BEING CUT AND CAPPED.
18. THIS PLAN DOES NOT INDICATE THE GENERAL SUBGRADE ELEVATION REQUIRES FOR CONSTRUCTION OF PROPOSED IMPROVEMENTS, NOR THE ADDITIONAL EXCAVATION FOR UNSATURABLE SOILS (IF ENCOUNTERED). CAISSONS, COLUMNS, FOOTINGS, SITE ACCESS/RAMPING AND BELOW SLAB UTILITIES (IF ANY). THE CONTRACTOR SHALL ACCOUNT FOR THE EXTRA EARTHMOVING RESULTING FROM THESE IMPROVEMENTS AND CONDITIONS AND THIS WORK WILL BE INCLUDED IN THE BASE CONTRACT.
19. CONTRACTOR SHALL DIG TEST PITS AT ALL UTILITY CROSSINGS TO VERIFY THE ELEVATION OF THE EXISTING UTILITY AND MAKE A MINIMUM OF TWO TESTS PRIOR TO COMMENCEMENT OF ANY WORK. THE ENGINEER SHALL BE PROVIDED WITH ELEVATIONS AT ALL LOCATIONS.
20. ALL STANDING WATER WITHIN THE DEMOLITION AREA SHALL BE PUMPED.
21. MAINTAIN AND PROTECT EXISTING INLETS AND HYDRANTS. SHOULD THE EXISTING INLETS OR HYDRANTS BE IN ANY WAY DAMAGED DURING CONSTRUCTION, THESE SHOULD BE REPAIRED TO THE SATISFACTION OF THE PWD INSPECTOR REPLACED PER PWD STANDARDS.

EXISTING LEGEND

- EXISTING PROPERTY LINE (APPROXIMATE)
- EXISTING RIGHT OF WAY LINE (APPROXIMATE)
- EXISTING BUILDING
- EXISTING CURB
- EXISTING SIDEWALK
- EXISTING EDGE OF MACADAM/GRAVEL
- EXISTING TRAFFIC MARKING
- EXISTING FENCE
- EXISTING TREE
- EXISTING MAJOR CONTOUR
- EXISTING MINOR CONTOUR
- EXISTING SIGN
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- EXISTING WATER LINE
- EXISTING UNDERGROUND TELEPHONE LINE
- EXISTING GAS LINE
- EXISTING UNDERGROUND ELECTRIC
- EXISTING OVERHEAD WIRES
- EXISTING UTILITY STRUCTURES

PROPOSED LEGEND

- UTILITY/SITE FEATURE TO BE REMOVED
- EXISTING PAVEMENT TO BE REMOVED
- CAP UTILITY
- SITE FEATURE TO BE REMOVED
- MILL AND OVERLAY
- PROPOSED LIMIT OF DISTURBANCE
- PROPOSED CONSTRUCTION FENCE
- PROPOSED TREE PROTECTION

REVISIONS		
ISSUE	DATE	DESCRIPTION
1	01/09/23	PWD PCSM/NPDES SUBMISSION
2	02/14/23	PWD PCSM RESUBMISSION
3	05/24/23	PWD PCSM RESUBMISSION



REVIEWED BY:
PROJECT COORDINATOR:
REALS:



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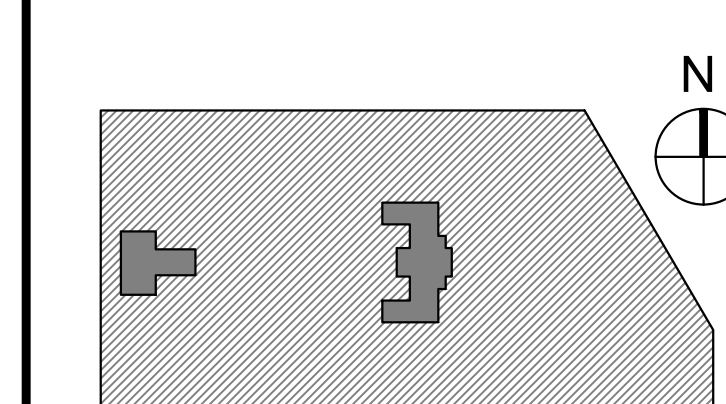
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1515 ARCH STREET
11TH FLOOR, ONE PARKWAY BUILDING
PHILADELPHIA, PENNSYLVANIA

PROJECT TITLE
KINGSESSING RECREATION CENTER

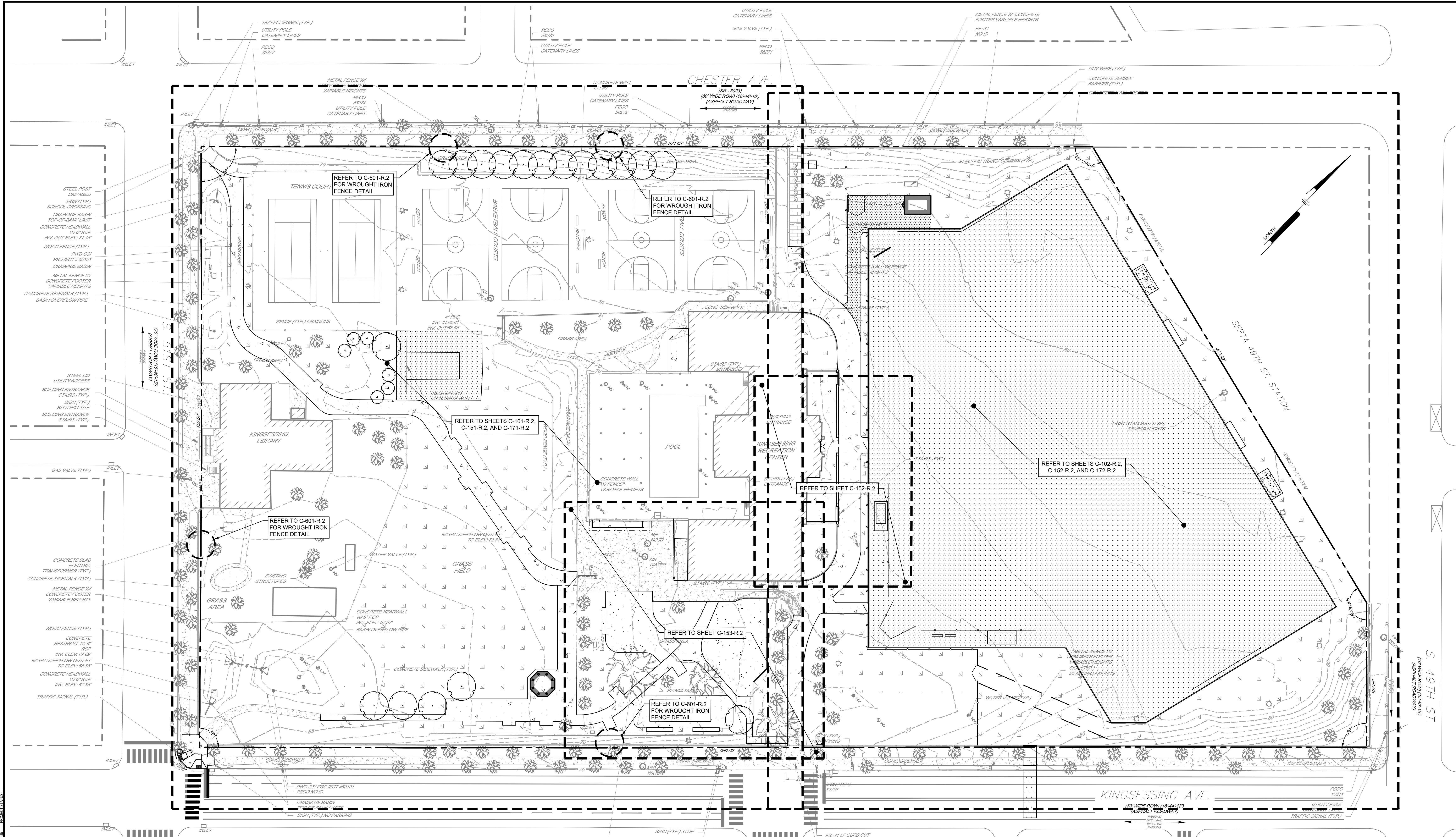
KEY PLAN



DRAWING TITLE
SITE DEMOLITION PLAN

PROJECT NO. KLMLX21003	DRAWING NO.
DATE 08/28/23	C-051-R.2
SCALE 1" = 30'	
DRAWN BY SDL	
CHECKED BY MJM	
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PACKAGE 2 - IFB
NOT FOR CONSTRUCTION
08/28/23
NPDES PERMIT #PAC510302
PWD TRACKING #FY22-KING-6800-01



GENERAL INFORMATION:

SITE:
4801 KINGSESSING AVE
PHILADELPHIA, PA 19143
QPW15030550

CLIENT:
CITY OF PHILADELPHIA
PARKS AND RECREATION
1515 ARCH STREET, 10TH FLOOR
PHILADELPHIA, PA 19102

ENGINEER:
PENNONI ASSOCIATES
1900 MARKET STREET, SUITE 300
PHILADELPHIA, PA 19103

SITE INFORMATION:

MUNICIPAL ZONING INFORMATION:
THE SITE IS LOCATED IN THE FOLLOWING ZONES IN THE CITY OF PHILADELPHIA:
ACTIVE PARKS AND OPEN SPACE (SP-PO-A)
ACCESSORY SIGN CONTROLS - SPECIAL CONTROLS FOR COBBES CREEK, ROOSEVELT BOULEVARD, AND DEPARTMENT OF PARKS AND RECREATION CODE

FOR COMPLETE ZONING INFORMATION PLEASE REFER TO THE ZONING CODE OF THE CITY OF PHILADELPHIA AS CURRENTLY AMENDED

FLOOD ZONE INFORMATION:
BY GRAPHIC PLOTTING ONLY, BASED UPON THE DATE OF THE FLOOD INSURANCE RATE COMMUNITY MAP NO. 420750184H WHICH BEARS AN EFFECTIVE DATE OF NOVEMBER 18, 2015, THE SITE IS LOCATED IN THE FOLLOWING AREAS:
ZONE X AREA OUTSIDE THE 0.2% ANNUAL CHANCE FLOODPLAIN
NO FIELD SURVEYING WAS PERFORMED TO DETERMINE THIS ZONE

REFERENCES:
1. TOPOGRAPHIC INFORMATION TAKEN FROM A PLAN BY AMERICAN ENGINEERS GROUP, LLC ENTITLED "KINGSESSING RECREATION CENTER TOPOGRAPHIC SURVEY", SHEET NO. 1, DATED 03/12/2021
2. BOUNDARY INFORMATION FROM GIS. BOUNDARY SURVEY NOT PERFORMED

SITE NOTES:

- PWD TRACKING #FY22-KING-6800-01
- UNLESS SPECIFICALLY STATED OR SHOWN HEREON TO THE CONTRARY, THIS SURVEY IS MADE SUBJECT TO AND DOES NOT LOCATE OR DELINEATE:
 - RIGHTS OR INTERESTS OF THE UNITED STATES OF AMERICA OR COMMONWEALTH OF PENNSYLVANIA OVER LANDS NOW OR FORMERLY FLOWED BY TIDEWATER BUT NO LONGER VISIBLE OR PHYSICALLY EVIDENT, OR LANDS CONTAINING ANY ANIMAL, MARINE, OR BOTANICAL SPECIES REGULATED BY OR UNDER THE JURISDICTION OF ANY FEDERAL, STATE, OR LOCAL AGENCY.
 - BUILDING SETBACK LINES, ZONING REGULATIONS OR LINES ESTABLISHED BY ANY FEDERAL, STATE OR LOCAL AGENCY WHICH MAY AFFECT THE BUILDING OR DEVELOPMENT POTENTIAL OF THE SUBJECT PROPERTY.
- ANY SUBSURFACE OR SUBTERRANEAN CONDITION, EASEMENTS OR RIGHTS INCLUDING BUT NOT LIMITED TO MINERAL OR MINING RIGHTS, OR THE LOCATION OF OR RIGHTS TO ANY SUBSURFACE STRUCTURES, CONTAINERS OR FACILITIES, OR ANY OTHER NATURAL, OR MAN-MADE SUBSURFACE CONDITION WHICH MAY OR MAY NOT AFFECT THE USE OR DEVELOPMENT POTENTIAL OF THE SUBJECT PROPERTY.
- LOCATIONS OF ON AND OFF SITE UTILITIES AS SHOWN ARE APPROXIMATE AND MAY OR MAY NOT BE COMPLETE. THE NATURE AND EXACT LOCATION OF EXISTING UTILITIES SHOULD BE VERIFIED PRIOR TO INITIATING ANY ACTIVITY THAT MAY AFFECT THEIR USE OR LOCATION.
- THE LOCATION OF THE EXISTING UNDERGROUND UTILITIES SHOWN ON THIS PLAN HAVE BEEN TAKEN FROM EXISTING UTILITY RECORDS AVAILABLE AT THE TIME THESE PLANS WERE PREPARED AND FROM SURFACE OBSERVATION OF THE SITE.
- COMPLETENESS OR ACCURACY OF LOCATION AND DEPTH OF UNDERGROUND UTILITIES AND STRUCTURES IS NOT GUARANTEED.
- IN ACCORDANCE WITH PA ACT 287 OF 1974 AS AMENDED BY PA ACT 121 OF 2008 ENTITLED "UNDERGROUND UTILITY LINE PROTECTION LAW", THE CONTRACTOR SHALL NOTIFY ALL UTILITIES WITHIN THE WORK AREA VIA THE PENNSYLVANIA ONE CALL SYSTEM INC. (800-242-1776) A MINIMUM OF 3 WORKING DAYS BEFORE THE START OF EXCAVATION.
- THE CONTRACTOR SHALL VERIFY LOCATIONS AND DEPTHS OF ALL UNDERGROUND UTILITIES AND STRUCTURES BEFORE THE START OF WORK.
- ALL BOUNDARY DIMENSIONS SHOWN ARE IN CITY OF PHILADELPHIA DISTRICT STANDARD MEASURE (DS) UNLESS DESIGNATED (US) TO DENOTE U.S. SURVEY FOOT.
- THE HORIZONTAL DATUM FOR THIS PROJECT REFERENCES THE PENNSYLVANIA STATE PLANE COORDINATE SYSTEM, SOUTH ZONE, NORTH AMERICAN DATUM OF 1983
- THE VERTICAL DATUM FOR THIS PROJECT REFERENCES THE CITY OF PHILADELPHIA VERTICAL DATUM.

EXISTING LEGEND		PROPOSED LEGEND	
	EXISTING PROPERTY LINE (APPROXIMATE)		PROPOSED BUILDING
	EXISTING RIGHT OF WAY LINE (APPROXIMATE)		PROPOSED DOOR
	EXISTING BUILDING		PROPOSED CONCRETE PAVEMENT
	EXISTING CURB		PROPOSED ASPHALT PAVEMENT
	EXISTING SIDEWALK		PROPOSED LANDSCAPED AREA
	EXISTING EDGE OF MACADAM/GRAVEL		PROPOSED UNIT PAVERS
	EXISTING TRAFFIC MARKING		PROPOSED POROUS PLAYGROUND SAFETY SURFACE (PIC)
	EXISTING FENCE		PROPOSED SYNTHETIC TURF FIELD
	EXISTING TREE		PROPOSED ADA RAMP
	EXISTING MAJOR CONTOUR		PROPOSED CURB
	EXISTING MINOR CONTOUR		PROPOSED DEPRESSED CURB
	EXISTING SIGN		PROPOSED WALL
	EXISTING BOLLARD		PROPOSED STOP BAR/PAVEMENT MARKING
	EXISTING UTILITY POLE		PROPOSED FENCE
	EXISTING LIGHT		PROPOSED SIGN
	EXISTING INLET		
	EXISTING SEWER		
	EXISTING STORM SEWER		
	EXISTING WATER LINE		
	EXISTING UNDERGROUND TELEPHONE LINE		
	EXISTING GAS LINE		
	EXISTING UNDERGROUND ELECTRIC		
	EXISTING OVERHEAD WIRES		
	EXISTING UTILITY STRUCTURES		

REVISIONS		
ISSUE	DATE	DESCRIPTION
1	01/09/23	PWD PCSMNPDES SUBMISSION
2	02/14/23	PWD PCSM RESUBMISSION
3	05/24/23	PWD PCSM RESUBMISSION

REVIEWED BY: _____
PROJECT COORDINATOR: _____
REALS: _____

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www.kmarchitects.com

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LEED CONSULTANT:
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CITY OF PHILADELPHIA
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11TH FLOOR, ONE PARKWAY BUILDING
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PROJECT TITLE
KINGSESSING RECREATION CENTER

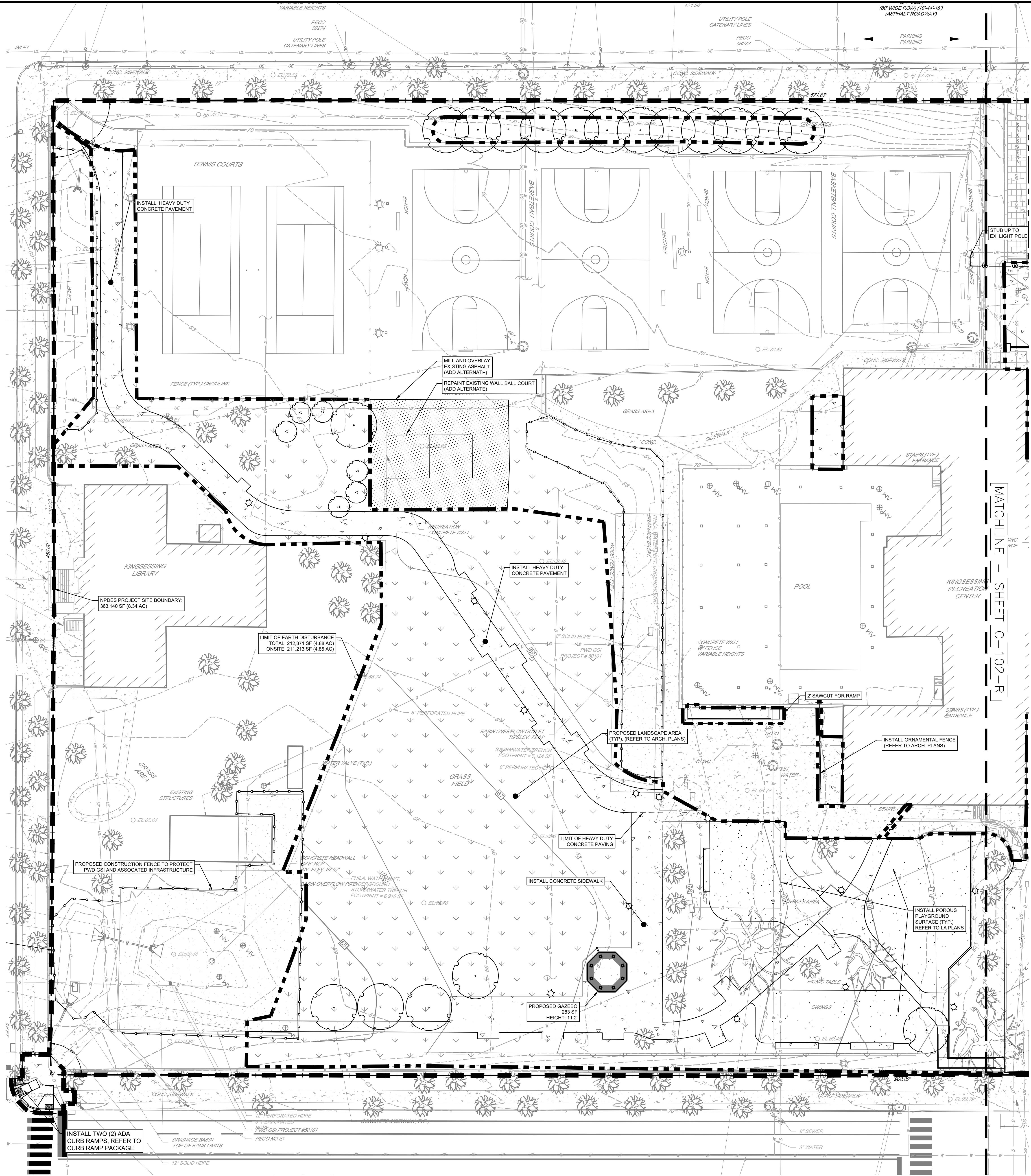
KEY PLAN

DRAWING TITLE
OVERALL SITE KEY PLAN

PROJECT NO. KMLXL21003	DRAWING NO.
DATE 08/28/23	C-100-R.2
SCALE 1" = 30'	
DRAWN BY SDL	
CHECKED BY MJM	

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NPDES PERMIT #PAC510302
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- | EXISTING LEGEND | PROPOSED LEGEND |
|----------------------------------------------|---------------------------------------------------|
| --- EXISTING PROPERTY LINE (APPROXIMATE) | ▭ PROPOSED BUILDING |
| --- EXISTING RIGHT OF WAY LINE (APPROXIMATE) | ▭ PROPOSED DOOR |
| ▭ EXISTING BUILDING | ▭ PROPOSED CONCRETE PAVEMENT |
| ▭ EXISTING CURB | ▭ PROPOSED ASPHALT PAVEMENT |
| ▭ EXISTING SIDEWALK | ▭ PROPOSED LANDSCAPED AREA |
| ▭ EXISTING EDGE OF MACADAM/GRAVEL | ▭ PROPOSED UNIT PAVERS |
| ▭ EXISTING TRAFFIC MARKING | ▭ PROPOSED POROUS PLAYGROUND SAFETY SURFACE (DIC) |
| ▭ EXISTING FENCE | ▭ PROPOSED SYNTHETIC TURF FIELD |
| ● EXISTING TREE | ▭ PROPOSED ADA RAMP |
| --- EXISTING MAJOR CONTOUR | ▭ PROPOSED CURB |
| --- EXISTING MINOR CONTOUR | ▭ PROPOSED DEPRESSED CURB |
| ▭ EXISTING SIGN | ▭ PROPOSED WALL |
| ▭ EXISTING BOLLARD | ▭ PROPOSED STOP BAR/PAVEMENT MARKING |
| ▭ EXISTING UTILITY POLE | ▭ PROPOSED FENCE |
| ▭ EXISTING LIGHT | ▭ PROPOSED SIGN |
| ▭ EXISTING INLET | ▭ PROPOSED LIMIT OF DISTURBANCE |
| ▭ EXISTING SEWER | ▭ PROPOSED CONSTRUCTION FENCE |
| ▭ EXISTING STORM SEWER | |
| ▭ EXISTING WATER LINE | |
| ▭ EXISTING UNDERGROUND TELEPHONE LINE | |
| ▭ EXISTING GAS LINE | |
| ▭ EXISTING UNDERGROUND ELECTRIC | |
| ▭ EXISTING OVERHEAD WIRES | |
| ⊙ EXISTING UTILITY STRUCTURES | |

SITE PLAN DETAIL - SOUTH
SCALE: 1"=20'-0"

PACKAGE 2 - IFB
NOT FOR CONSTRUCTION
08/28/23
NPDES PERMIT #PAC510302
PWD TRACKING #FY22-KING-6800-01

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PROJECT COORDINATOR:

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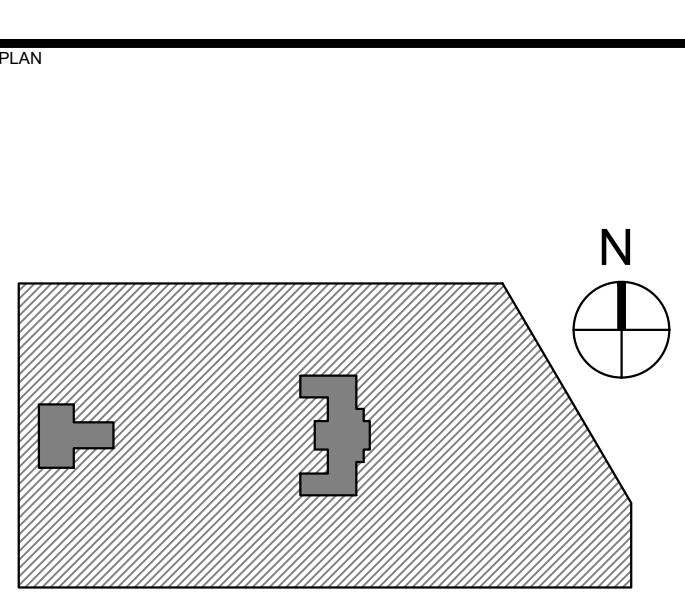
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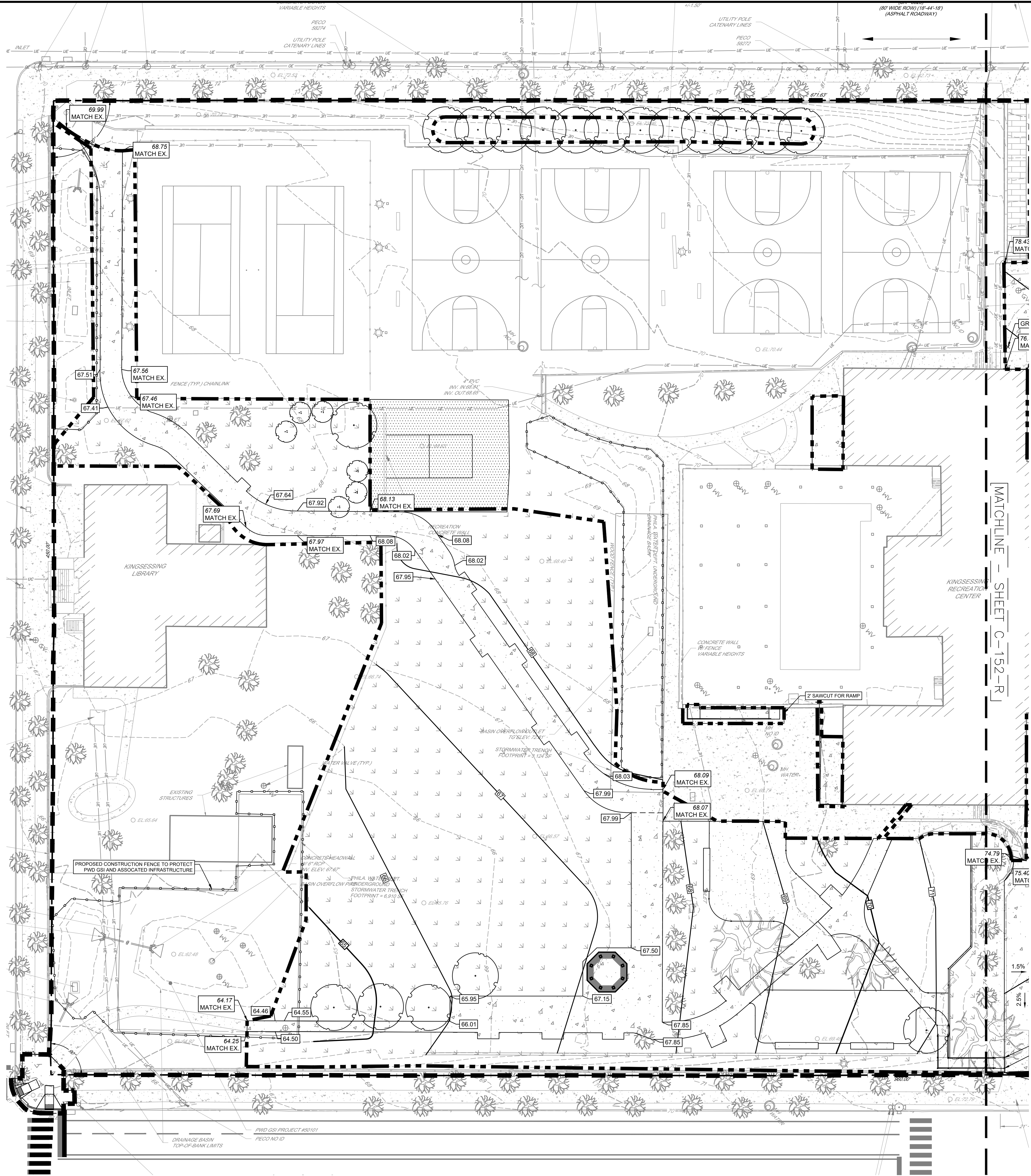
PROJECT TITLE
KINGSESSING RECREATION CENTER



DRAWING TITLE
SITE IMPROVEMENT PLAN SOUTH

PROJECT NO. KLMLX21003	DRAWING NO. C-101-R.2
DATE 08/28/23	SCALE 1" = 20'
DRAWN BY SDL	CHECKED BY MJM

NOTE: ALL DIMENSIONS AND CONDITIONS SHALL BE VERIFIED BY THE CONTRACTOR AT THE SITE BEFORE PROCEEDING WITH THE WORK.



GRADING PLAN DETAIL SOUTH
SCALE: 1"=20'-0"

GRADING AND EARTHWORK NOTES:

- A GEOTECHNICAL ENGINEER IS REQUIRED TO INSPECT, TEST AND CERTIFY TO THE COMPACTION OF ALL LOAD BEARING FILLS. ALL EXISTING UNDERGROUND UTILITIES SHALL BE REMOVED OR RELOCATED. THE PREPARED SUBGRADE SHALL BE PROOF ROLLED WITH A SMOOTH-DRUM VIBRATING ROLLER TO DELINEATE SOFT UNSTABLE AREAS AND COMPACT SOILS DISTURBED DURING EXCAVATION OPERATIONS. AREAS WHICH EXHIBIT INSTABILITY SHALL BE UNDERCUT AND REPLACED WITH LOAD-BEARING FILL.
- MINIMUM PAVEMENT GRADE SHALL BE 1.0% SLOPE UNLESS NOTED OTHERWISE.
- SEEKING REQUIREMENTS SPECIFIED HEREIN ARE TO BE CONSIDERED AS MINIMUMS FOR RELATIVELY DRY, STABLE EARTH CONDITIONS. ADDITIONAL BEDDING SHALL BE REQUIRED FOR ROCK TRENCHES AND WET AREAS. CONTRACTOR SHALL HAVE THE RESPONSIBILITY TO PROVIDE SUCH ADDITIONAL BEDDING AS MAY BE REQUIRED TO PROPERLY CONSTRUCT THE WORK.
- COMPACTION OF THE BACKFILL OF ALL TRENCHES SHALL BE COMPACTED TO THE DENSITY OF 95% OF THEORETICAL MAXIMUM DRY DENSITY (ASTM D698). BACKFILL MATERIAL SHALL BE FREE FROM ROOTS, STUMPS, OR OTHER FOREIGN DEBRIS AND SHALL BE PLACED IN LIFTS NOT TO EXCEED 6 INCHES IN COMPACTED FILL THICKNESS. CORRECTION OF ANY TRENCH SETTLEMENT WITHIN A YEAR FROM THE DATE OF APPROVAL WILL BE THE RESPONSIBILITY OF THE CONTRACTOR.
- THE CONTRACTOR WILL INSURE THAT POSITIVE AND ADEQUATE DRAINAGE IS MAINTAINED AT ALL TIMES WITHIN THE PROJECT LIMITS. THIS MAY INCLUDE, BUT NOT BE LIMITED TO, REPLACEMENT OR RECONSTRUCTION OF EXISTING DRAINAGE STRUCTURES THAT HAVE BEEN DAMAGED OR REMOVED OR REGRADING AS REQUIRED BY THE ENGINEER, EXCEPT FOR THOSE DRAINAGE ITEMS SHOWN AT SPECIFIC LOCATIONS AND HAVING SPECIFIC PAY ITEMS IN THE DETAILED ESTIMATE. NO SEPARATE PAYMENT WILL BE MADE FOR ANY COSTS INCURRED TO COMPLY WITH THIS REQUIREMENT.
- THE CONTRACTOR SHALL PROVIDE ANY AND ALL EXCAVATION AND MATERIAL SAMPLES NECESSARY TO CONDUCT REQUIRED SOIL TESTS. ALL ARRANGEMENTS AND SCHEDULING FOR THE TESTING SHALL BE THE CONTRACTOR'S RESPONSIBILITY.
- SOILS TESTING AND ON-SITE INSPECTION SHALL BE PERFORMED BY AN INDEPENDENT GEOTECHNICAL ENGINEER. THE SOILS ENGINEER SHALL PROVIDE COPIES OF TEST REPORTS TO THE CONTRACTOR, THE OWNER AND THE OWNER'S REPRESENTATIVE AND SHALL PROMPTLY NOTIFY THE OWNER, HIS REPRESENTATIVE AND THE CONTRACTOR SHOULD WORK PERFORMED BY THE CONTRACTOR FAIL TO MEET THESE SPECIFICATIONS.
- CONTRACTOR SHALL FURNISH AND MAINTAIN ALL NECESSARY BARRICADES AROUND THE WORK AREA AND SHALL PROVIDE PROTECTION AGAINST WATER DAMAGE AND SOIL EROSION.
- MAXIMUM SIDEWALK CROSS SLOPE IS 2% AND MINIMUM SIDEWALK CROSS SLOPE IS 1.0% WITHIN THE CITY RIGHT OF WAY.
- ELEVATIONS ARE BASED ON CITY OF PHILADELPHIA VERTICAL DATUM.
- REFER TO GEOTECHNICAL REPORT FOR BELOW GRADE CONDITIONS AND SITE PREPARATION/EXCAVATION REQUIREMENTS.
- ALL UTILITY VALVES AND MANHOLE STRUCTURES WITHIN THE SIDEWALKS AND ROADWAYS SHALL BE ADJUSTED TO THE PROPOSED GRADES AS NECESSARY. CONTRACTOR SHALL COORDINATE WITH THE APPROPRIATE UTILITY COMPANY.
- FILL CAN BE REUSED AS A COMPACTED FILL FOR BACKFILL, AS LONG AS IT IS FREE OF TRASH, ENVIRONMENTAL HAZARDS, AND OTHER DELETERIOUS MATERIAL. ADJUSTING MOISTURE CONTENT PRIOR TO FILL PLACEMENT SHOULD BE EXPECTED.
- ALL DIMENSIONS SHOWN ARE IN US SURVEY FEET (US) UNLESS DESIGNATED CITY OF PHILADELPHIA DISTRICT STANDARD MEASURE (DS).
- THE DISTRICT STANDARD FOR CONVERSION IS US = DS X 1.0025.
- DEMOLITION DEBRIS ASSOCIATED WITH THE STRUCTURE TO BE REMOVED CONSISTING OF CONCRETE AND OTHER MASONRY PRODUCTS CAN BE CRUSHED AND REUSED IN NEW LOAD-BEARING FILL, PROVIDED IT IS FREE OF THE LIMITATIONS DESCRIBED IN NOTE #13 ABOVE.
- CONTROLLED/COMPACTED FILL SHALL BE PLACED IN EARLY STAGES OF CONSTRUCTION TO ALLOW FOR SITE SETTLEMENT DURING CONSTRUCTION OF BUILDING.
- ALL SITE PREPARATION AND EARTHWORK OPERATIONS SHALL BE CARRIED OUT IN THE FULL-TIME PRESENCE OF A QUALIFIED REPRESENTATIVE OF THE GEOTECHNICAL ENGINEER.
- WHERE OBSTRUCTIONS SUCH AS CONCRETE, FOUNDATIONS, BRICK, AND WOOD ARE ENCOUNTERED ABOVE THE COMPRESSIBLE LAYERS, PRE-DRILLING FOR THE PVDs WILL BE REQUIRED. THEREFORE, CONSIDERABLE EXTRA COSTS COULD BE INVOLVED AND A CONTINGENCY FOR PRE-DRILLING SHOULD BE CARRIED IN THE CONSTRUCTION BUDGET. THE RUBBLE FROM DEMOLITION SHOULD BE CRUSHED OR STAGED FOR REUSE AND IMPORTED FILL SHOULD NOT CONTAIN EXCESS OVERSIZE MATERIALS THAT WOULD PREVENT THE DIRECT PUSH INSTALLATION OF THE WICK DRAINS.
- GIVEN THE LIMITED WORK AREA, IT WILL BE IMPORTANT TO MAINTAIN POSITIVE DRAINAGE ACROSS THE SITE. A DRAINAGE BLANKET CONSISTING OF GRAVEL OR SAND WITH LESS THAN 10 PERCENT FINES SHOULD BE PLACED ACROSS THE WORK AREA BEFORE PLACING ALL OF THE FILL. THE BLANKET SHOULD BE AT LEAST 12 INCHES THICK AND SHOULD CONVEY WATER TO A PERIMETER DITCH. ALTERNATIVELY, OR IN COMBINATION WITH THE DRAINAGE BLANKET, HORIZONTAL STRIP DRAINS CAN BE ATTACHED TO THE VERTICAL DRAINS.
- ALL FILL MATERIALS, INCLUDING SUBGRADE SOILS, SHOULD BE PLACED IN 8" TO 10-INCH THICK LIFTS AND BE COMPACTED TO A MINIMUM OF 95% OF THE MAXIMUM DRY DENSITY IN GENERAL ACCORDANCE WITH ASTM D698. THE FILL SOILS SHOULD HAVE AN IN-PLACE WET WEIGHT OF ABOUT 125 TO 135 POUNDS PER CUBIC FOOT TO BE CONSISTENT WITH THE LOADINGS ASSUMED IN OUR ANALYSIS.
- FOLLOWING THE STRIPPING OF ASPHALT AND ORGANIC MATERIALS, IF FILL MATERIALS ARE REQUIRED THE EXPOSED SUBGRADE SOILS SHOULD BE EVALUATED. IDEALLY, THE EVALUATION SHOULD CONSIST OF PROOFROLLING AND COMPACTING THE SOILS TO A DENSE AND UNYIELDING CONSISTENCY BY SEVERAL PASSES OF A LOADED TRIAXIAL DUMP TRUCK, OFF-ROAD DUMP TRUCK, OR LARGE SMOOTH DRUM VIBRATORY COMPACTOR WITH A STATIC DRUM WEIGHT OF AT LEAST TEN TONS, ALTHOUGH SOME OTHER METHOD MAY BE DEEMED MORE APPROPRIATE BY THE GEOTECHNICAL ENGINEER DEPENDING ON THE PREVAILING WEATHER CONDITIONS. SOILS THAT ARE OBSERVED TO BE SOFT OR UNSTABLE DURING THE EVALUATION SHOULD BE SELECTIVELY OVER-EXCAVATED, AND THE RESULTANT EXCAVATIONS SHOULD BE BACKFILLED WITH CONTROLLED AND COMPACTED FILL.
- UPON SATISFACTORY EVALUATION OF THE SUBGRADE, STRUCTURAL FILL SHOULD BE PLACED IN LIFTS AND COMPACTED IN ACCORDANCE WITH THE SPECIFICATIONS INCLUDED IN THIS REPORT. THE MATERIALS WILL TYPICALLY NEED TO BE WITHIN 2 TO 3 PERCENTAGE POINTS OF THE STANDARD PROCTOR MAXIMUM DRY DENSITY. COMPACTIVE EFFORT IS APPLIED. OFF-SITE BORROW SHOULD GENERALLY MEET UNIFIED SOIL CLASSIFICATION SYSTEM (USCS) DESIGNATION SM, SP-SM, GP-CM, GM, OR GW AND BE APPROVED BY THE GEOTECHNICAL ENGINEER PRIOR TO USE. CONSIDERATION OF OFF-SITE BORROW MEETING OTHER USCS CLASSIFICATIONS WOULD BE MADE BY THE GEOTECHNICAL ENGINEER BASED ON THE LOCATION AND DEPTH TO WHICH THOSE MATERIALS ARE PLACED.
- ALL STRUCTURAL FILL SHOULD BE CONSTRUCTED IN MAXIMUM 8-INCH THICK LOOSE LIFTS AND BE COMPACTED TO 95 PERCENT OF THE STANDARD PROCTOR MAXIMUM DRY DENSITY, WITH MOISTURE CONTENTS WITHIN 3 PERCENTAGE POINTS OF OPTIMUM FOR COMPACTION.
- ALL TEMPORARY SURCHARGE FILL SHALL BE CONSTRUCTED IN MAXIMUM 8-INCH THICK LOOSE LIFT AND BE COMPACTED TO 95 PERCENT OF THE STANDARD PROCTOR MAXIMUM DRY DENSITY, WITH MOISTURE CONTENTS WITHIN 3 PERCENTAGE POINTS OF OPTIMUM FOR COMPACTION.
- FILL SUBGRADES AND EACH LIFT OF FILL SHOULD BE OBSERVED AND TESTED BY A SOILS TECHNICIAN ON A FULL-TIME BASIS, UNDER THE SUPERVISION OF A REGISTERED ENGINEER AS REQUIRED PER THE INTERNATIONAL BUILDING CODE. ALL COMPACTIVE EFFORT SHOULD BE VERIFIED BY IN-PLACE DENSITY TESTING. NEW FILLS CONSTRUCTED ON SLOPES STEEPER THAN 3H:1V (HORIZONTAL TO VERTICAL) SHOULD BE KEPT INTO EXISTING SLOPED FOR STABILITY CONSIDERATIONS. GTA RECOMMENDS THAT ALL PERMANENT SLOPES SHOULD BE GRADED 3H:1V OR FLATTER. ALL FILL SLOPES STEEPER THAN 3H:1V SHOULD GENERALLY BE PLACED AS STRUCTURAL FILL AND BE CONTROLLED AND COMPACTED TO MINIMUM DENSITIES AS SPECIFIED ABOVE.
- POSITIVE DRAINAGE SHALL BE MAINTAINED ACROSS THE SITE DURING CONSTRUCTION TO PREVENT PONDING OF WATER. SINCE THE EXPOSED SUBGRADES COULD DESTABILIZE IN COMBINATION WITH CONSTRUCTION TRAFFIC AND PRECIPITATION, FURTHERMORE, HEAVY CONSTRUCTION TRAFFIC SHOULD GENERALLY BE RUN ON DESIGNATED HAUL ROADS DURING PERIODS OF WET WEATHER TO REDUCE THE POTENTIAL FOR DESTABILIZATION OF MORE SUBGRADE AREAS THAN NECESSARY. IF THE SUBGRADE IS DISTURBED BY CONSTRUCTION TRAFFIC AND BECOMES UNSTABLE, UNDERCUTTING AND REPLACEMENT OF THESE SUBGRADE MATERIALS WILL LIKELY BE REQUIRED.

EXISTING LEGEND

- EXISTING PROPERTY LINE (APPROXIMATE)
- EXISTING RIGHT OF WAY LINE (APPROXIMATE)
- EXISTING BUILDING
- EXISTING CURB
- EXISTING SIDEWALK
- EXISTING EDGE OF MACADAM/GRAVEL
- EXISTING TRAFFIC MARKING
- EXISTING FENCE
- EXISTING TREE
- EXISTING MAJOR CONTOUR
- EXISTING MINOR CONTOUR
- EXISTING SIGN
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- EXISTING GAS LINE
- EXISTING UNDERGROUND ELECTRIC
- EXISTING OVERHEAD WIRES
- EXISTING UTILITY STRUCTURES

PROPOSED LEGEND

- PROPOSED BUILDING
- PROPOSED DOOR
- PROPOSED CONCRETE PAVEMENT
- PROPOSED ASPHALT PAVEMENT
- PROPOSED LANDSCAPED AREA
- PROPOSED UNIT PAVERS
- PROPOSED POROUS PLAYGROUND SAFETY SURFACE (PIC)
- PROPOSED SYNTHETIC TURF FIELD
- PROPOSED ADA RAMP
- PROPOSED CURB
- PROPOSED DEPRESSED CURB
- PROPOSED WALL
- PROPOSED STOP BAR/PAVEMENT MARKING
- PROPOSED FENCE
- PROPOSED SIGN
- PROPOSED LIMIT OF DISTURBANCE
- PROPOSED CONSTRUCTION FENCE
- PROPOSED MAJOR CONTOUR
- PROPOSED MINOR CONTOUR
- PROPOSED SPOT ELEV. 6.45
- PROPOSED LOW POINT ELEV. 7C.50
- PROPOSED TOP OF CURB ELEV. 7C.75
- PROPOSED BOTTOM OF CURB ELEV. 7C.70

REVISIONS

ISSUE	DATE	DESCRIPTION
1	01/09/23	PWD PCSMNPDES SUBMISSION
2	02/14/23	PWD PCSM SUBMISSION
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REVIEWED BY: _____
PROJECT COORDINATOR

SCALE: _____



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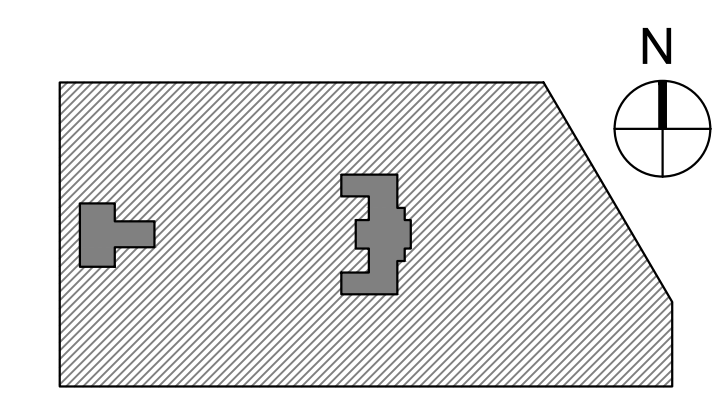
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PROJECT TITLE
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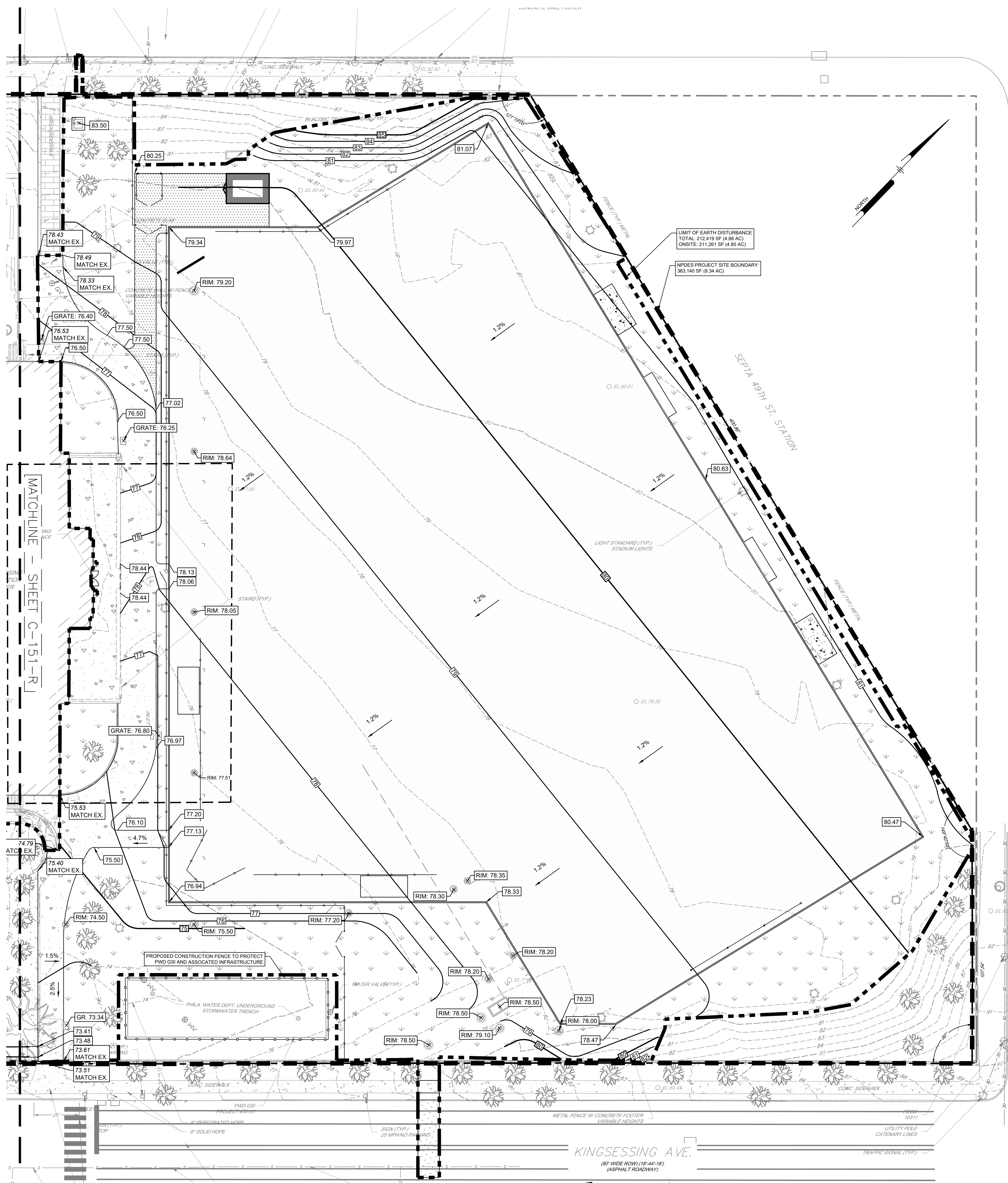
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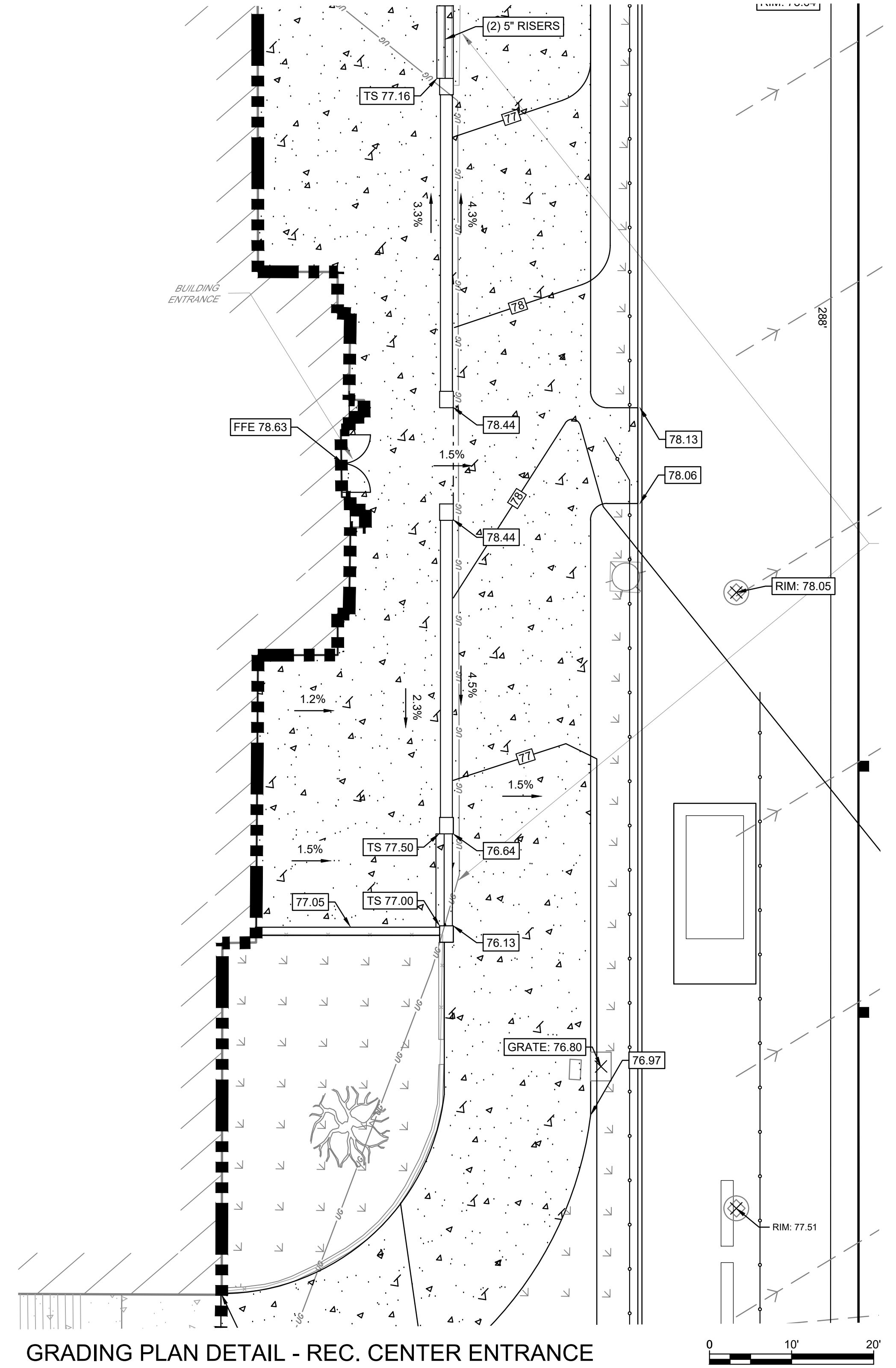
DRAWING TITLE
**DETAIL GRADING
PLAN SOUTH**

PROJECT NO. KLMLX21003	DRAWING NO. C-151-R.2
DATE 08/28/23	SCALE 1" = 20'
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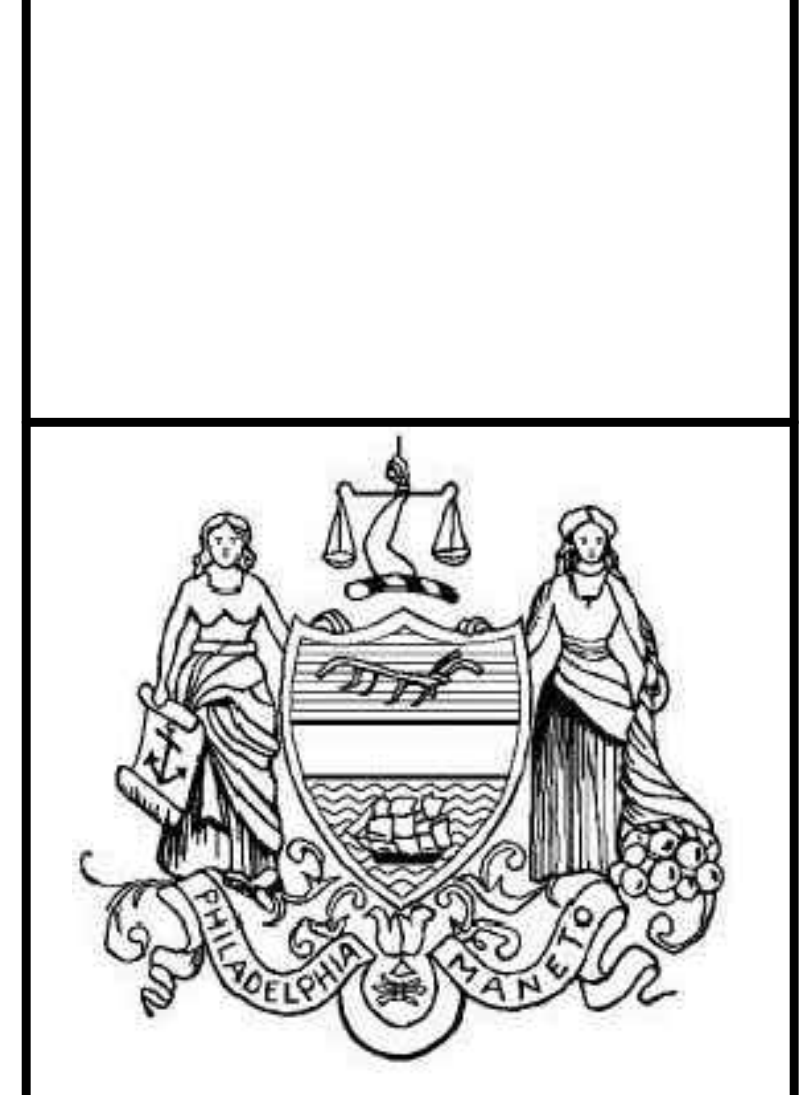
GRADING PLAN DETAIL - NORTH
SCALE: 1"=20'-0"



GRADING PLAN DETAIL - REC. CENTER ENTRANCE
SCALE: 1"=10'-0"

EXISTING LEGEND	PROPOSED LEGEND
--- EXISTING PROPERTY LINE (APPROXIMATE)	▭ PROPOSED BUILDING
- - - EXISTING RIGHT OF WAY LINE (APPROXIMATE)	▭ PROPOSED DOOR
▭ EXISTING BUILDING	▭ PROPOSED CONCRETE PAVEMENT
▭ EXISTING CURB	▭ PROPOSED ASPHALT PAVEMENT
▭ EXISTING SIDEWALK	▭ PROPOSED LANDSCAPED AREA
▭ EXISTING EDGE OF MACADAM/GRAVEL	▭ PROPOSED UNIT PAVERS
▭ EXISTING TRAFFIC MARKING	▭ PROPOSED POROUS PLAYGROUND SAFETY SURFACE (DIC)
▭ EXISTING FENCE	▭ PROPOSED SYNTHETIC TURF FIELD
● EXISTING TREE	▭ PROPOSED ADA RAMP
▭ EXISTING MAJOR CONTOUR	▭ PROPOSED CURB
▭ EXISTING MINOR CONTOUR	▭ PROPOSED DEPRESSED CURB
▭ EXISTING SIGN	▭ PROPOSED WALL
▭ EXISTING BOLLARD	▭ PROPOSED STOP BAR/PAVEMENT MARKING
▭ EXISTING UTILITY POLE	▭ PROPOSED FENCE
▭ EXISTING LIGHT	▭ PROPOSED SIGN
▭ EXISTING INLET	▭ PROPOSED LIMIT OF DISTURBANCE
▭ EXISTING SEWER	▭ PROPOSED CONSTRUCTION FENCE
▭ EXISTING STORM SEWER	▭ PROPOSED MAJOR CONTOUR
▭ EXISTING WATER LINE	▭ PROPOSED MINOR CONTOUR
▭ EXISTING UNDERGROUND TELEPHONE LINE	6.45 PROPOSED SPOT ELEV.
▭ EXISTING GAS LINE	LP 6.00 PROPOSED LOW POINT ELEV.
▭ EXISTING UNDERGROUND ELECTRIC	TC 7.50 PROPOSED TOP OF CURB ELEV.
▭ EXISTING OVERHEAD WIRES	BC 7.00 PROPOSED BOTTOM OF CURB ELEV.
⊙ EXISTING UTILITY STRUCTURES	

REVISIONS		
ISSUE	DATE	DESCRIPTION
1	01/09/23	PWD PCSM/NPDES SUBMISSION
2	02/14/23	PWD PCSM RESUBMISSION
3	05/24/23	PWD PCSM RESUBMISSION



REVIEWED BY:
PROJECT COORDINATOR:
REALS:



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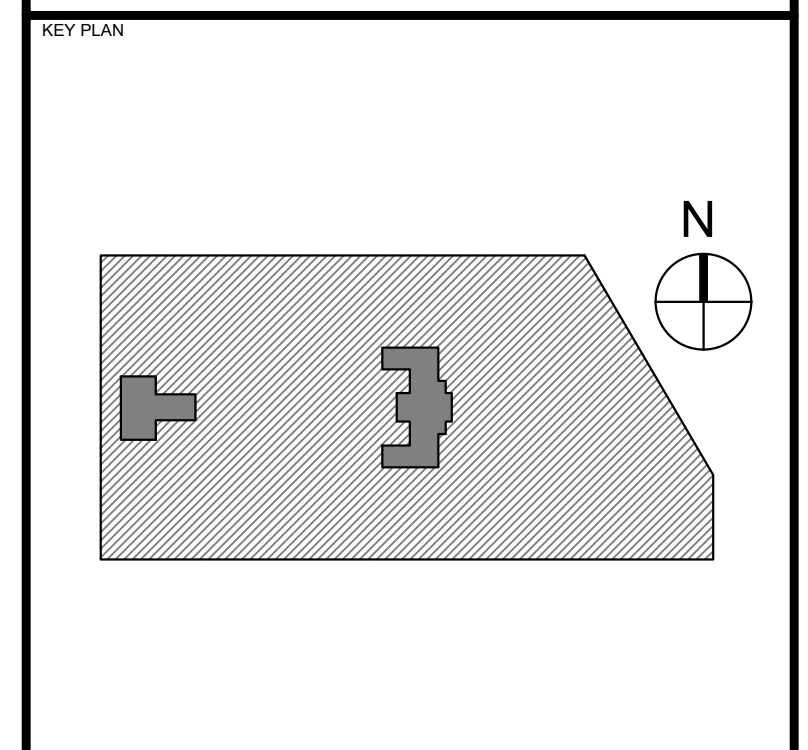
LANDSCAPE ARCHITECT:
Salt Design Studio
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M.E.P./F.P./SITE CIVIL ENGINEERS:
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CITY OF PHILADELPHIA
DEPARTMENT OF PUBLIC PROPERTY
1515 ARCH STREET
11TH FLOOR, ONE PARKWAY BUILDING
PHILADELPHIA, PENNSYLVANIA

PROJECT TITLE
KINGSESSING RECREATION CENTER



DRAWING TITLE
DETAIL GRADING PLAN NORTH

PROJECT NO. KMLX21003	DRAWING NO. C-152-R.2
DATE 08/28/23	SCALE AS NOTED
DRAWN BY SDJ	CHECKED BY MJM

PACKAGE 2 - IFB
NOT FOR CONSTRUCTION
08/28/23
NPDES PERMIT #PAC510302
PWD TRACKING #FY22-KING-6800-01

NOTE: ALL DIMENSIONS AND CONDITIONS SHALL BE VERIFIED BY THE CONTRACTOR AT THE SITE BEFORE PROCEEDING WITH THE WORK.

REVISIONS

ISSUE	DATE	DESCRIPTION
1	01/09/23	PWD PCSM/NPDES SUBMISSION
2	02/14/23	PWD PCSM RESUBMISSION
3	05/24/23	PWD PCSM RESUBMISSION



REVIEWED BY:

PROJECT COORDINATOR

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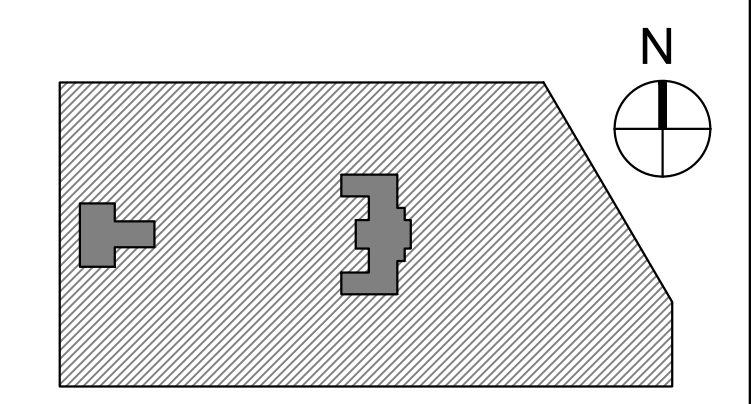
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PHILADELPHIA, PENNSYLVANIA

PROJECT TITLE
KINGSESSING RECREATION CENTER

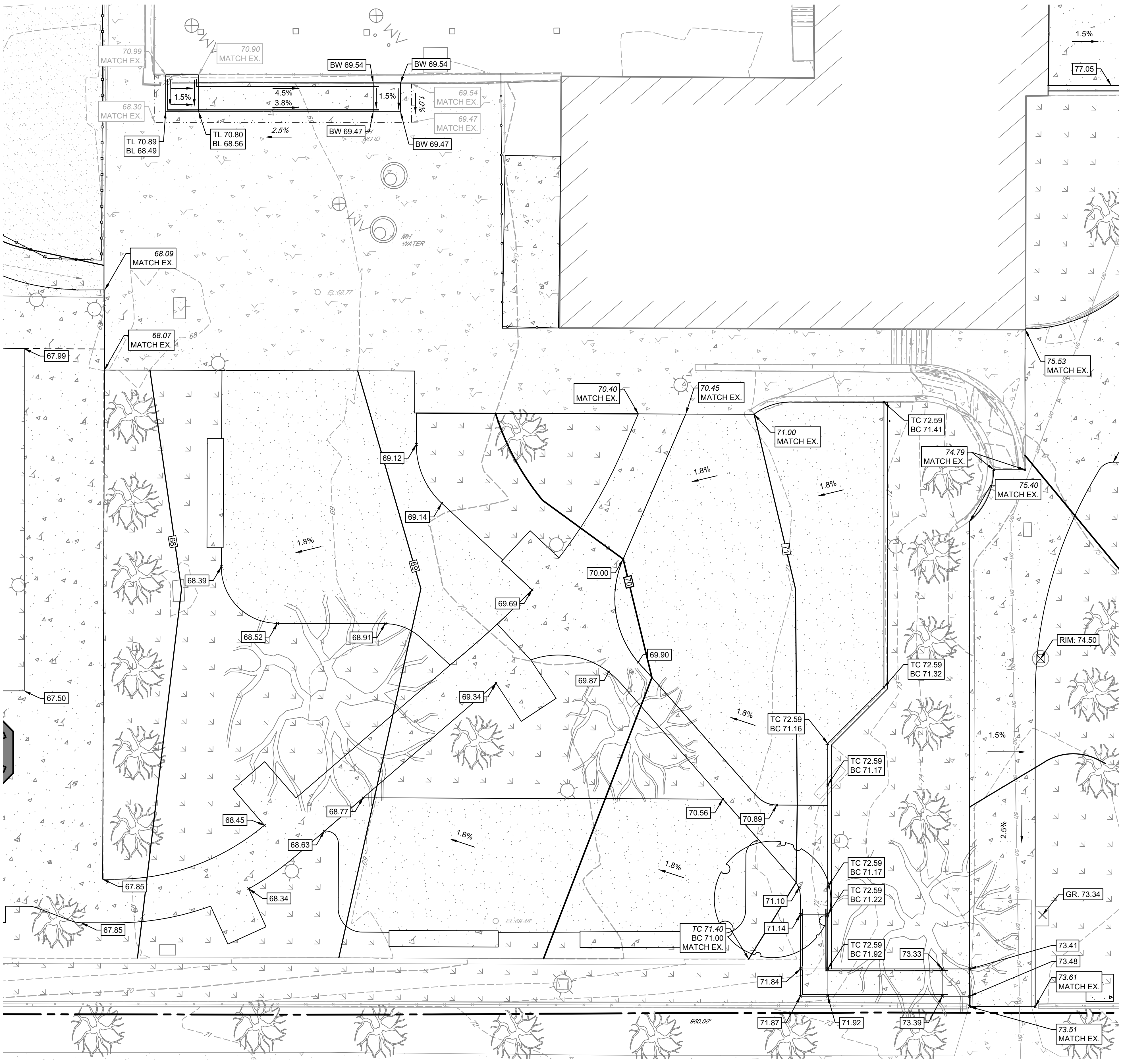
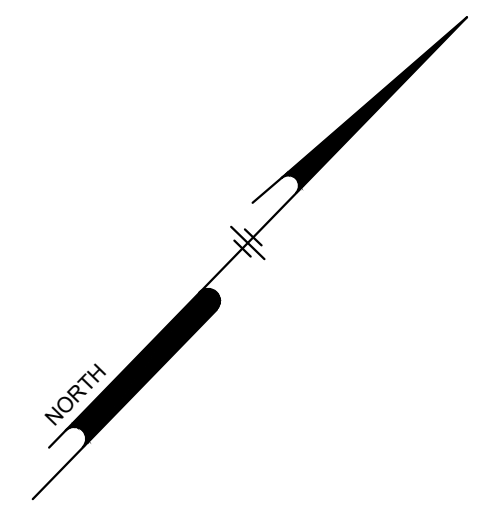
KEY PLAN



DRAWING TITLE
DETAIL GRADING
PLAN PLAYGROUND

PROJECT NO. KMLX21003	DRAWING NO. C-153-R.2
DATE 08/28/23	SCALE 1" = 10'
DRAWN BY SDL	CHECKED BY MJM

NOTE: ALL DIMENSIONS AND CONDITIONS SHALL BE VERIFIED BY THE CONTRACTOR AT THE SITE BEFORE PROCEEDING WITH THE WORK.



EXISTING LEGEND	PROPOSED LEGEND
--- EXISTING PROPERTY LINE (APPROXIMATE)	▭ PROPOSED BUILDING
- - - EXISTING RIGHT OF WAY LINE (APPROXIMATE)	▭ PROPOSED DOOR
▭ EXISTING BUILDING	▭ PROPOSED CONCRETE PAVEMENT
▭ EXISTING CURB	▭ PROPOSED ASPHALT PAVEMENT
▭ EXISTING SIDEWALK	▭ PROPOSED LANDSCAPED AREA
▭ EXISTING EDGE OF MACADAM/GRAVEL	▭ PROPOSED UNIT PAVERS
▭ EXISTING TRAFFIC MARKING	▭ PROPOSED POROUS PLAYGROUND SAFETY SURFACE (DIC)
▭ EXISTING FENCE	▭ PROPOSED SYNTHETIC TURF FIELD
▭ EXISTING TREE	▭ PROPOSED ADA RAMP
▭ EXISTING MAJOR CONTOUR	▭ PROPOSED CURB
▭ EXISTING MINOR CONTOUR	▭ PROPOSED DEPRESSED CURB
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▭ EXISTING BOLLARD	▭ PROPOSED STOP BAR/PAVEMENT MARKING
▭ EXISTING UTILITY POLE	▭ PROPOSED FENCE
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▭ EXISTING INLET	▭ PROPOSED MAJOR CONTOUR
▭ EXISTING SEWER	▭ PROPOSED MINOR CONTOUR
▭ EXISTING STORM SEWER	▭ PROPOSED SPOT ELEV.
▭ EXISTING WATER LINE	▭ PROPOSED LOW POINT ELEV.
▭ EXISTING UNDERGROUND TELEPHONE LINE	▭ PROPOSED TOP OF CURB ELEV.
▭ EXISTING GAS LINE	▭ PROPOSED BOTTOM OF CURB ELEV.
▭ EXISTING UNDERGROUND ELECTRIC	
▭ EXISTING OVERHEAD WIRES	
⊙ ⊙ ⊙ ⊙ EXISTING UTILITY STRUCTURES	



CALL BEFORE YOU DIG
BEFORE YOU DIG ANYWHERE IN PENNSYLVANIA
CALL 1-800-242-1776
PA ACT 287 OF 1974 REQUIRES THREE WORKING DAYS
NOTICE TO UTILITIES BEFORE YOU EXCAVATE, DRILL OR
BLAST PENNSYLVANIA ONE-CALL SYSTEM, INC.
SERIAL NUMBER(S): 20212583952

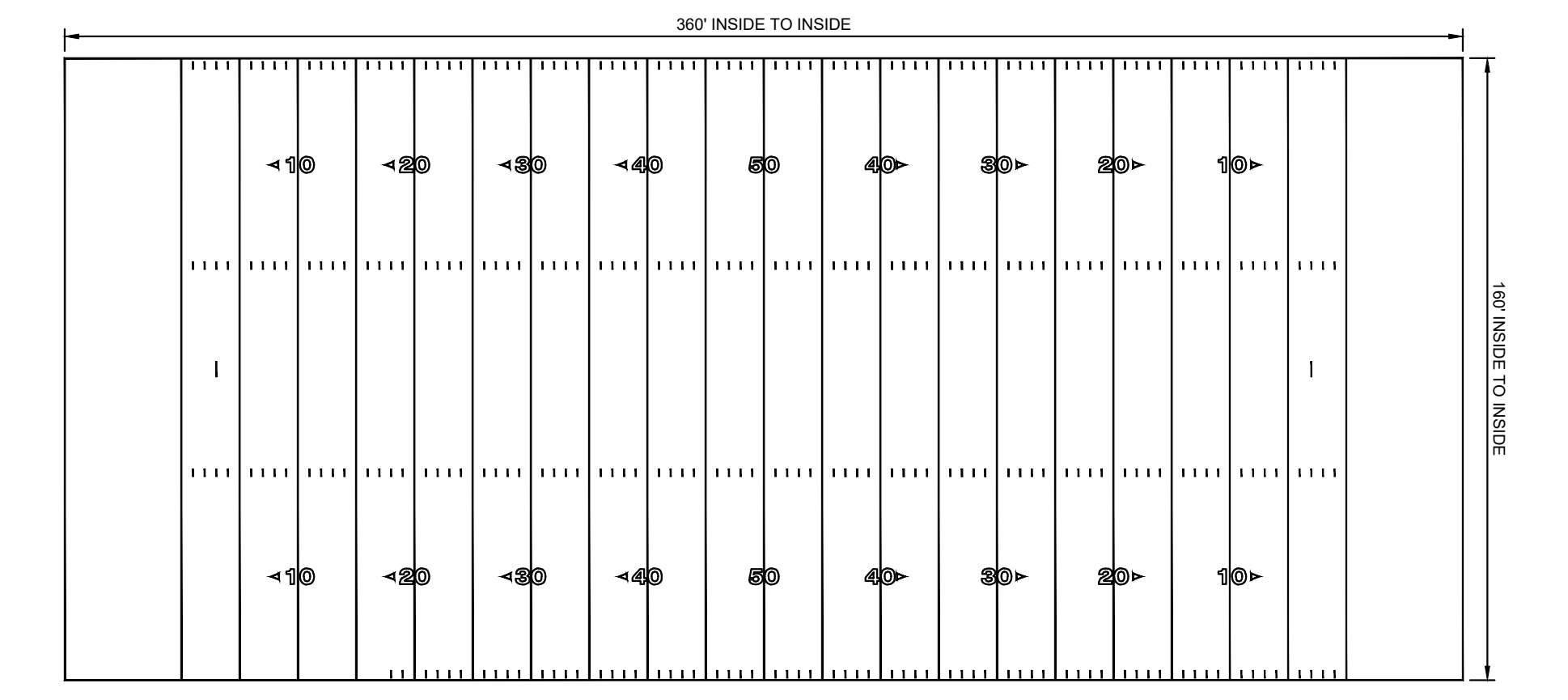
PACKAGE 2 - IFB
NOT FOR CONSTRUCTION
08/28/23
NPDES PERMIT #PAC510302
PWD TRACKING #FY22-KING-6800-01

GRADING PLAN DETAIL - PLAYGROUND
SCALE: 1"=10'-0"

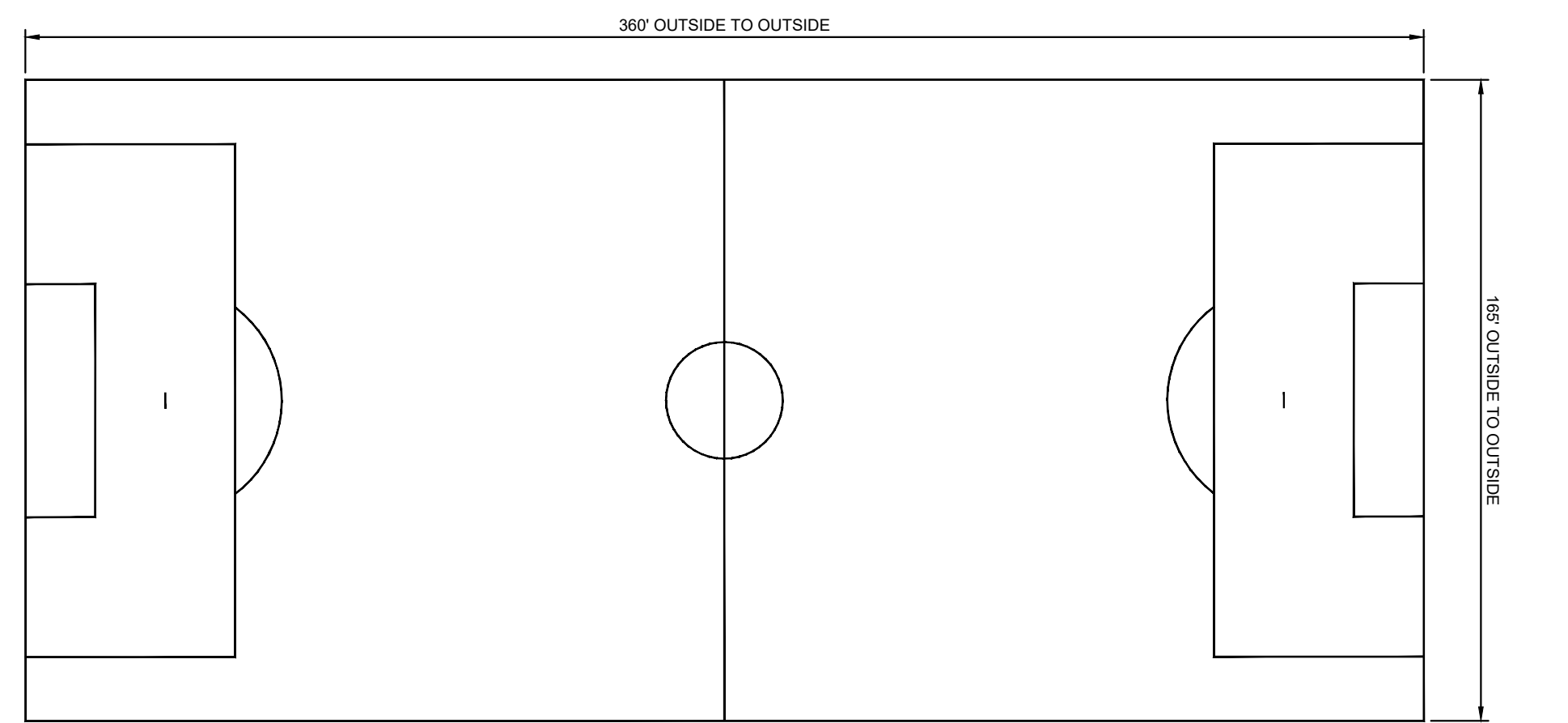




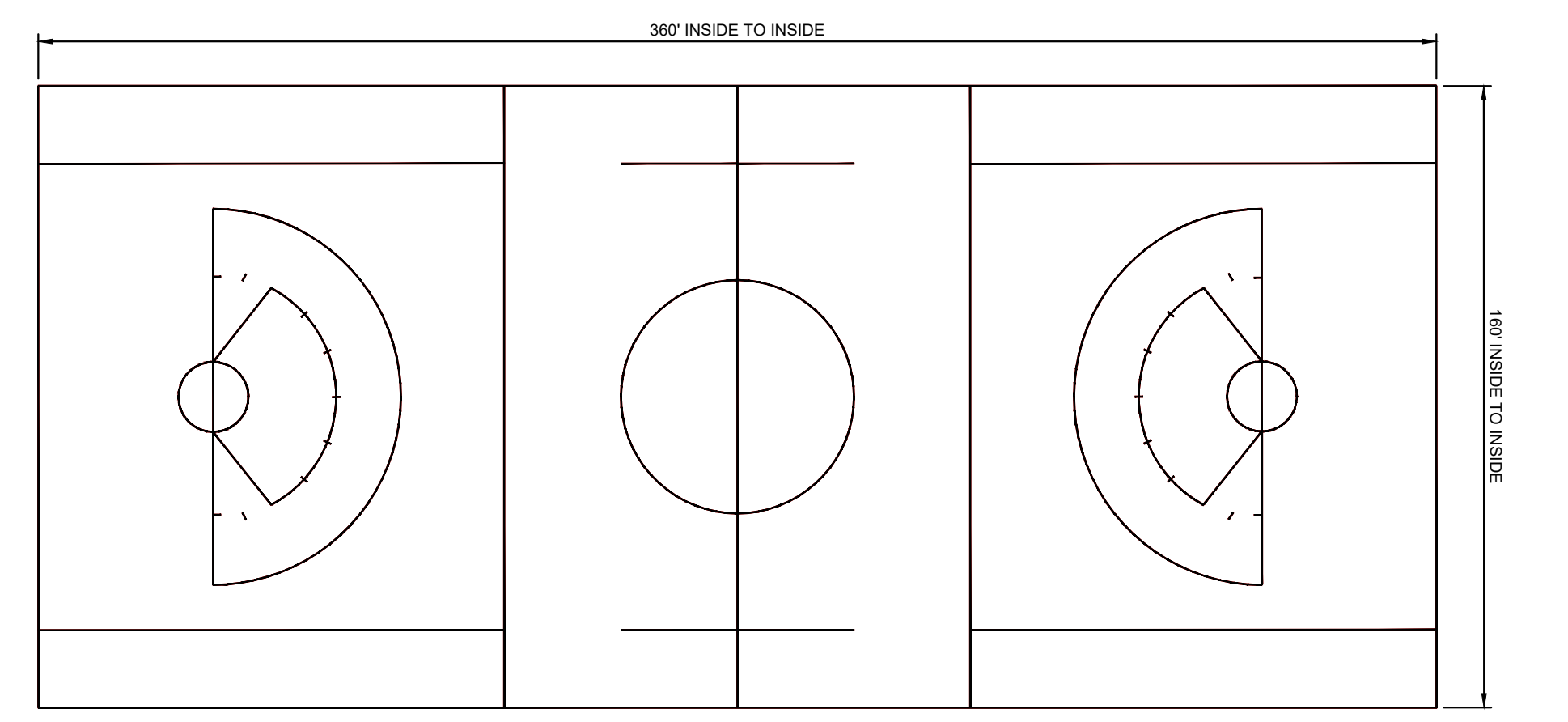
ATHLETIC EVENT GRAPHICS - STADIUM FIELD
SYNTHETIC TURF ATHLETIC FIELD GRAPHICS:
 THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PERMANENT COLORED TURF AND/OR INLAY OF ANY AND ALL PIAA REGULATION AND LINES AFFILIATED WITH FOOTBALL, SOCCER, LACROSSE, BASEBALL, AND SOFTBALL EVENTS. THE CONTRACTOR SHALL SUBMIT A LAYOUT PLAN OF ALL LINES AND GRAPHICS TO BE INCLUDED TO THE LANDSCAPE ARCHITECT FOR APPROVAL PRIOR TO FINALIZING SYNTHETIC TURF MANUFACTURING.
 FOOTBALL - INCLUDING ALL FIELD MARKINGS NECESSARY FOR PERFORMANCE OF SPORT. (160' X 360') COLOR: WHITE
 SOCCER - INCLUDING ALL FIELD MARKINGS NECESSARY FOR PERFORMANCE OF SPORT. (225' X 360') COLOR: YELLOW
 LACROSSE - INCLUDING ALL FIELD MARKINGS NECESSARY FOR PERFORMANCE OF SPORT. (160' X 360') COLOR: RED
 BASEBALL/SOFTBALL - INCLUDING ALL FIELD MARKINGS NECESSARY FOR PERFORMANCE OF SPORT. COLOR: CAROLINA BLUE
FIELD LINE GRAPHICS - LINE SEQUENCE:
 THE FOLLOWING COLOR SEQUENCE DELINEATES THE ORDER (HIGHEST PRIORITY TO LOWEST PRIORITY) IN WHICH THE INSTALLATION OF THE FIELD LINES SHALL GRAPHICALLY TAKE PRECEDENCE.
 1. WHITE FIELD LINES
 2. YELLOW FIELD LINES
 3. RED FIELD LINES
 4. CAROLINA BLUE FIELD LINES
NOTES:
 A. ALL FIELD LAYOUT DIMENSIONS SHALL CONFORM TO THE NATIONAL FEDERATION OF STATE HIGH SCHOOL ASSOCIATIONS.
 B. NO FIELD DELINEATION DOTS SHALL BE OBLITERATED BY OTHER FIELD GRAPHICS.
 C. CONTRACTOR SHALL SUBMIT DETAILED SHOP DRAWINGS FOR APPROVAL PRIOR TO FIELD INSTALLATION.



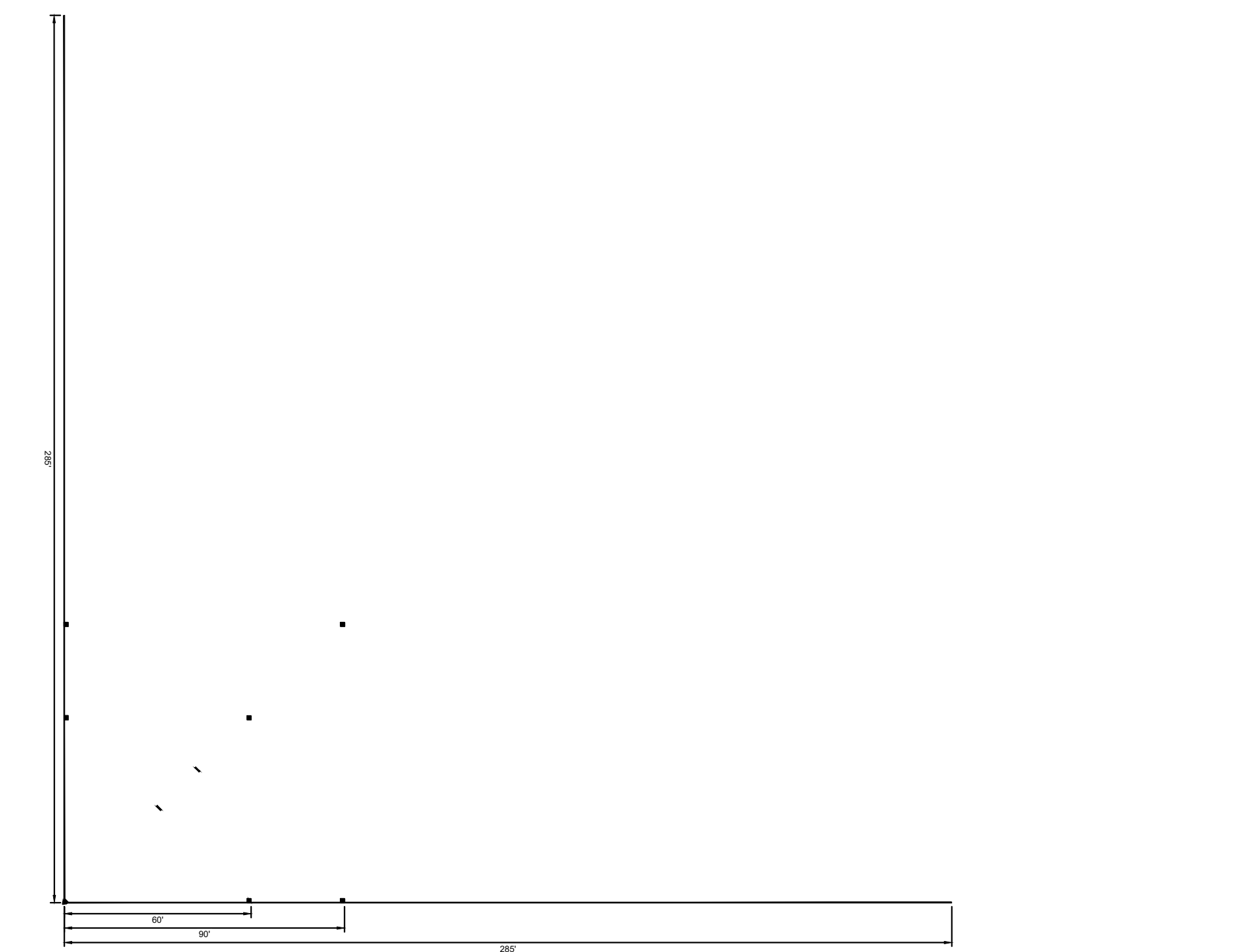
FOOTBALL LINES - WHITE
 NOT TO SCALE



SOCCER LINES - YELLOW
 NOT TO SCALE



LACROSSE LINES - BOY/GIRL UNIFIED - RED
 NOT TO SCALE



BASEBALL AND SOFTBALL - CAROLINA BLUE
 NOT TO SCALE

REVISIONS

ISSUE	DATE	DESCRIPTION
1	01/09/23	PWD PCSM/NPDES SUBMISSION
2	02/14/23	PWD PCSM RESUBMISSION
3	05/24/23	PWD PCSM RESUBMISSION



REVIEWED BY:

PROJECT COORDINATOR:

REALS:



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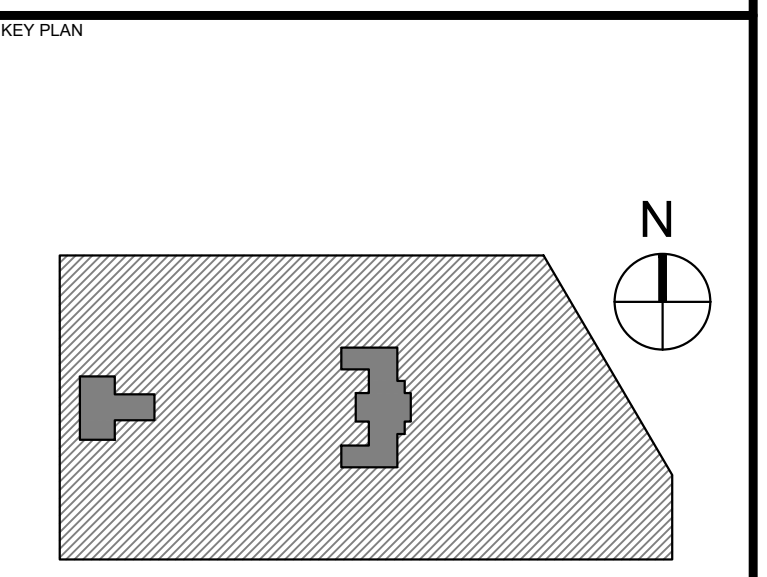
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CITY OF PHILADELPHIA
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 11TH FLOOR, ONE PARKWAY BUILDING
 PHILADELPHIA PENNSYLVANIA

PROJECT TITLE
KINGSESSING RECREATION CENTER



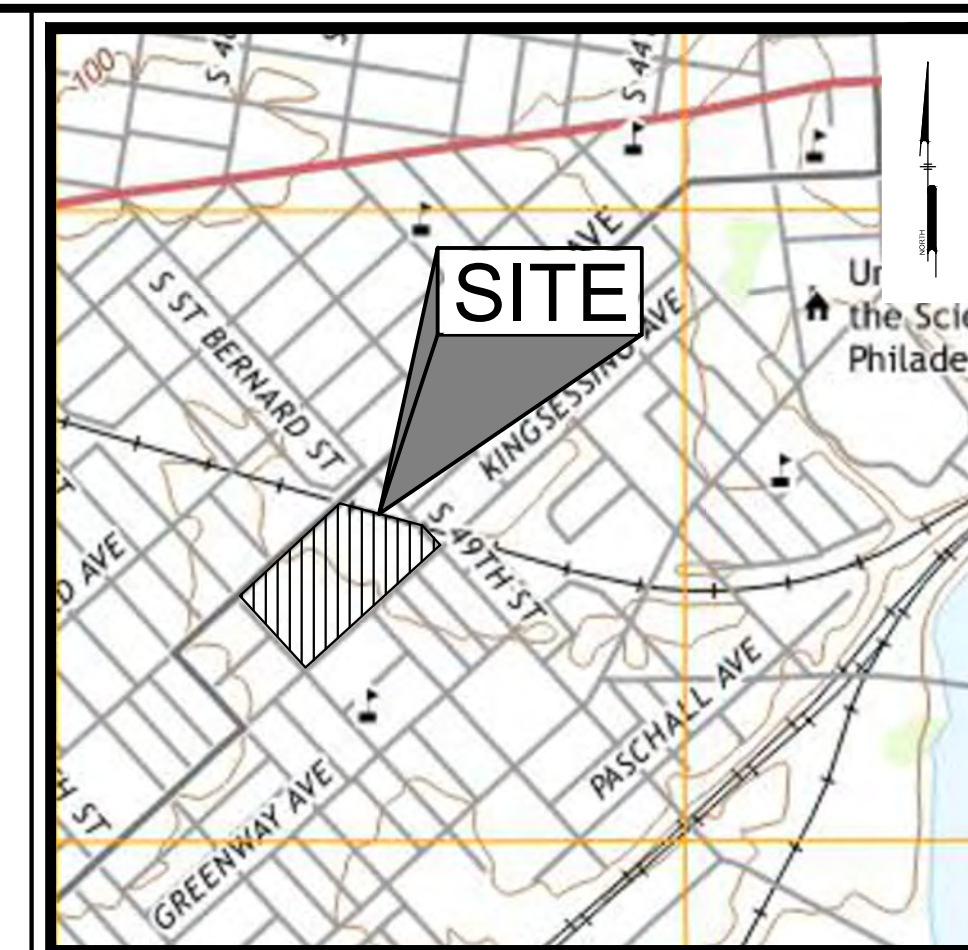
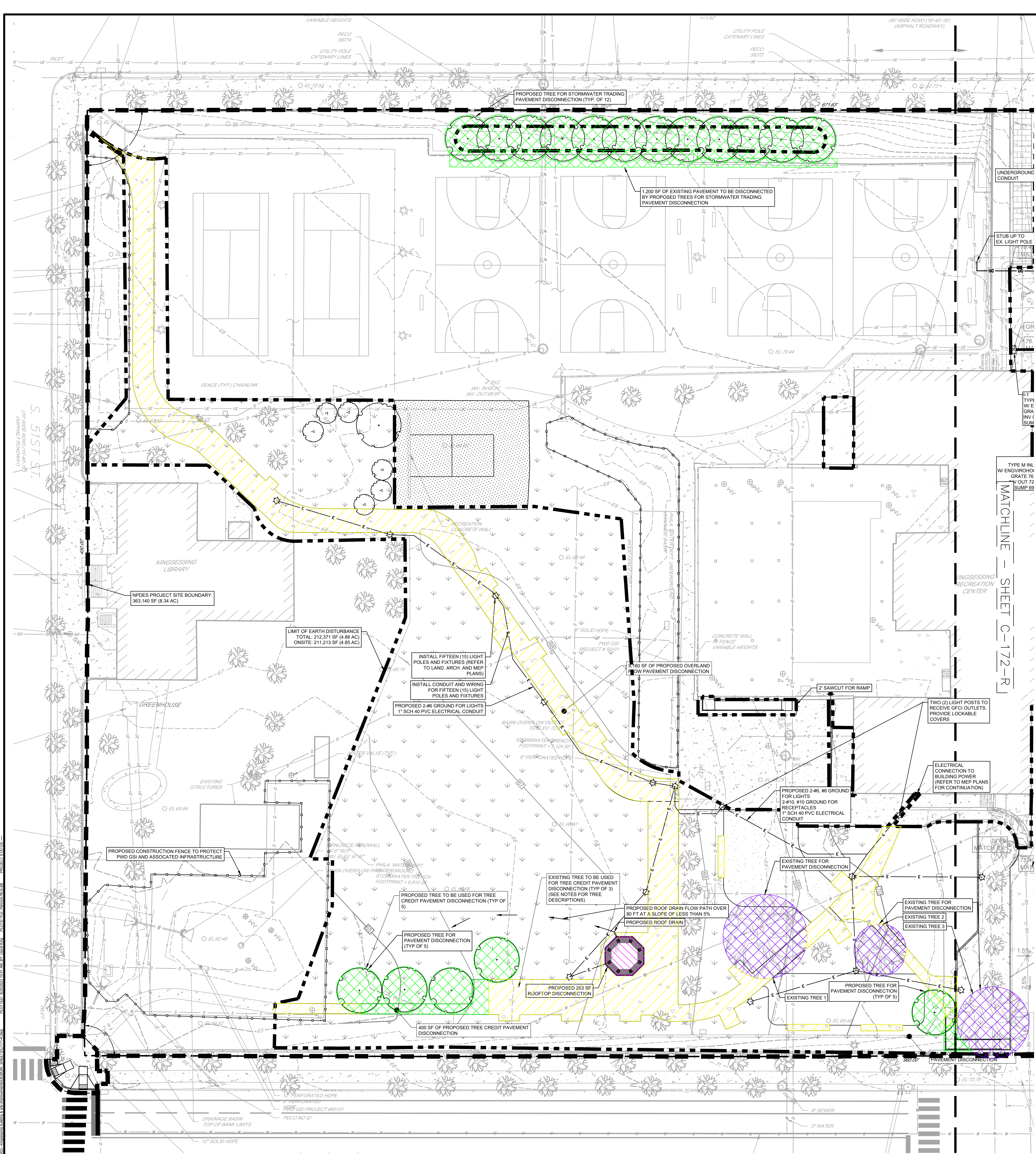
DRAWING TITLE
SYNTHETIC TURF FIELD SPORTS STRIPING PLAN

PROJECT NO. KLMLX21003	DRAWING NO. CS154-R.2
DATE 08/28/23	
SCALE 1" = 20'	
DRAWN BY SDL	
CHECKED BY MJM	

NOTE: ALL DIMENSIONS AND CONDITIONS SHALL BE VERIFIED BY THE CONTRACTOR AT THE SITE BEFORE PROCEEDING WITH THE WORK.

SITE PLAN DETAIL
 SCALE: 1"=20'-0"

PACKAGE 2 - IFB
NOT FOR CONSTRUCTION
 08/28/23
 NPDES PERMIT #PAC510302
 PWD TRACKING #FY22-KING-6800-01



LOCATION MAP
Scale: 1" = 100'
WATERSHED: LOWER SCHUYLKILL RIVER
FLOOD MANAGEMENT DISTRICT: A
FEMA FLOODPLAIN: ZONE X

REVISIONS		
ISSUE	DATE	DESCRIPTION
1	01/09/23	PWD PCSM/NPDES SUBMISSION
2	02/14/23	PWD PCSM REVISION
3	05/24/23	PWD PCSM REVISION



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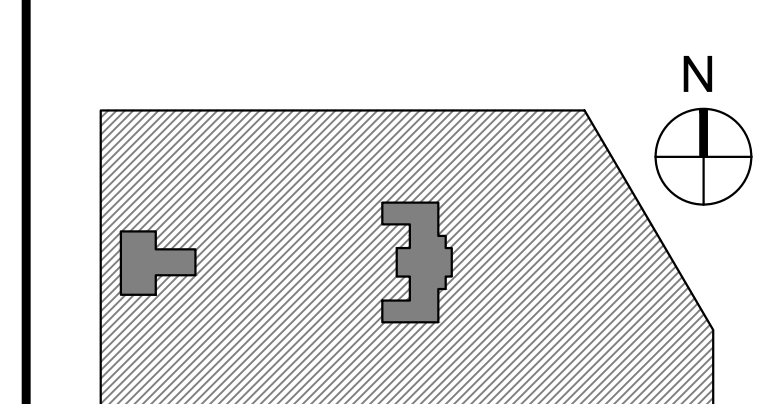
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DEPARTMENT OF PUBLIC PROPERTY
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11TH FLOOR, ONE PARKWAY BUILDING
PHILADELPHIA, PENNSYLVANIA

PROJECT TITLE
KINGSESSING RECREATION CENTER

KEY PLAN



DRAWING TITLE
PCSM/UTILITY PLAN SOUTH

PROJECT NO. KMLX21003	DRAWING NO. C-171-R.2
DATE 08/28/23	SCALE 1" = 20'
DRAWN BY SDL	CHECKED BY MJM

- UTILITY NOTES:**
- ALL PIPE LENGTHS AND DISTANCES BETWEEN STRUCTURES ARE MEASURED FROM CENTER OF STRUCTURE TO CENTER OF STRUCTURE ALONG A HORIZONTAL PLANE.
 - ALL STORM DRAINAGE PIPE SHALL BE LAID ON SMOOTH CONTINUOUS GRADES WITH NO VISIBLE BENDS AT JOINTS.
 - BEDDING REQUIREMENTS SPECIFIED IN THE CONSTRUCTION DOCUMENTS ARE TO BE CONSIDERED AS MINIMUMS FOR RELATIVELY DRY, STABLE PERMITS CONDITIONS. ADDITIONAL BEDDING SHALL BE REQUIRED FOR ROCK TRENCHES AND WET AREAS. CONTRACTOR SHALL HAVE THE RESPONSIBILITY TO PROVIDE SUCH ADDITIONAL BEDDING AS MAY BE REQUIRED TO PROPERLY CONSTRUCT THE WORK.
 - COMPACTION OF THE BACKFILL OF ALL TRENCHES SHALL BE COMPACTED TO THE DENSITY OF 95% OF THEORETICAL MAXIMUM DRY DENSITY (ASTM D998). BACKFILL MATERIAL SHALL BE FREE FROM ROOTS, STUMPS, OR OTHER FOREIGN DEBRIS AND SHALL BE PLACED IN LIFTS NOT TO EXCEED 6 INCHES IN COMPACTED FILL THICKNESS.
 - THE CONTRACTOR WILL INSURE THAT POSITIVE AND ADEQUATE DRAINAGE IS MAINTAINED AT ALL TIMES WITHIN THE PROJECT LIMITS. THIS MAY INCLUDE, BUT NOT BE LIMITED TO, REPLACEMENT OR RECONSTRUCTION OF EXISTING DRAINAGE STRUCTURES THAT HAVE BEEN DAMAGED OR REMOVED OR REGRADING AS REQUIRED BY THE ENGINEER, EXCEPT FOR THOSE DRAINAGE ITEMS SHOWN AT SPECIFIC LOCATIONS AND HAVING SPECIFIC PAY ITEMS IN THE DETAILED ESTIMATE. NO SEPARATE PAYMENT WILL BE MADE FOR ANY COSTS INCURRED TO COMPLY WITH THIS REQUIREMENT.
 - EXISTING UTILITY LATERALS FOR THE PREVIOUS USE ARE NOT TO BE REUSED.
 - CONTRACTOR MUST APPLY FOR ALL UTILITY CONNECTION APPLICATIONS.
 - CONTRACTOR IS RESPONSIBLE FOR ALL UTILITY CONNECTION FEES.
 - UTILITY CONNECTION AND UTILITY COMPANY DETAILS FOR RECONNECTION AND NEW SERVICE WERE NOT PROVIDED BY THE UTILITY COMPANIES. CONTRACTOR MUST OBTAIN ANY UTILITY DETAILS FOR RECONNECTION OF EXISTING SERVICES OR NEW SERVICE AND IS RESPONSIBLE FOR THE CONSTRUCTION OF EACH NEW SERVICE FOR THE APPROPRIATE UTILITY COMPANY'S SPECIFICATIONS.
 - IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO DETERMINE THE LOCATION OF ALL EXISTING UNDERGROUND UTILITIES AND TO TAKE WHATEVER STEPS NECESSARY TO PROVIDE FOR THEIR PROTECTION. THE ENGINEER HAS DILIGENTLY ATTEMPTED TO LOCATE AND INDICATE ALL EXISTING FACILITIES ON THESE PLANS. HOWEVER, THIS INFORMATION IS SHOWN FOR THE CONTRACTOR'S CONVENIENCE ONLY. THE ENGINEER ASSUMES NO RESPONSIBILITY FOR THE LOCATIONS OF UTILITIES SHOWN OR NOT SHOWN. THE CONTRACTOR SHALL CONTACT THE UTILITY COMPANIES FOR EXACT LOCATIONS OF THEIR UTILITIES PRIOR TO STARTING CONSTRUCTION. IT SHALL BE THE SOLE RESPONSIBILITY OF

- GREEN STORMWATER INFRASTRUCTURE NOTES**
- THE CONTRACTOR PERFORMING THE GREEN STORMWATER INFRASTRUCTURE INSTALLATION MUST BE PREQUALIFIED BY THE PHILADELPHIA WATER DEPARTMENT (PWD) CONTACT MS. TRISHA GRACE OF THE PROJECTS MANAGEMENT UNIT AT (215) 681-6336.
 - THE APPROVED WORK SHALL BE DONE IN THE PRESENCE OF A PWD INSPECTOR.
 - THE CONTRACTOR PERFORMING THIS WORK IS TO NOTIFY THE PWD CONSTRUCTION DIVISION 1101 MARKET STREET, 2ND FLOOR, PHONE (215) 681-6345, AT LEAST 7 DAYS IN ADVANCE FOR ASSIGNMENT OF AN INSPECTOR TO THE JOB.
 - THE CONTRACTOR MUST PROVIDE OFFICE SPACE FOR USE BY THE PWD INSPECTOR DURING CONSTRUCTION.
 - APPROVAL OF THESE PLANS BY THE WATER DEPARTMENT IS STRICTLY LIMITED TO THE DESIGN OF GREEN STORMWATER INFRASTRUCTURE SHOWN WITHIN THE LIMITS OF THE CITY OF PHILADELPHIA PUBLIC RIGHT OF WAY.
 - CONTACT PWD-WATER TRANSPORTATION RECORDS, 1101 MARKET STREET, 2ND FLOOR, PHONE (215) 681-6321, FOR ADDITIONAL APPROVAL AND PERMITS REQUIRED FOR ALL SEWER CONNECTIONS TO EXISTING AND/OR PROPOSED PWD FACILITIES.
 - FIELD-FABRICATED WYE BRANCHES AND BENDS ARE NOT PERMITTED.
 - A LIST OF ALL MATERIALS AND SUPPLIERS MUST BE SUBMITTED TO THE PWD CONSTRUCTION BRANCH FOR APPROVAL.
 - THE CONTRACTOR OR ENGINEER IS RESPONSIBLE FOR OBTAINING ALL ADDITIONAL PERMITS AND APPROVALS FROM ALL AFFECTED CITY AGENCIES AND UTILITIES.
 - PRIOR TO OBTAINING A BUILDING PERMIT, THE CONTRACTOR IS REQUIRED TO OBTAIN PWD STORMWATER MANAGEMENT APPROVAL AND SEWAGE FACILITIES PLANNING (ACT 537) APPROVAL.
 - ANY CHANGE TO, OR DEVIATION FROM, THE FINAL APPROVED DESIGN PLANS DURING CONSTRUCTION MUST BE APPROVED BY THE ASSIGNED PWD CONSTRUCTION DIVISION ENGINEER AND BY THE PWD DESIGN PROJECT ENGINEER.
 - PWD RESERVES THE RIGHT TO REQUEST ADDITIONAL BORINGS DURING CONSTRUCTION SHOULD THE SOIL EXCAVATED IN THAT AREA APPEAR TO BE UNSUITABLE.
 - ANY TRENCH WHERE THE CUT IS DEEPER THAN 10 FEET WILL REQUIRE A DETAILED SHORING PLAN PREPARED BY A REGISTERED PROFESSIONAL ENGINEER TO BE SUBMITTED TO PWD FOR APPROVAL BEFORE WORK CAN BEGIN.
 - PLACE AND COMPACT BACKFILL IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS FOR EXCAVATION, REFILLING, GRADING, LANDSCAPING AND REPAIRING.
 - ALL SIDEWALK AND CURBING TO BE REPLACED IN KIND ALONG FULL LIMITS OF CONSTRUCTION TO NEXT EXISTING JOINT OR AS DIRECTED BY PWD.

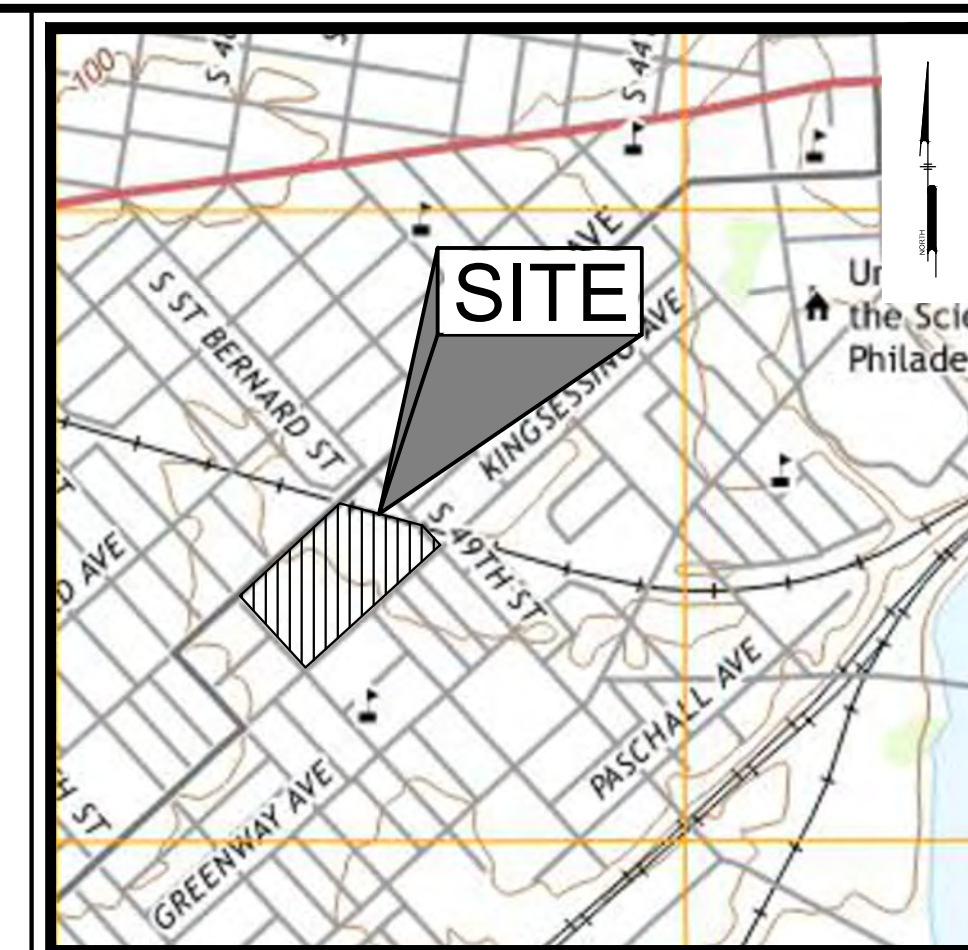
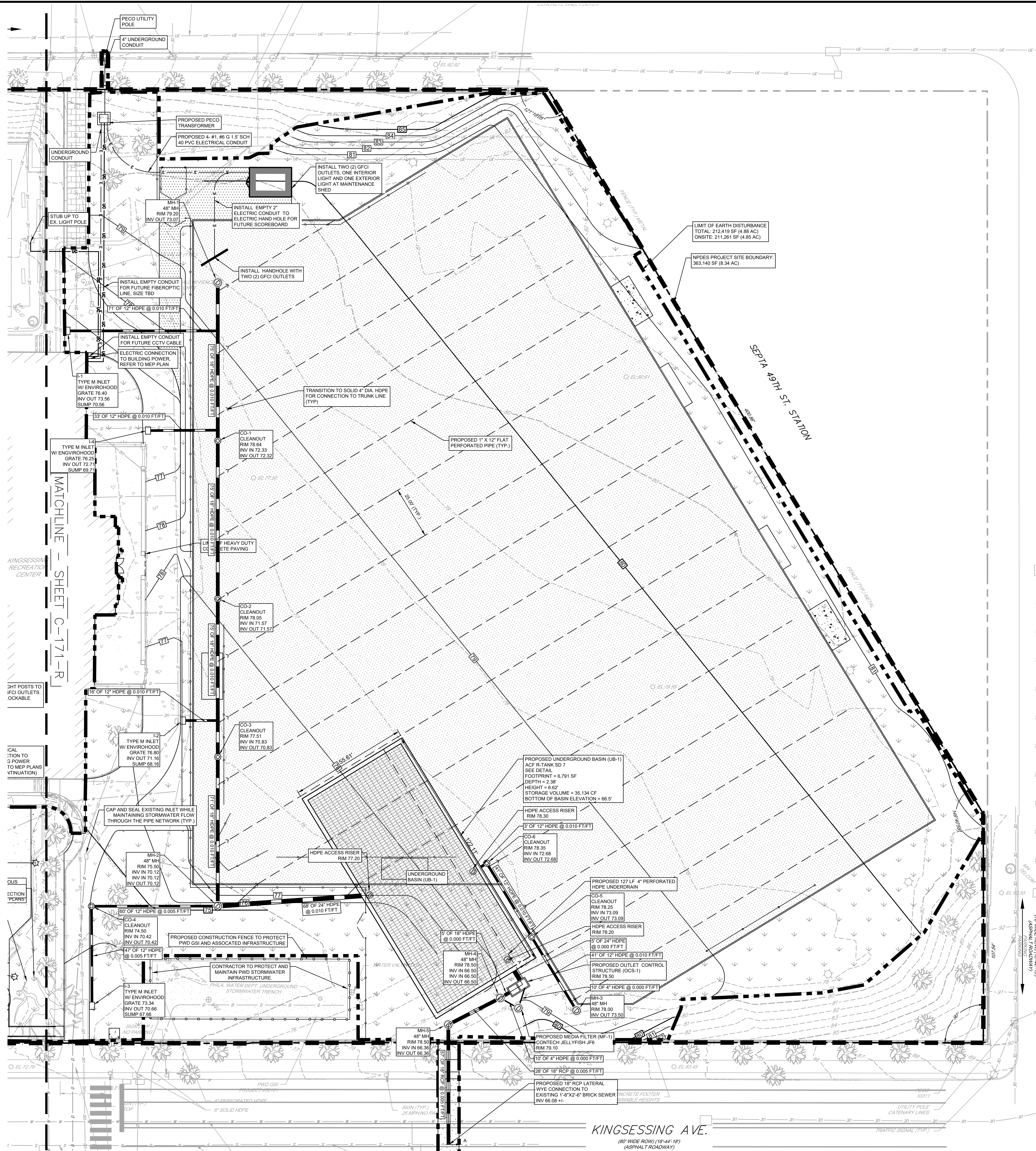
EXISTING LEGEND		PROPOSED LEGEND	
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	EXISTING RIGHT OF WAY LINE (APPROXIMATE)		PROPOSED DOOR
	EXISTING BUILDING		PROPOSED CONCRETE PAVEMENT
	EXISTING CURB		PROPOSED ASPHALT PAVEMENT
	EXISTING SIDEWALK		PROPOSED LANDSCAPED AREA
	EXISTING EDGE OF MACADAM/GRAVEL		PROPOSED UTILITY PAVERS
	EXISTING TRAFFIC MARKING		PROPOSED POROUS PLAYGROUND SAFETY SURFACE (DIC)
	EXISTING FENCE		PROPOSED SYNTHETIC TURF FIELD
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	EXISTING MAJOR CONTOUR		PROPOSED CURB
	EXISTING MINOR CONTOUR		PROPOSED DEPRESSED CURB
	EXISTING SIGN		PROPOSED WALL
	EXISTING BOLLARD		PROPOSED STOP BAR/PAVEMENT MARKING
	EXISTING UTILITY POLE		PROPOSED FENCE
	EXISTING LIGHT		PROPOSED SIGN
	EXISTING INLET		PROPOSED MAJOR CONTOUR
	EXISTING SEWER		PROPOSED MINOR CONTOUR
	EXISTING STORM SEWER		PROPOSED STORM INLET
	EXISTING WATER LINE		PROPOSED STORM MANHOLE
	EXISTING UNDERGROUND TELEPHONE LINE		PROPOSED STORM CLEANOUT
	EXISTING GAS LINE		PROPOSED STORM SEWER
	EXISTING UNDERGROUND ELECTRIC		PROPOSED SANITARY SEWER
	EXISTING OVERHEAD WIRES		PROPOSED WATER LINE
	EXISTING UTILITY STRUCTURES		PROPOSED FIRE LINE
			PROPOSED NATURAL GAS LINE
			PROPOSED CABLE/TELEPHONE LINE
			PROPOSED ELECTRIC LINE
			PROPOSED LIMIT OF DISTURBANCE
			PROPOSED NPDES PROJECT SITE BOUNDARY
			PROPOSED CONSTRUCTION FENCE
			EXISTING TREE DISCONNECTION
			PROPOSED TREE DISCONNECTION
			PAVEMENT DISCONNECTION
			ROOFTOP DISCONNECTION

EXISTING TREES FOR PAVEMENT DISCONNECTION

TREE #	SPECIES	CALIPER DIAMETER (IN)
1	HONEY LOCUST	23.50
2	BLACK GUM	9.70
3	BLACK GUM	15.20

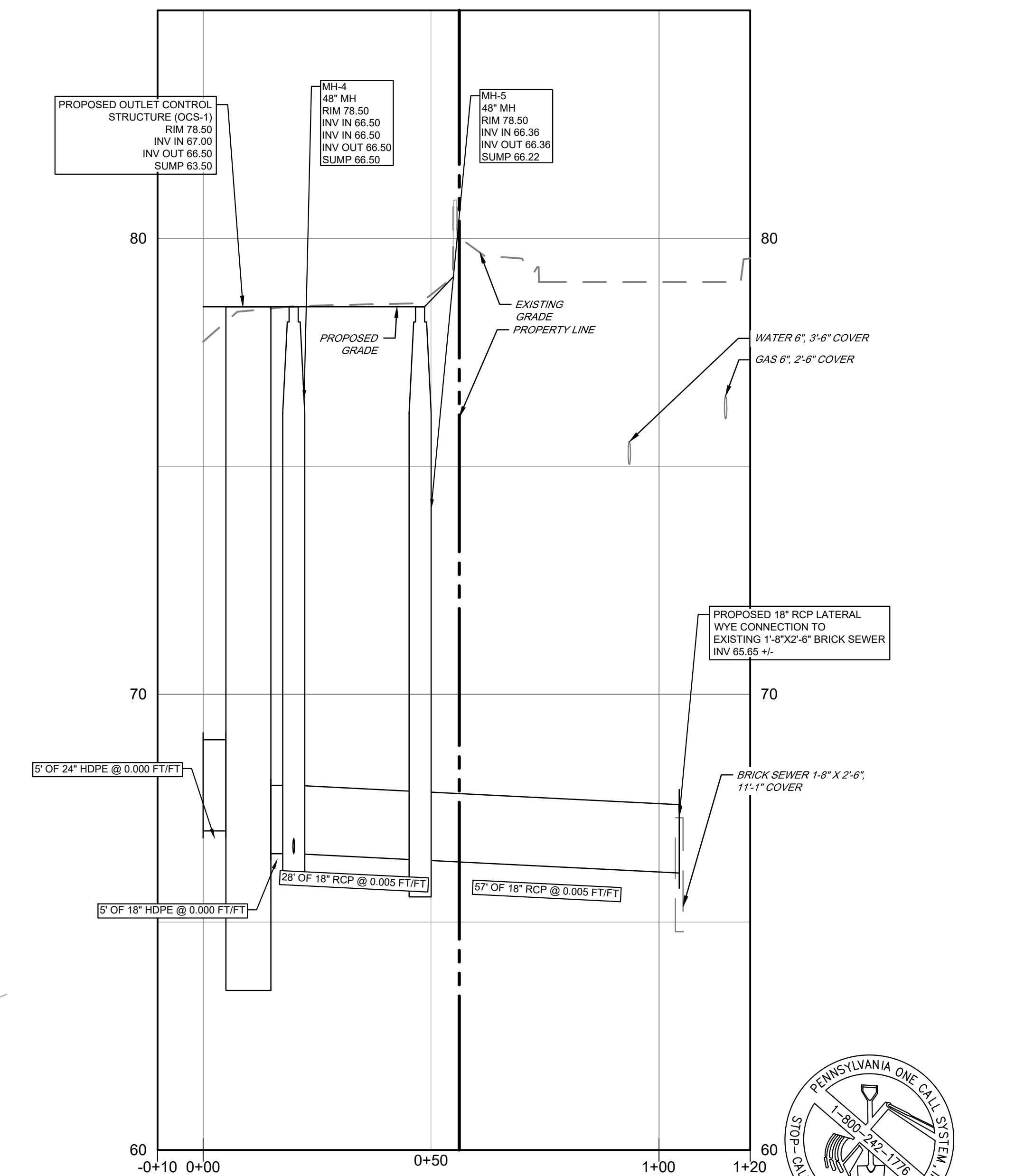
UTILITY PLAN DETAIL - SOUTH
SCALE: 1"=20'-0"

CALL BEFORE YOU DIG
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NPDES PERMIT #PAC510302
PWD TRACKING #FY22-KING-6800-01



LOCATION MAP
Scale: 1" = 100'
WATERSHED: LOWER SCHUYLKILL RIVER
FLOOD MANAGEMENT DISTRICT: A
FEMA FLOODPLAIN: ZONE X

- | | |
|----------------------------------------------|---------------------------------------------------|
| EXISTING LEGEND | PROPOSED LEGEND |
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| ▭ EXISTING UNDERGROUND ELECTRIC | ▭ PROPOSED SANITARY SEWER |
| ▭ EXISTING OVERHEAD WIRES | ▭ PROPOSED WATER LINE |
| ▭ EXISTING UTILITY STRUCTURES | ▭ PROPOSED FIRE LINE |
| | ▭ PROPOSED NATURAL GAS LINE |
| | ▭ PROPOSED CABLE/TELEPHONE LINE |
| | ▭ PROPOSED ELECTRIC LINE |
| | ▭ PROPOSED LIMIT OF DISTURBANCE |
| | ▭ PROPOSED NPDES PROJECT SITE BOUNDARY |
| | ▭ PROPOSED CONSTRUCTION FENCE |



SECTION A-A: UB-1 TO KINGSESSING AVE.
HORIZONTAL SCALE: 1" = 20'
VERTICAL SCALE: 1" = 2'

CALL BEFORE YOU DIG
PENNSYLVANIA ONE-CALL SYSTEM
800-488-7827
OR
BLAST PENNSYLVANIA ONE-CALL SYSTEM, INC.
(610) 202-2585

REVISIONS		
ISSUE	DATE	DESCRIPTION
1	01/09/23	PWD PCSM/NPDES SUBMISSION
2	02/14/23	PWD PCSM RESUBMISSION
3	05/24/23	PWD PCSM RESUBMISSION

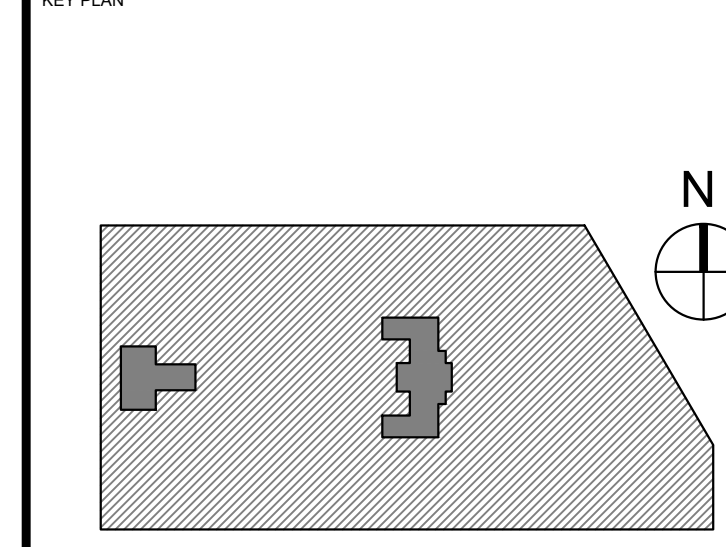


REVIEWED BY:
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KELLY MAIELLO ARCHITECTS
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CITY OF PHILADELPHIA
DEPARTMENT OF PUBLIC PROPERTY
1515 ARCH STREET
PHILADELPHIA, PENNSYLVANIA

PROJECT TITLE
KINGSESSING RECREATION CENTER

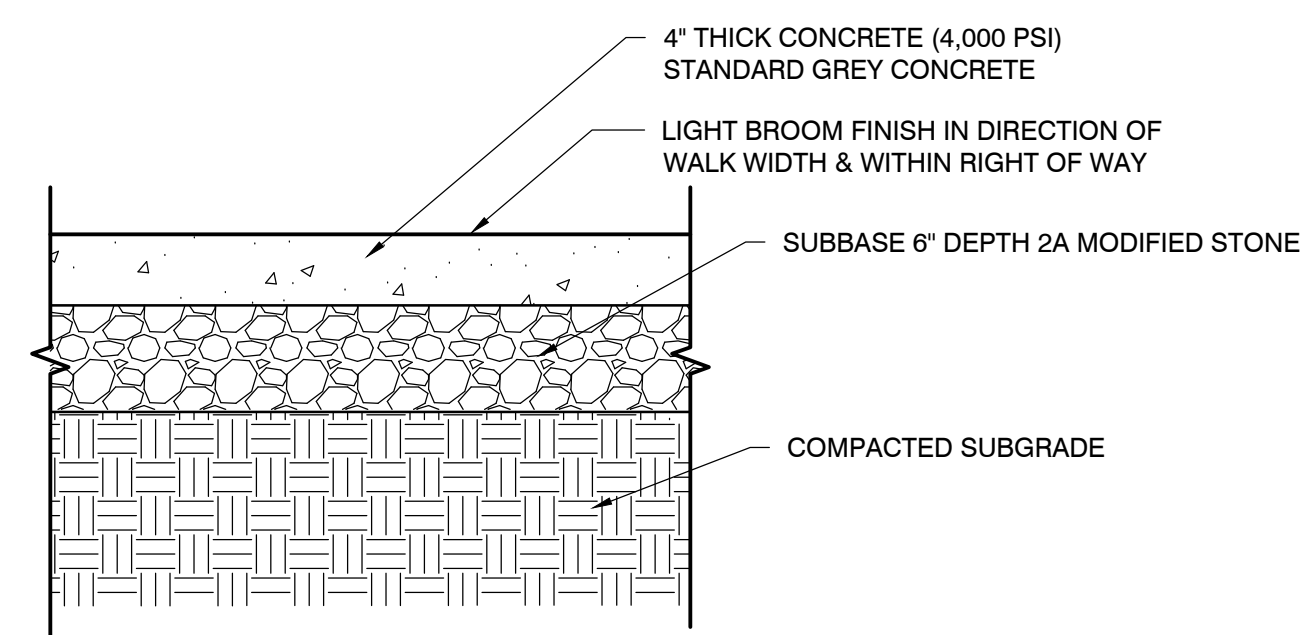
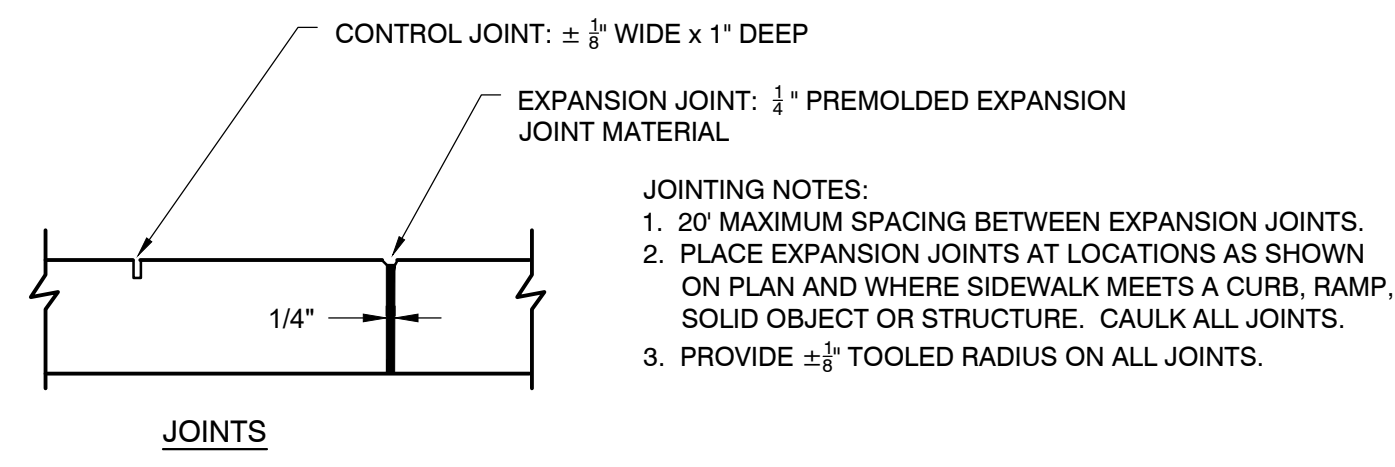


DRAWING TITLE
PCSM / UTILITY PLAN NORTH

PROJECT NO. KLMLX21003	DRAWING NO. C-172-R.2
DATE 08/28/23	SCALE 1" = 20'
DRAWN BY SDL	CHECKED BY MJM

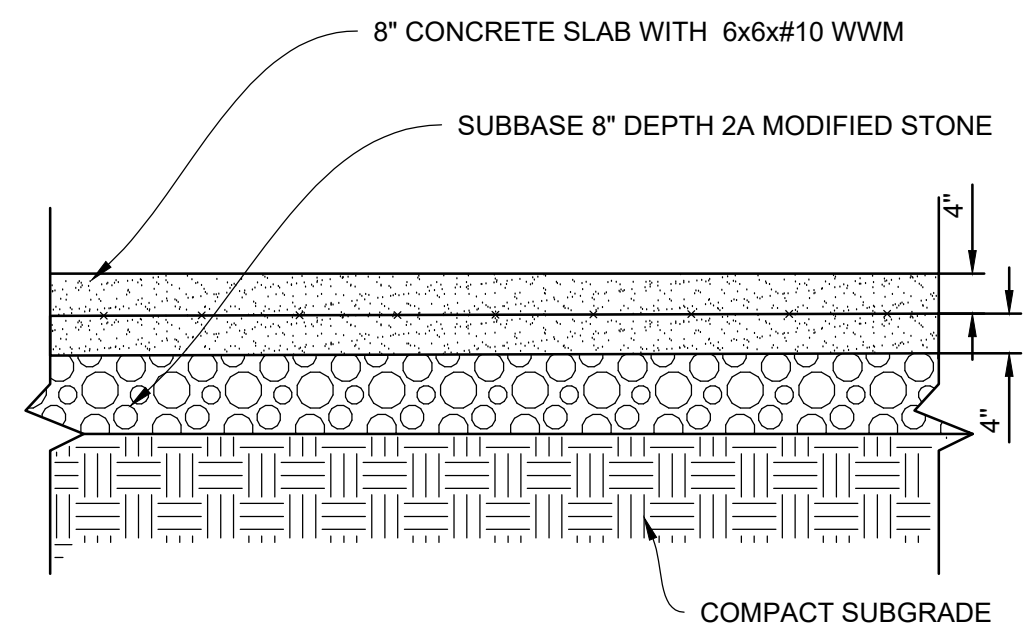
NPDES PERMIT #PAC510302
PWD TRACKING #FY22-KING-6800-01

UTILITY PLAN DETAIL - NORTH
SCALE: 1"=20'-0"



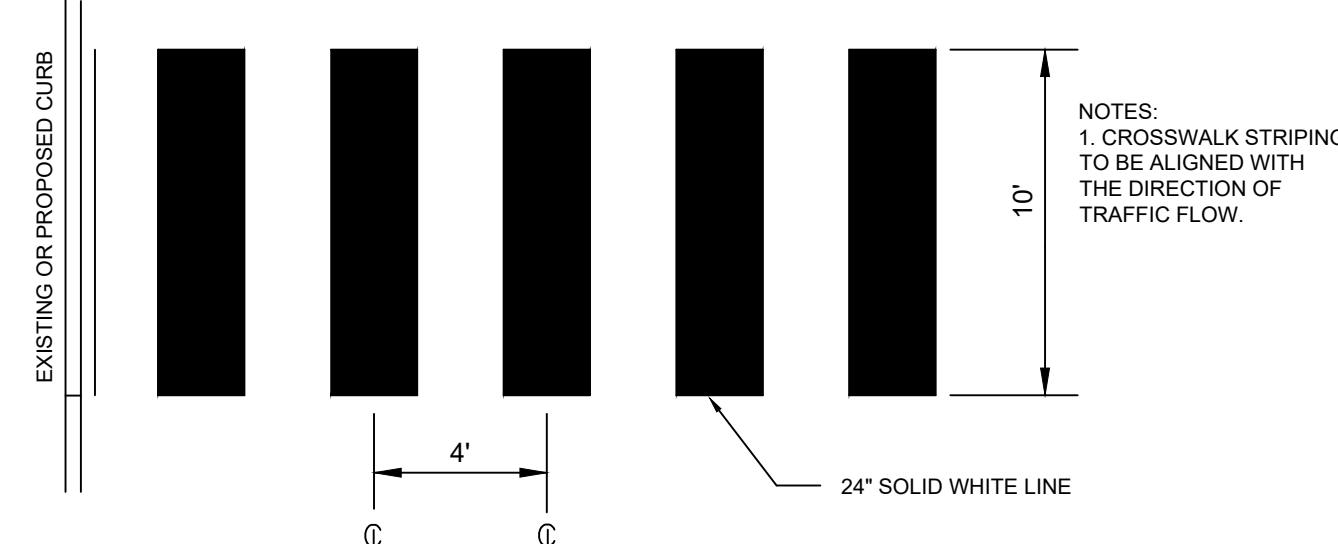
SIDEWALK NOTES:
1. ALL CONCRETE SHALL BE 4,000 PSI AT 28 DAYS, AIR ENTRAINED, W/C RATIO 0.45, MAX. SLUMP 3".
2. COMPACTED SUBGRADE AS DIRECTED BY GEOTECHNICAL ENGINEER.

DETAIL- SIDEWALK AND JOINT
N.T.S.

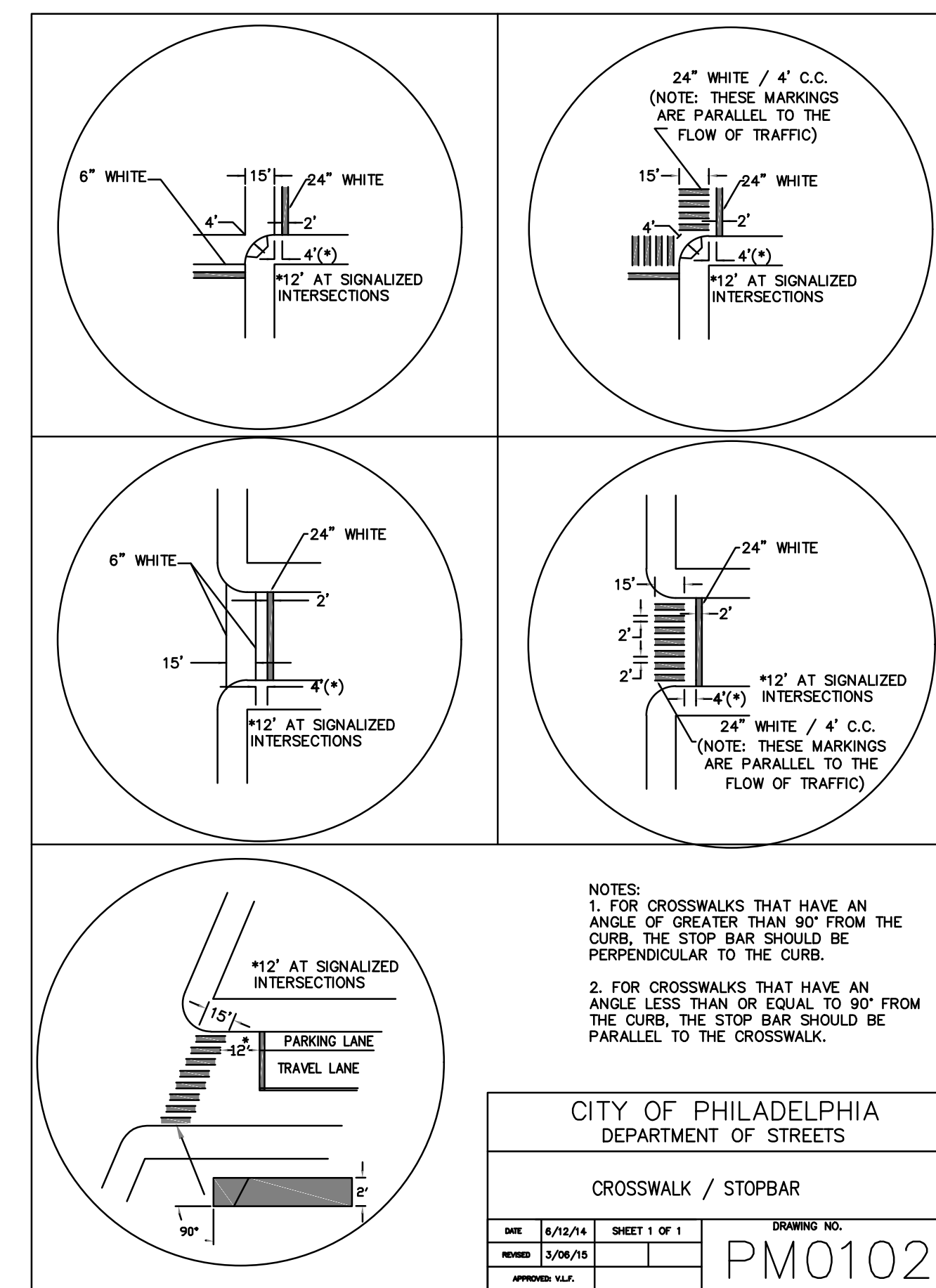


NOTE:
EXPANSION JOINTS SHALL BE PROVIDED AT EQUAL DISTANCES OF NOT MORE THAN 20' AND AT BUILDING WALL. JOINTS SHALL BE FILLED WITH PREFORMED EXPANSION JOINT FILLER. 1/2" THICK CONFORMING TO PENNSYLVANIA DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION. THE JOINT SHALL BE RECESSED 1/4" FROM THE TOP AND FRONT OF CONCRETE SLAB.
NOTE: CONCRETE SHALL BE 4,000 PSI AT 28 DAYS

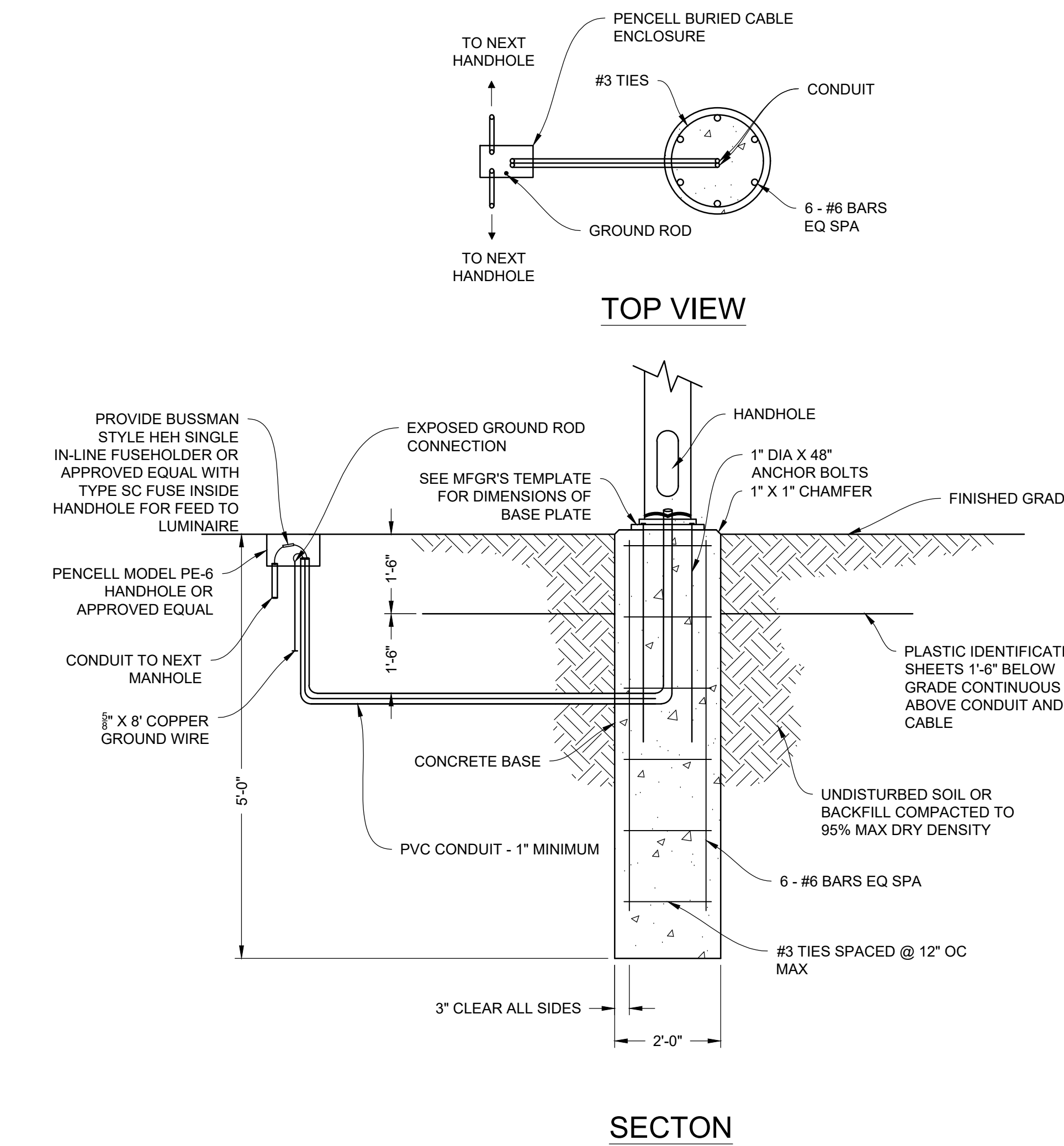
DETAIL- HEAVY DUTY CONCRETE PAVING SECTION (VEHICLE RATED)
N.T.S.



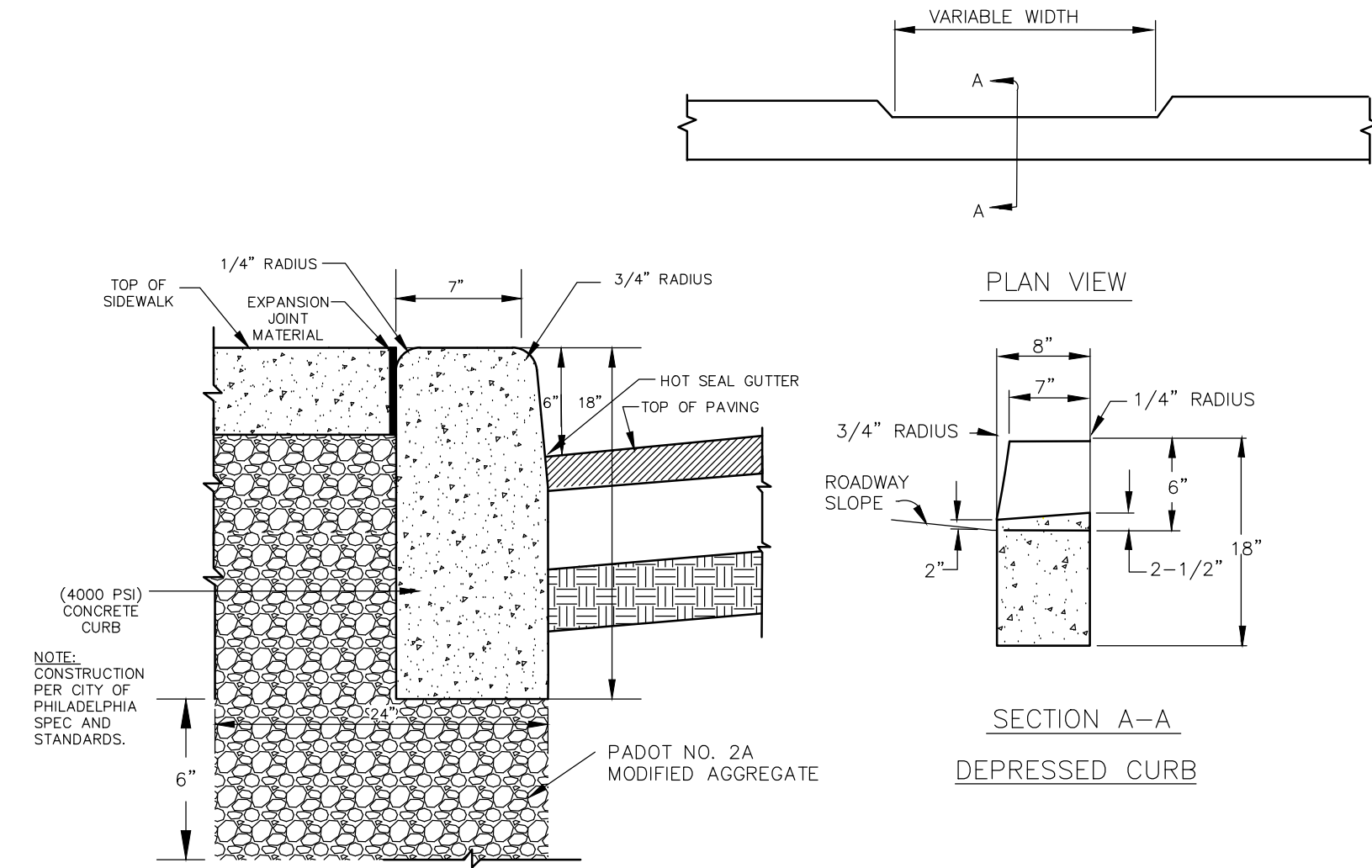
DETAIL- CROSSWALK STRIPING DETAIL
N.T.S.



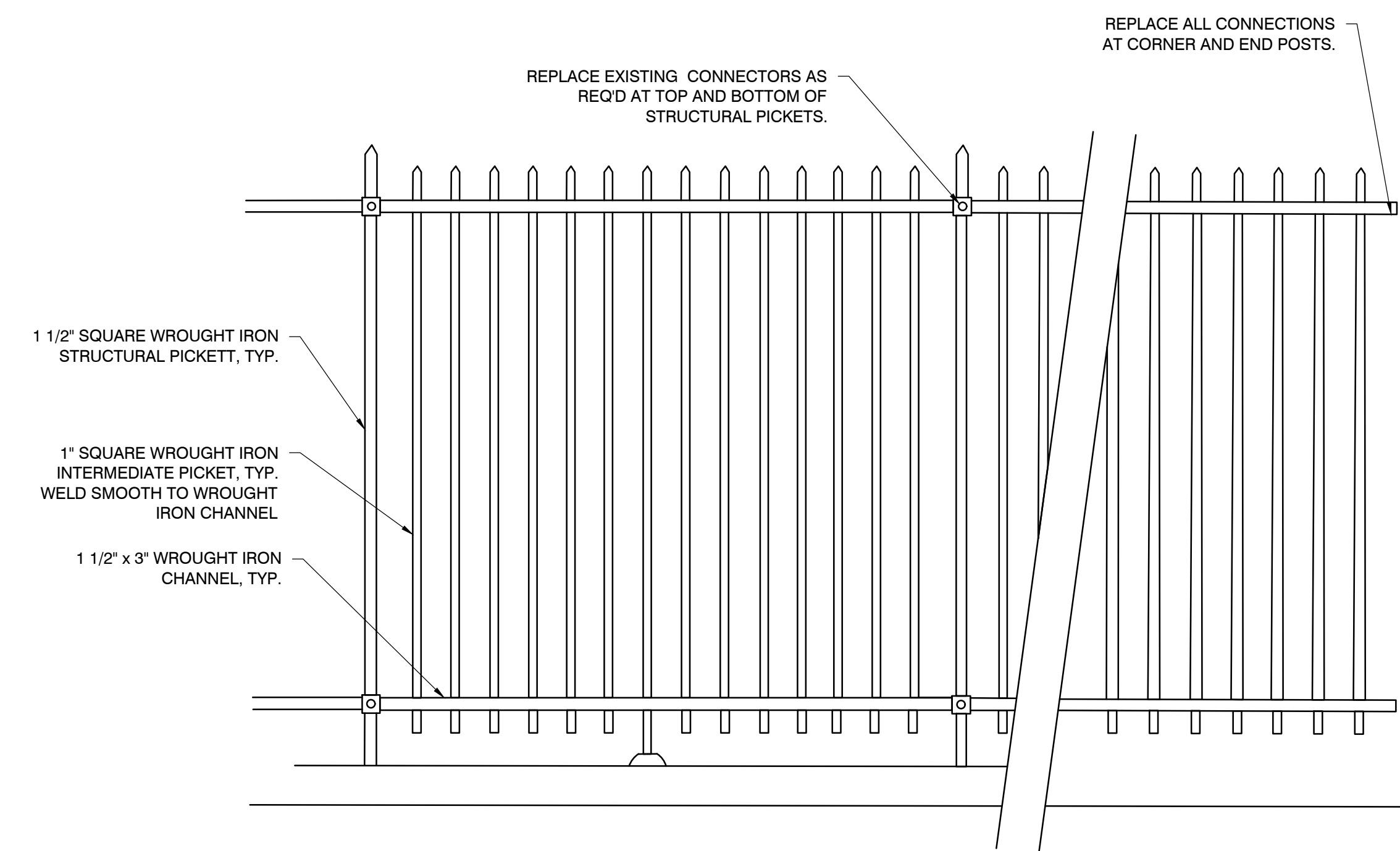
DETAIL- CITY OF PHILADELPHIA - CROSSWALK / STOPBAR (PM0102)
N.T.S.



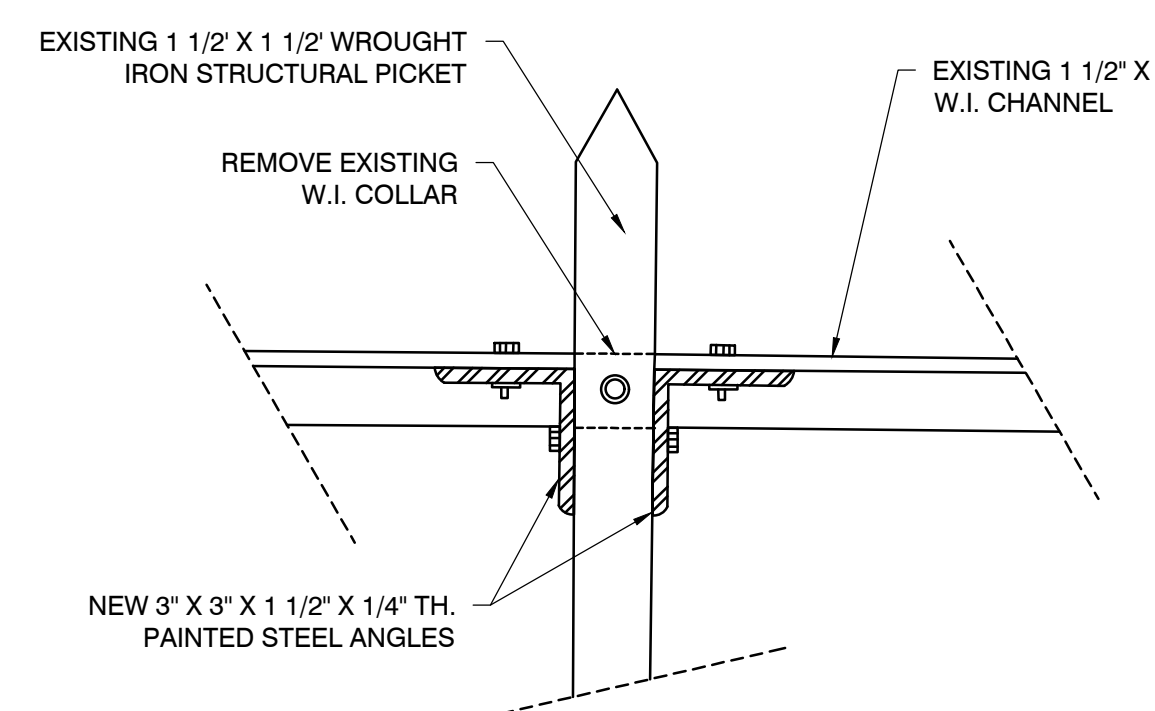
DETAIL- LIGHT POLE BASE DETAIL
N.T.S.



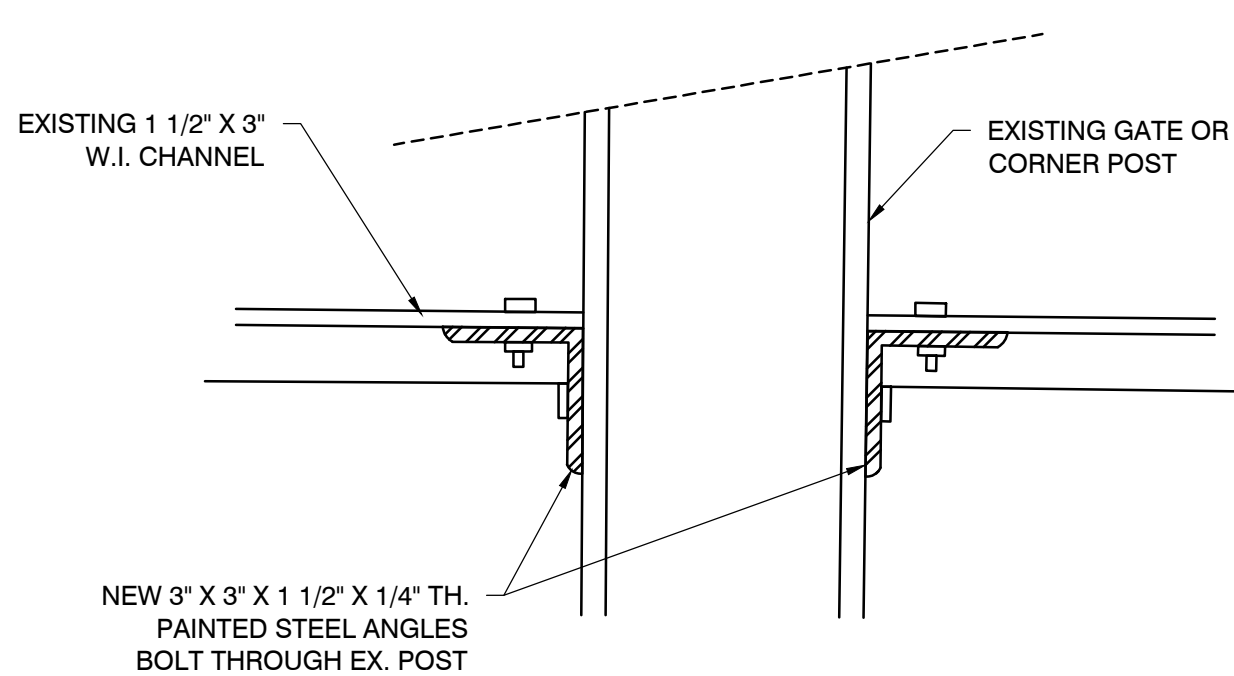
DETAIL- CONCRETE CURB
N.T.S.



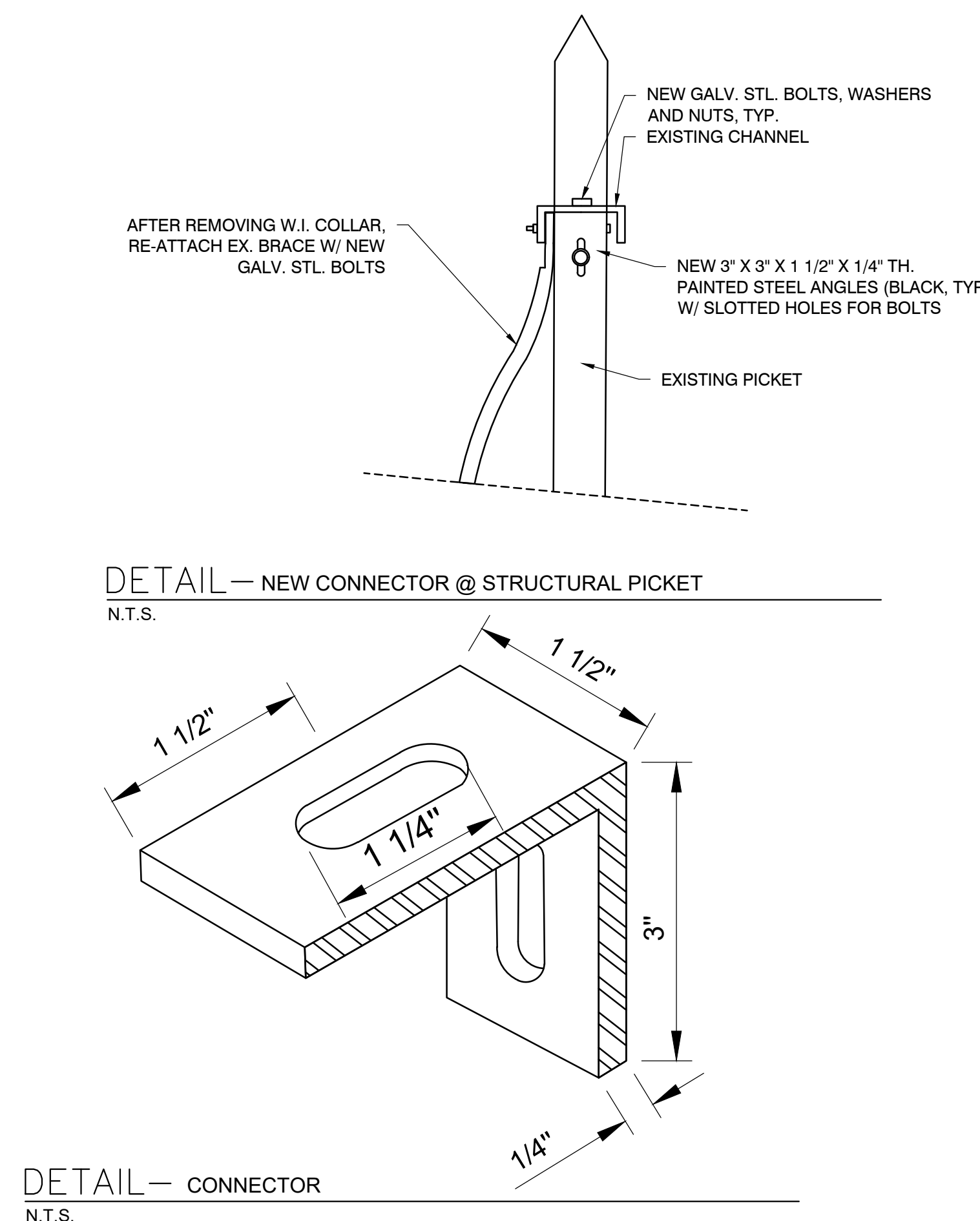
DETAIL- WROUGHT IRON FENCE
N.T.S.



DETAIL- NEW CONNECTOR @ STRUCTURAL PICKET
N.T.S.



DETAIL- NEW CONNECTORS @ GATE OR CORNER POST
N.T.S.



DETAIL- CONNECTOR
N.T.S.

PACKAGE 2 - IFB
NOT FOR CONSTRUCTION
08/28/23
NPDES PERMIT #PAC510302
PWD TRACKING #FY22-KING-6800-01

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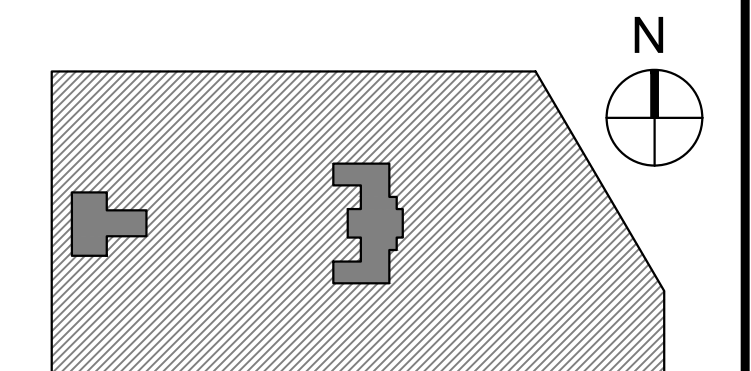
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PROJECT TITLE
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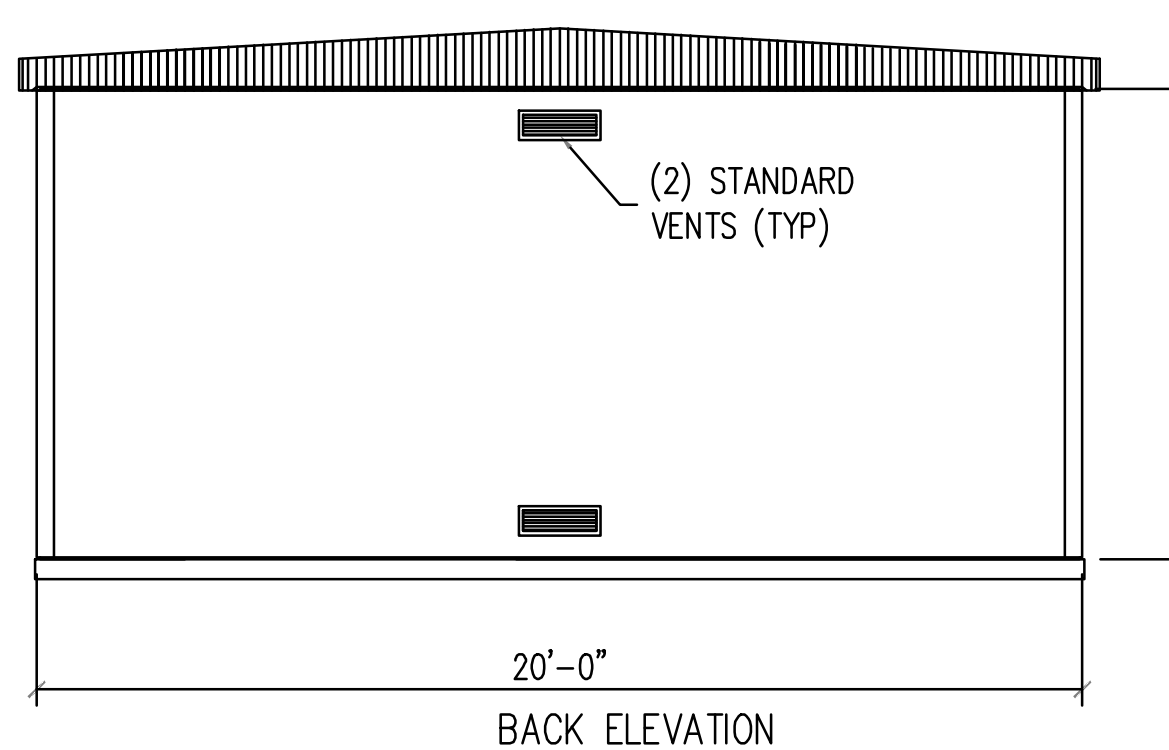
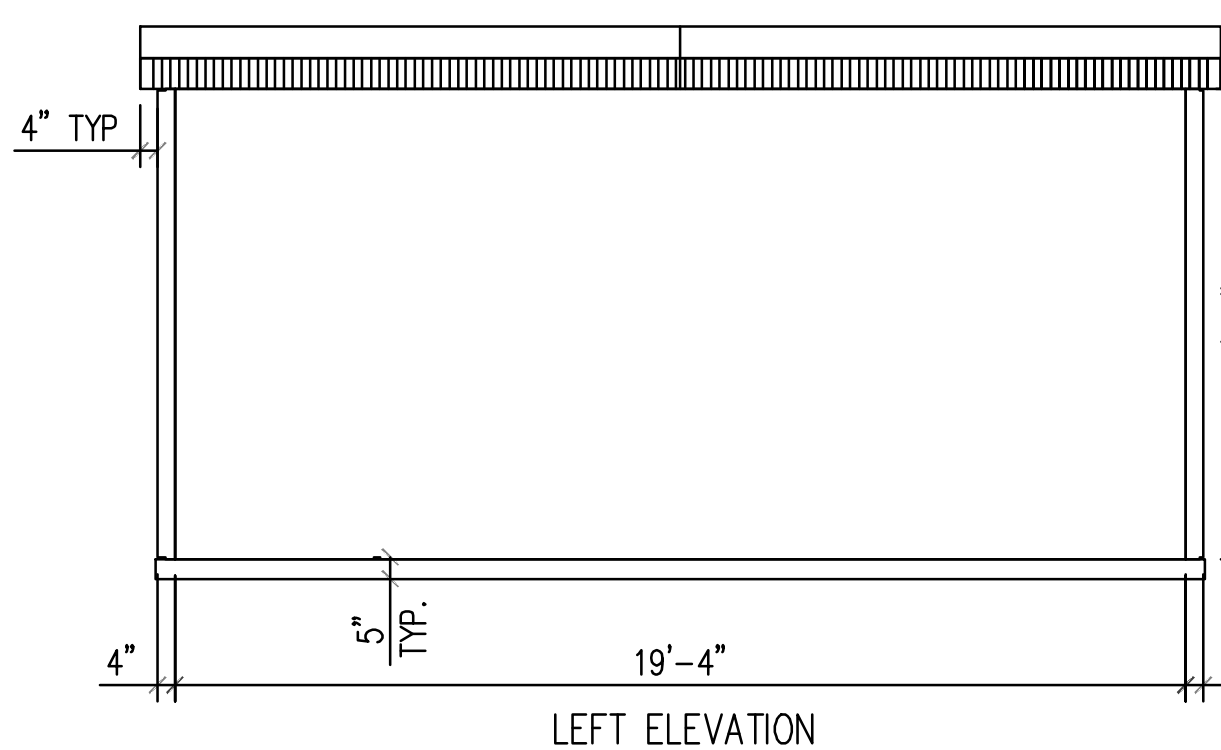
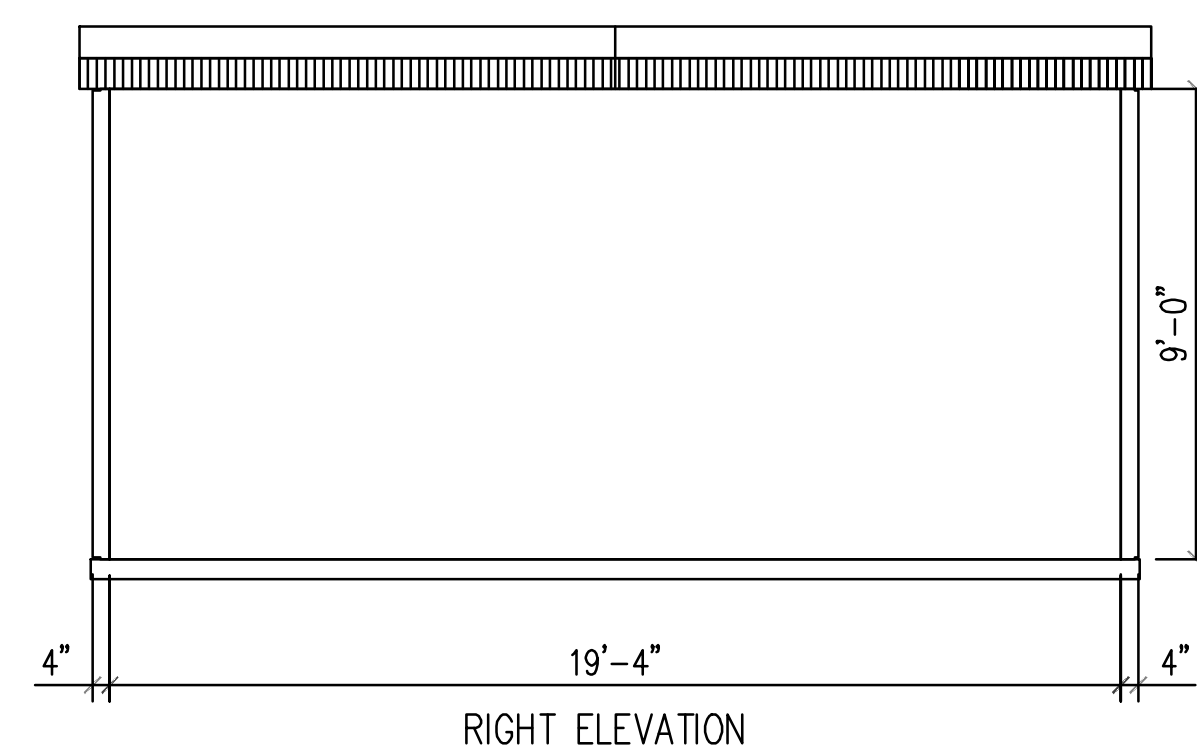
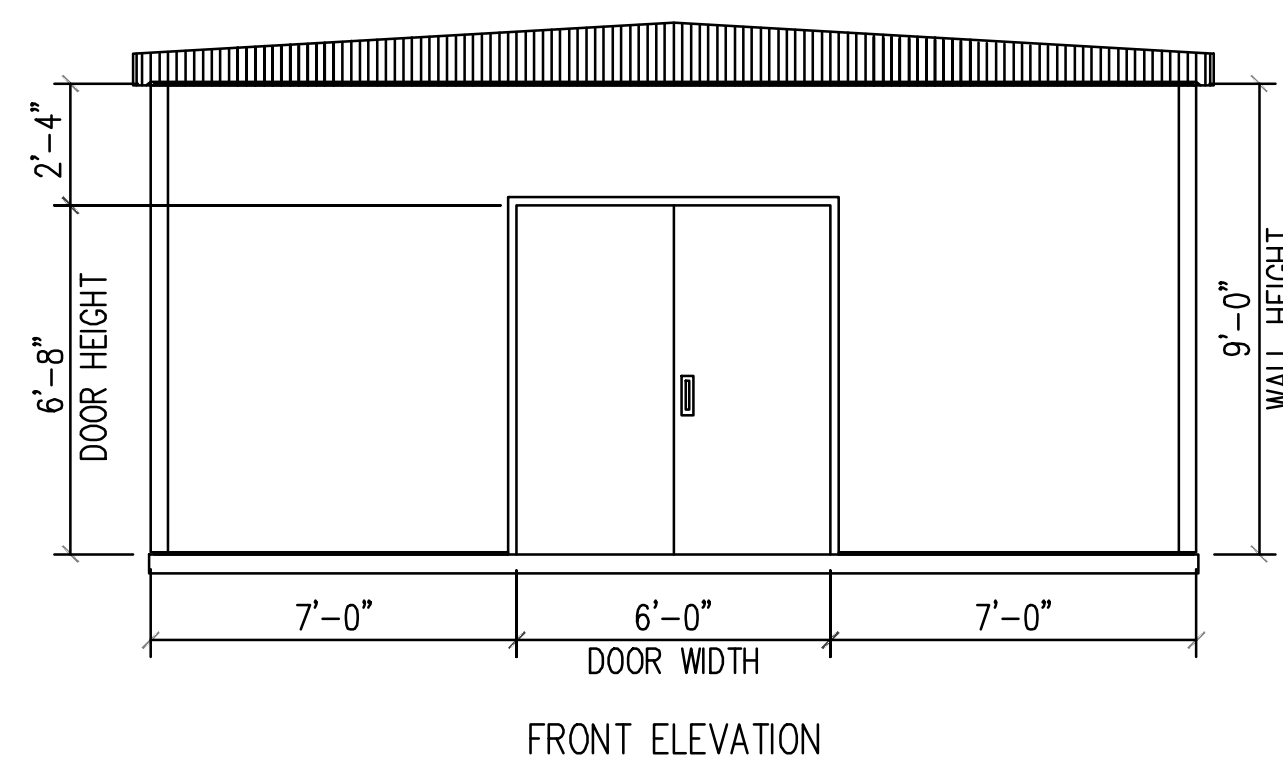
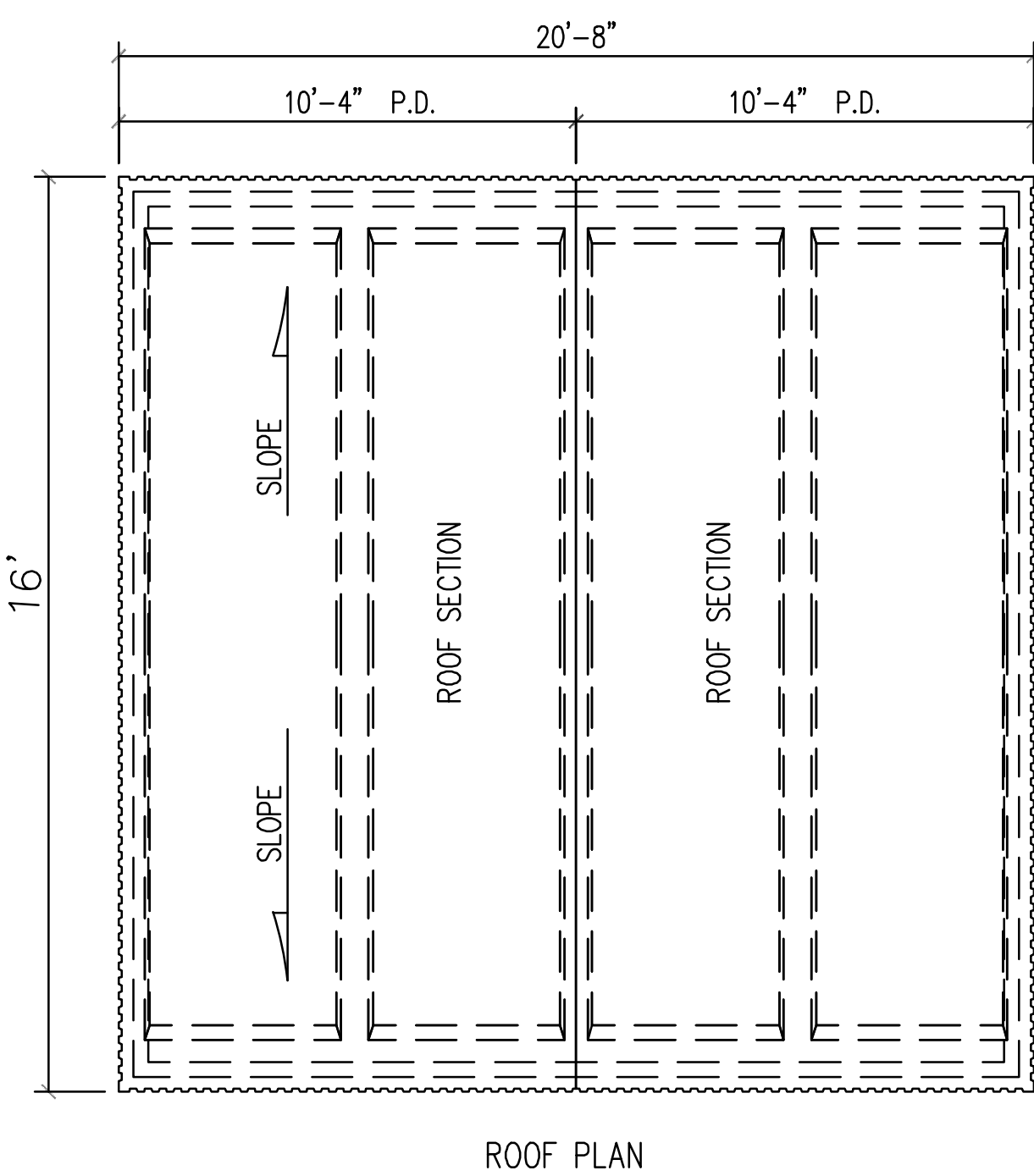
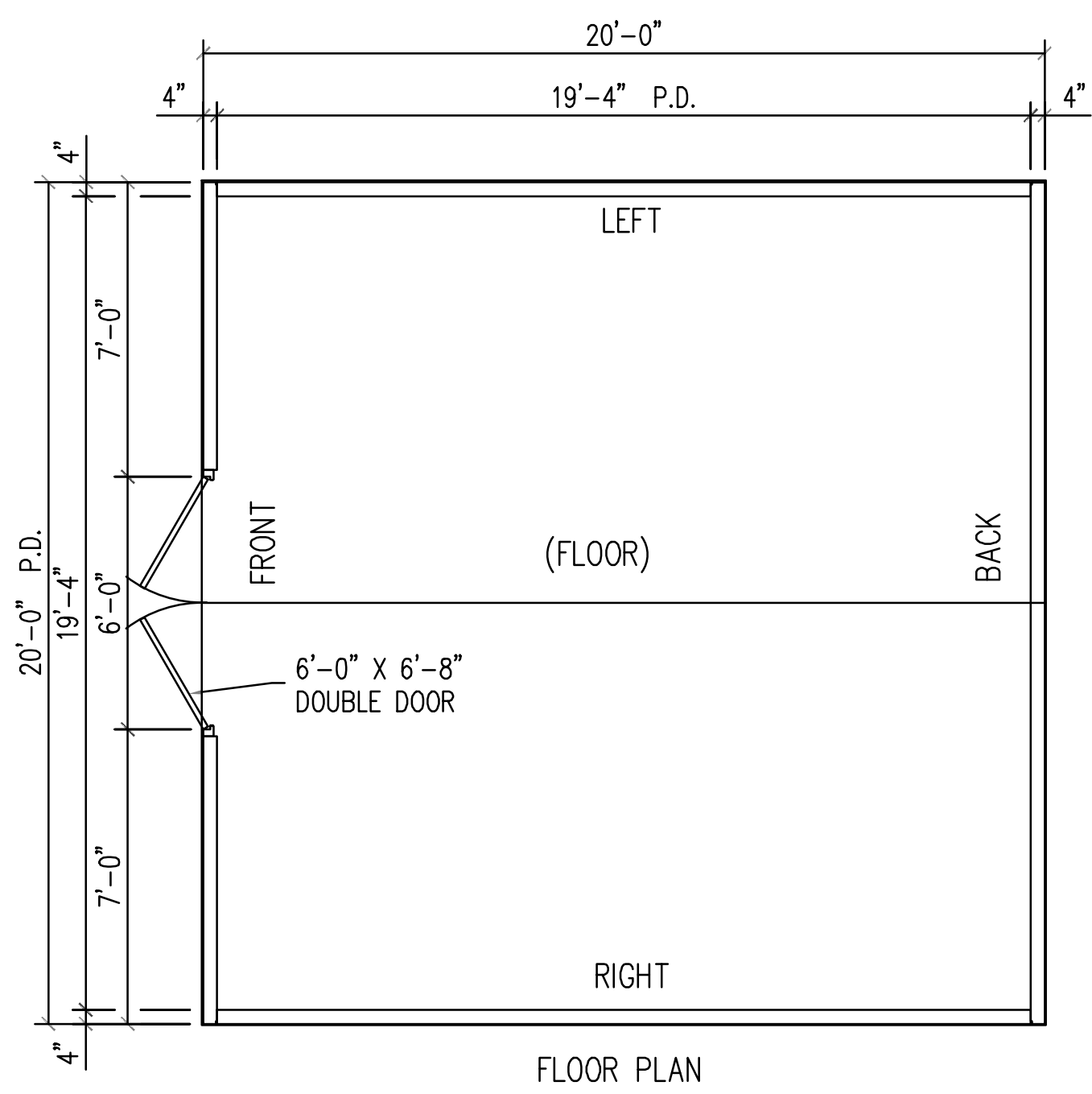
KEY PLAN



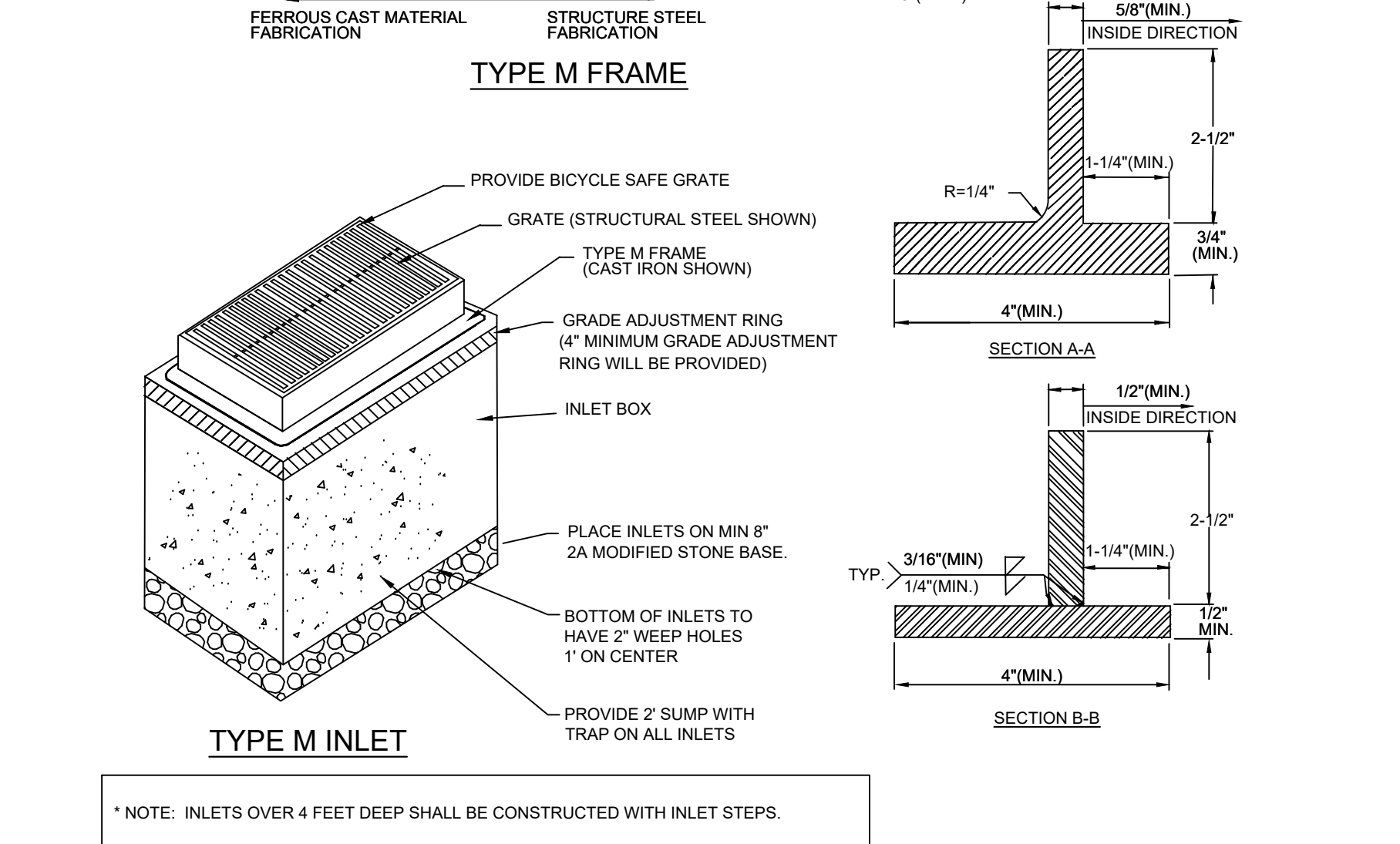
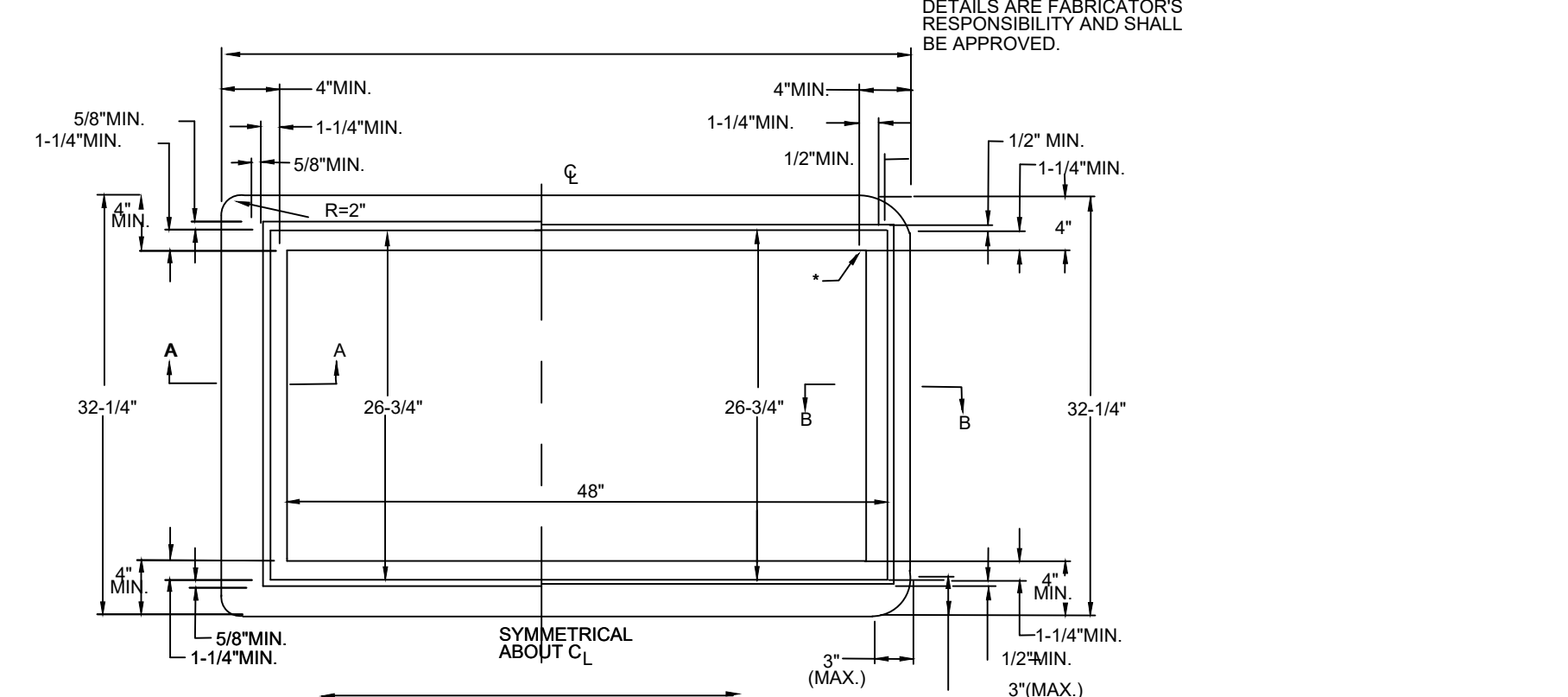
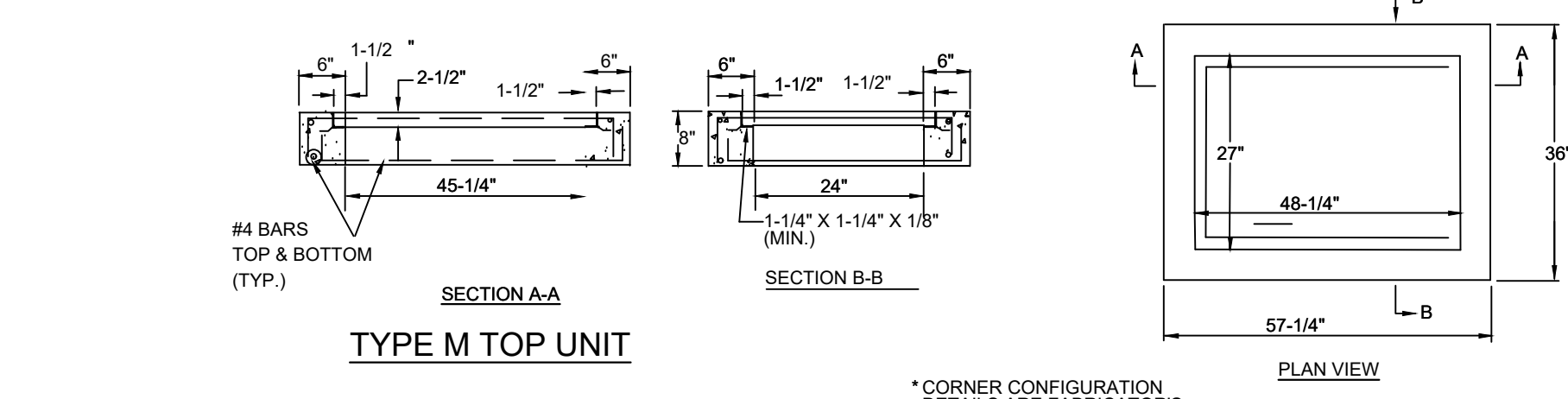
DRAWING TITLE
SITE DETAILS

PROJECT NO.	DRAWING NO.
KLMLX21003	CS601-R.2
DATE: 08/28/23	SCALE: AS NOTED
DRAWN BY: SDL	CHECKED BY: MJM

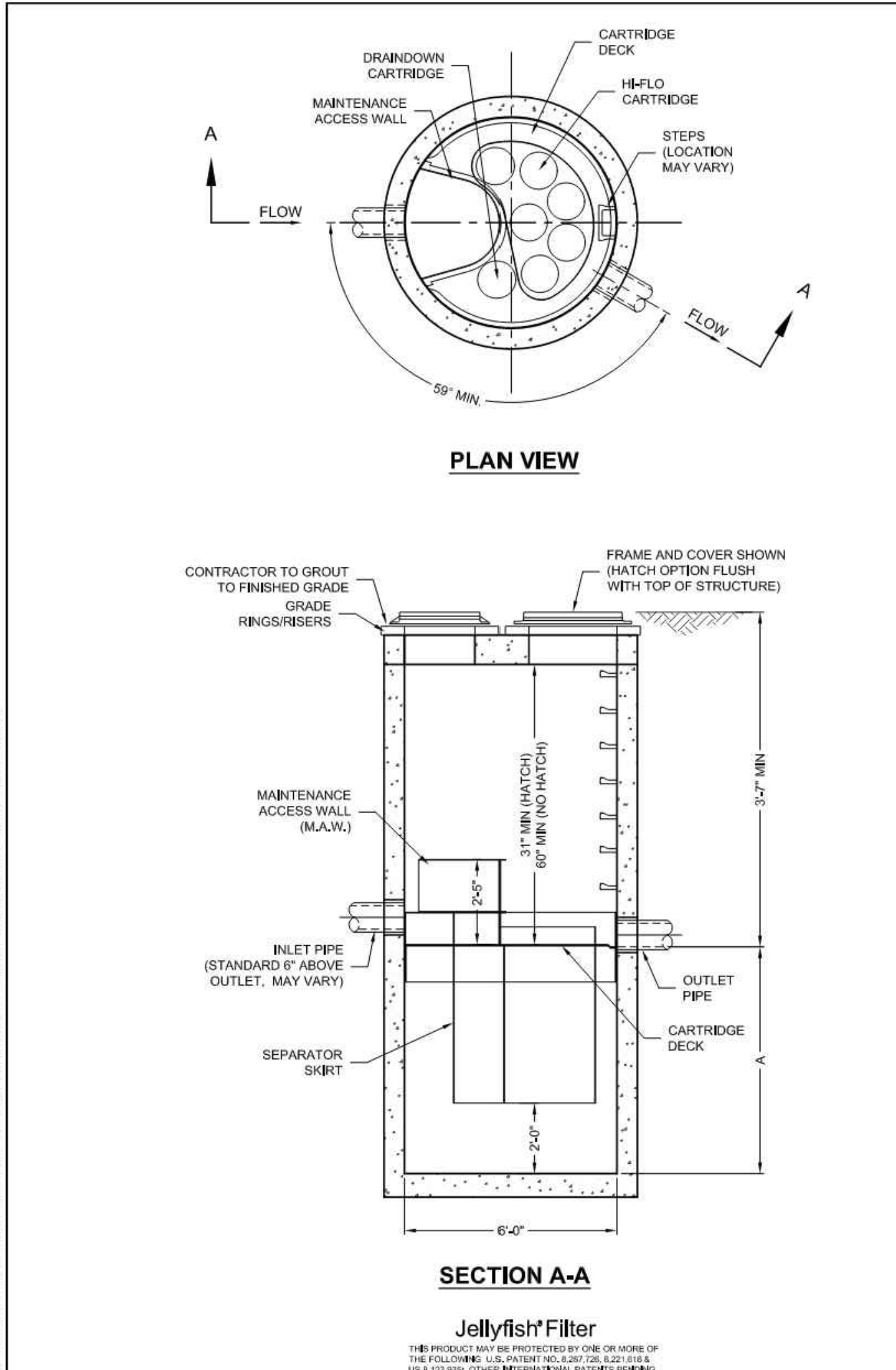
NOTE: ALL DIMENSIONS AND CONDITIONS SHALL BE VERIFIED BY THE CONTRACTOR AT THE SITE BEFORE PROCEEDING WITH THE WORK.



DETAIL- MAINTENANCE SHED
N.T.S.



DETAIL- TYPE M INLET
N.T.S.



JELLYFISH DESIGN NOTES

JELLYFISH TREATMENT CAPACITY IS A FUNCTION OF THE CARTRIDGE SELECTION AND THE NUMBER OF CARTRIDGES. THE STANDARD MANHOLE STYLE IS SHOWN. 87% MAXIMUM JELLYFISH TREATMENT CAPACITY IS 1.16 CFS. IF THE SITE CONDITIONS EXCEED 1.16 CFS AN UPSTREAM BYPASS STRUCTURE IS REQUIRED.

CARTRIDGE SELECTION	48"	40"	32"	24"
CARTRIDGE DEPTH	48"	34"	24"	30"
OUTLET INVERT TO STRUCTURE INVERT (ft)	0.49	0.42	0.42	0.30
FLOW RATE HIGH FLOW / DRAINDOWN (cfs) (per cart)	0.18 / 0.09	0.13 / 0.065	0.09 / 0.045	0.05 / 0.025
MAX. CARTS. HIGH FLOW / DRAINDOWN	8 / 1			

SITE SPECIFIC DATA REQUIREMENTS

PIPE DATA	I.E.	MATERIAL	DIAMETER
INLET PIPE #1	-	-	-
INLET PIPE #2	-	-	-
OUTLET PIPE	-	-	-
INLET ELEVATION	-	-	-
ANTI-FLOTATION BALLAST	WIDTH	HEIGHT	

NOTES/SPECIAL REQUIREMENTS:
* PER ENGINEER OF RECORD

GENERAL NOTES:

- CONTRACTOR TO PROVIDE ALL MATERIALS UNLESS NOTED OTHERWISE.
- FOR SITE SPECIFIC DRAWINGS WITH DETAILED STRUCTURE DIMENSIONS AND WEIGHT, PLEASE CONTACT YOUR CONTECH ENGINEERING SOLUTIONS REPRESENTATIVE. www.contech-engineering.com
- JELLYFISH WATER QUALITY STRUCTURE SHALL BE IN ACCORDANCE WITH ALL DESIGN DATA AND INFORMATION CONTAINED IN THESE DRAWINGS. CONTRACTOR TO CONFIRM STRUCTURE MEETS REQUIREMENTS OF PROJECT.
- STRUCTURE SHALL MEET ASHTO HS-20 OR PER APPLICABLE JURISDICTION REQUIREMENTS, WHICHEVER IS MORE STRINGENT. ASSUMING EARTH COVER OF 6'-0" AND GROUNDWATER ELEVATION AT OR BELOW THE OUTLET PIPE INVERT ELEVATION. ENGINEER OF RECORD TO CONFIRM ACTUAL GROUNDWATER ELEVATION. CASTINGS SHALL MEET ASHTO MRS LOAD RATING AND BE CAST WITH THE CONTECH LOGO.
- STRUCTURE SHALL BE PRECAST CONCRETE CONFORMING TO ASTM C-479 AND ASHTO LOAD FACTOR DESIGN METHOD.
- NO PRODUCT SUBSTITUTIONS SHALL BE ACCEPTED UNLESS SUBMITTED 10 DAYS PRIOR TO PROJECT BID DATE, OR AS DIRECTED BY THE ENGINEER OF RECORD.

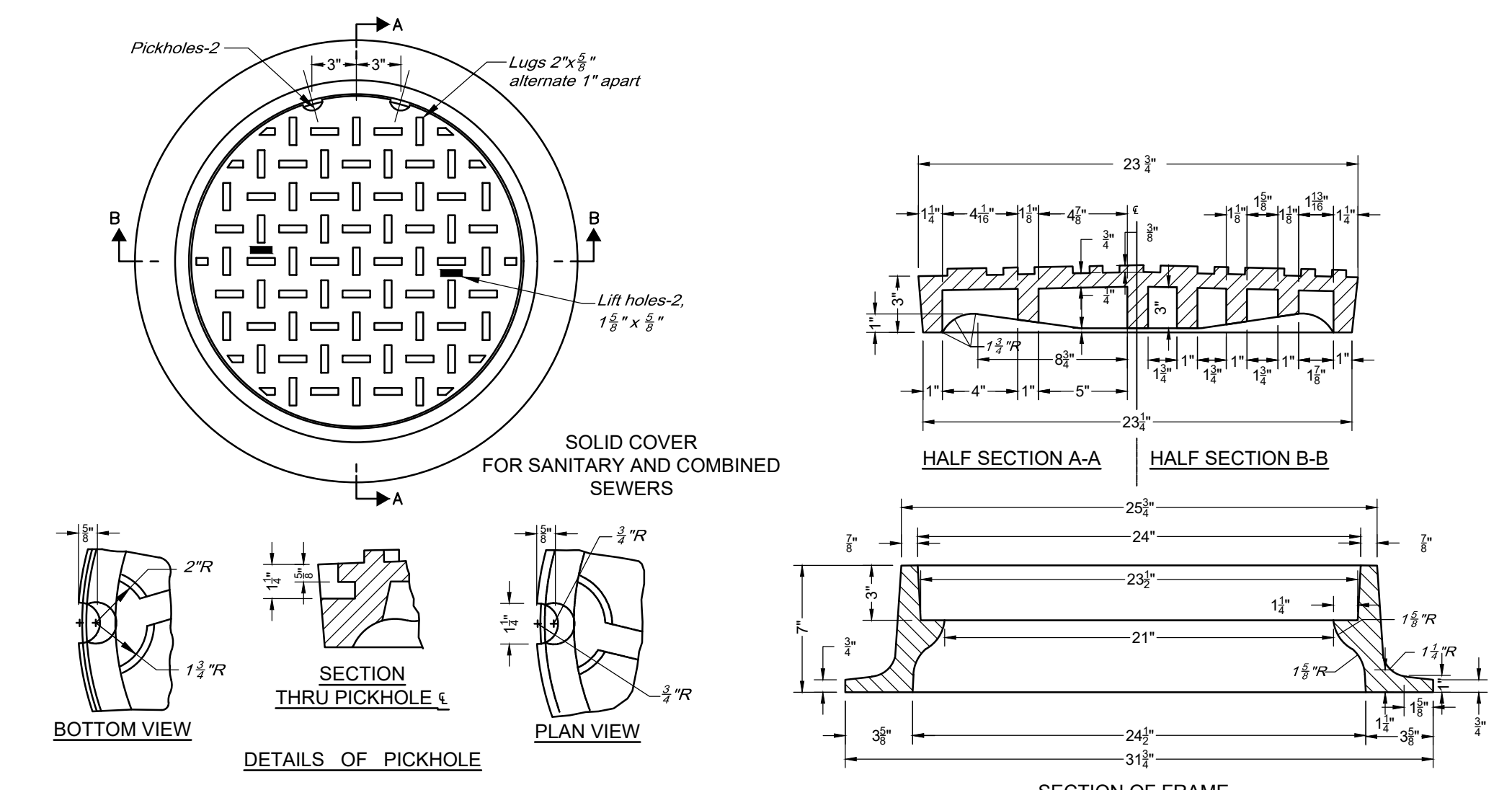
INSTALLATION NOTES:

- ANY SUBBASE, BACKFILL DEPTH, AND/OR ANTI-FLOTATION PROVISIONS ARE SITE-SPECIFIC DESIGN CONSIDERATIONS AND SHALL BE SPECIFIED BY ENGINEER OF RECORD.
- CONTRACTOR TO PROVIDE EQUIPMENT WITH SUFFICIENT LIFTING AND REACH CAPACITY TO LIFT AND SET THE STRUCTURE (LIFTING CLUTCHES PROVIDED).
- CONTRACTOR WILL INSTALL AND LEVEL THE STRUCTURE, SEALING THE JOINTS, LINE ENTRY AND EXIT POINTS (NON-SHRINK GROUT WITH APPROVED WATERSTOP OR FLEXIBLE BOOT).
- CONTRACTOR TO TAKE APPROPRIATE MEASURES TO PROTECT CARTRIDGES FROM CONSTRUCTION-RELATED EROSION RUNOFF.
- CARTRIDGE INSTALLATION, BY CONTECH, SHALL OCCUR ONLY AFTER SITE HAS BEEN STABILIZED AND THE JELLYFISH UNIT IS CLEAN AND FREE OF DEBRIS. CONTACT CONTECH TO COORDINATE CARTRIDGE INSTALLATION WITH SITE STABILIZATION AT (866) 746-3318.

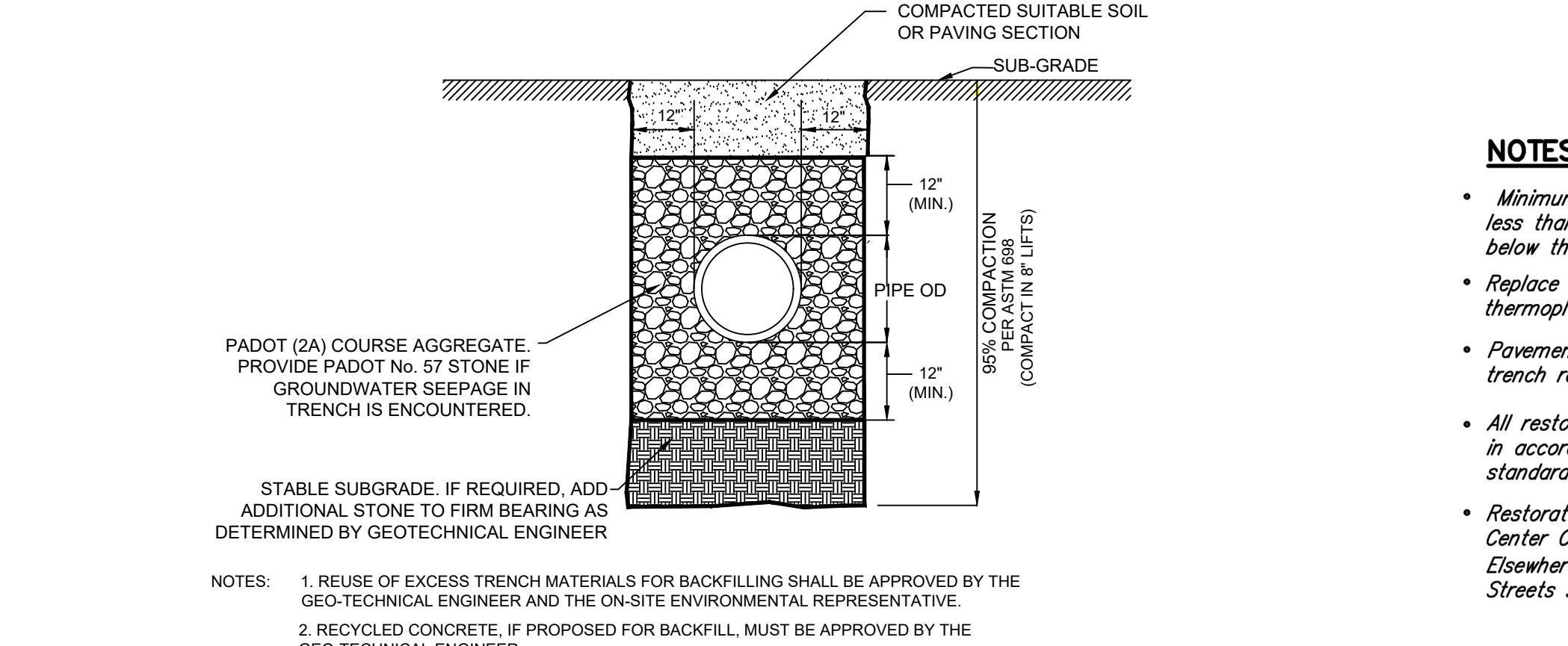
JELLYFISH JF6 STANDARD DETAIL OFFLINE CONFIGURATION

CONTECH ENGINEERING SOLUTIONS LLC
www.contech-engineering.com
9025 Center Park Dr., Suite 400, West Chester, OH 45380
937.636.1122 937.636.4700 937.636.7000 FAX

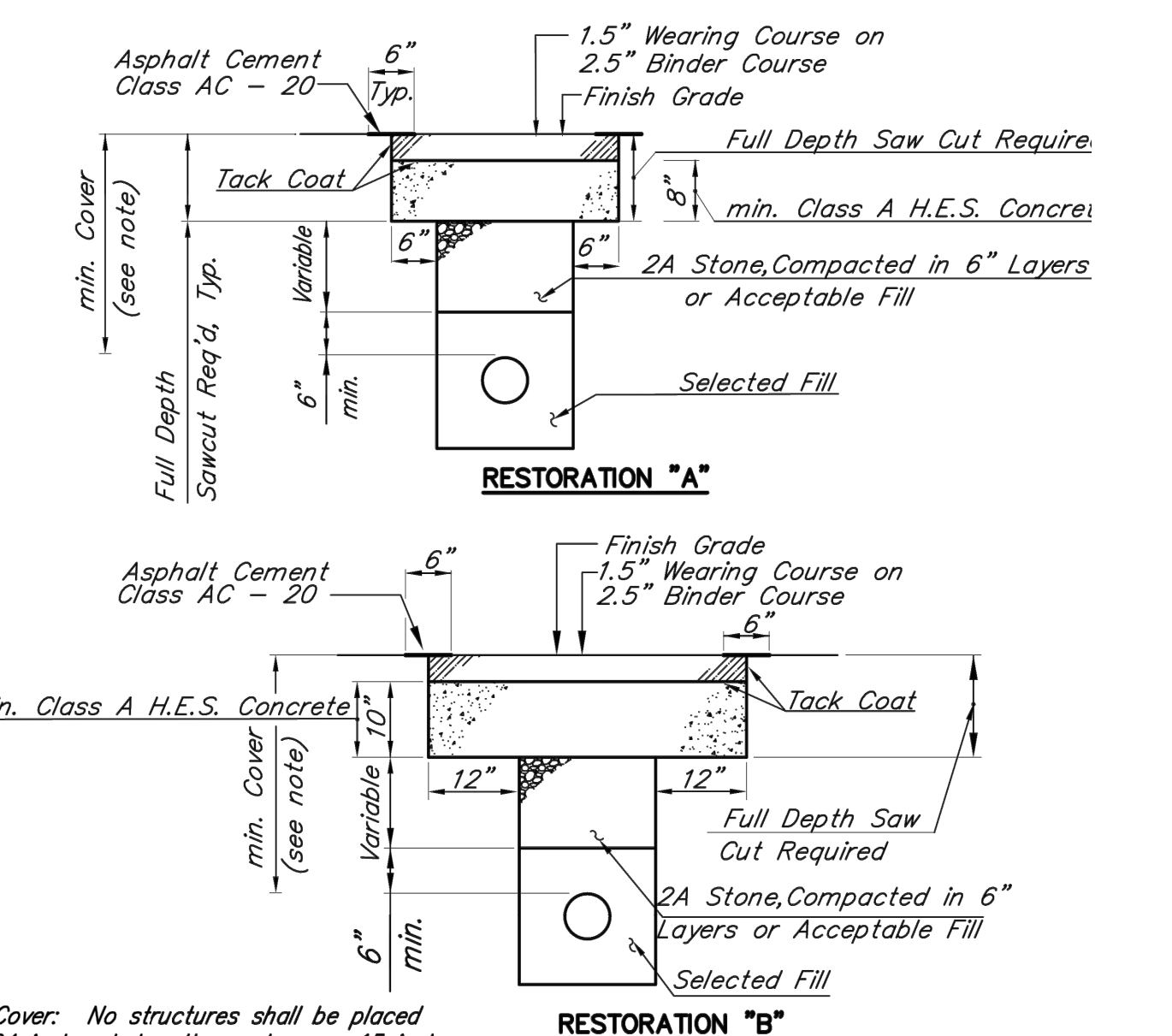
DETAIL- CONTECH JELLYFISH FILTER
N.T.S.



DETAIL- MANHOLE COVER
N.T.S.



DETAIL- TRENCH RESTORATION (ON SITE)
N.T.S.



NOTES:

- Minimum Cover: No structures shall be placed less than 24 inches below the cartway or 15 inches below the footway.
- Replace all disturbed pavement markings with thermoplastic tape.
- Pavement Markings must be restored within 7 days of trench restoration.
- All restoration by permittee to be in accordance with the latest Streets Department standards.
- Restoration 'B' shall be used for all streets within Center City and all arterial streets outside Center City. Elsewhere, Restoration 'A' shall be used. Historic Streets shall be restored in kind.

DETAIL- TRENCH RESTORATION WITHIN RIGHT-OF-WAY
N.T.S.

REVISIONS

ISSUE	DATE	DESCRIPTION
1	01/09/23	PWD PCSM/NPDES SUBMISSION
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PROJECT COORDINATOR

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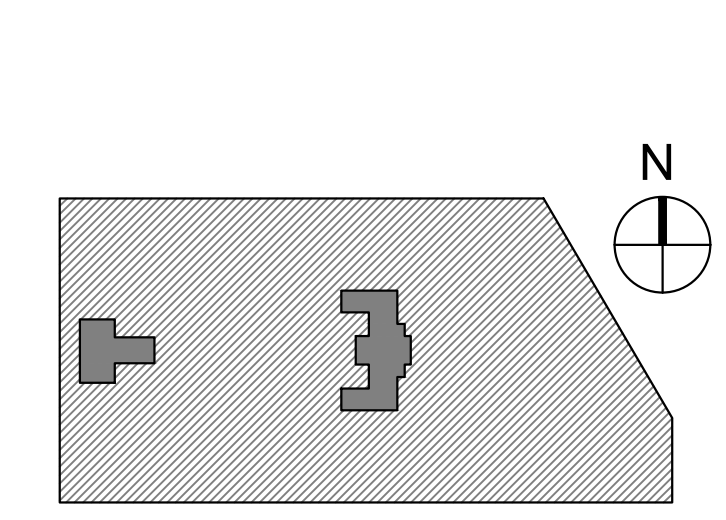
LEED CONSULTANT:
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CITY OF PHILADELPHIA
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1515 ARCH STREET
11TH FLOOR, ONE PARKWAY BUILDING
PHILADELPHIA PENNSYLVANIA

PROJECT TITLE

KINGSESSING RECREATION CENTER

KEY PLAN



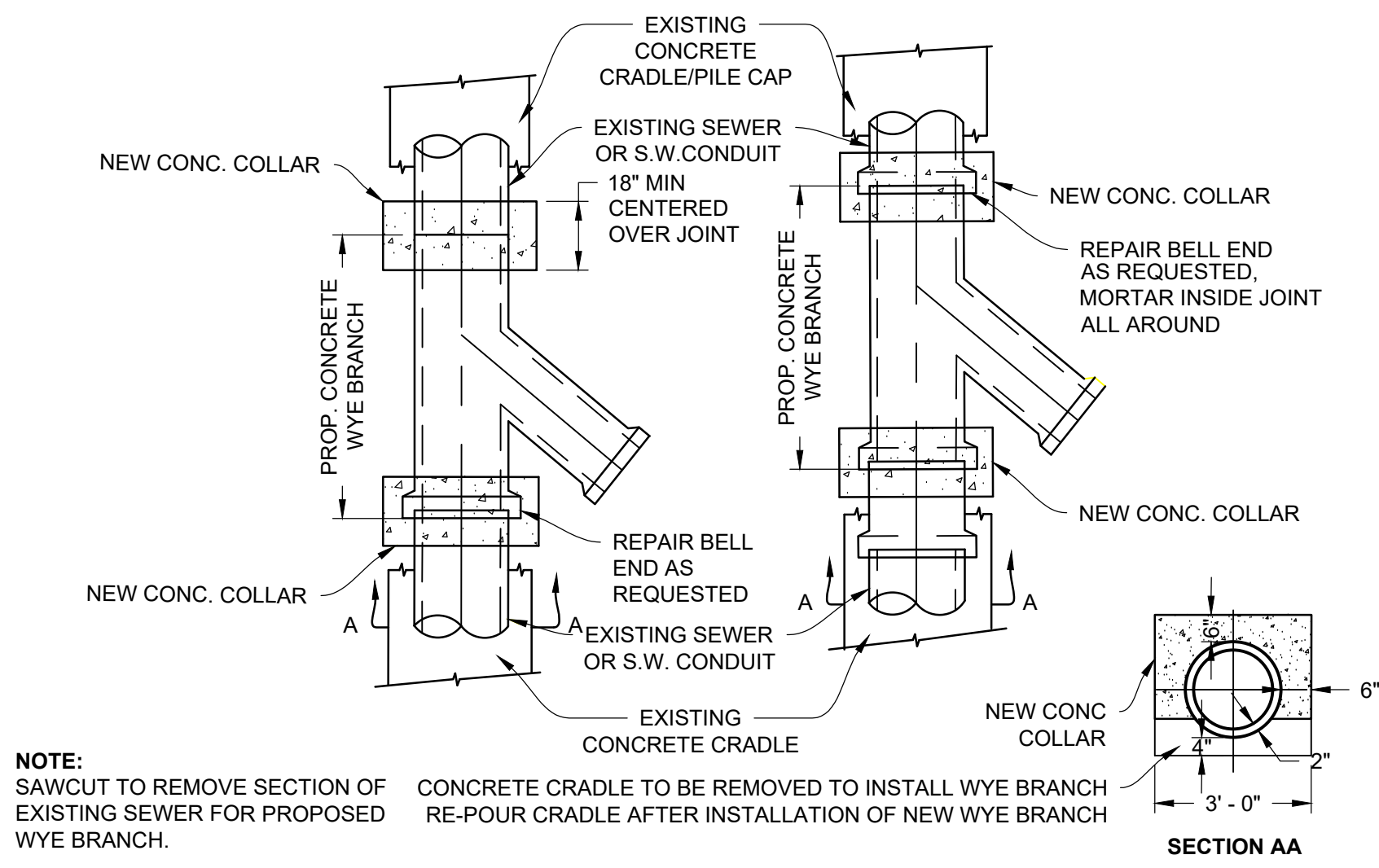
DRAWING TITLE

SITE DETAILS

PROJECT NO.	KLMLX21003	DRAWING NO.	CS602-R.2
DATE	08/28/23	SCALE	AS NOTED
DRAWN BY	SDL	CHECKED BY	MJM

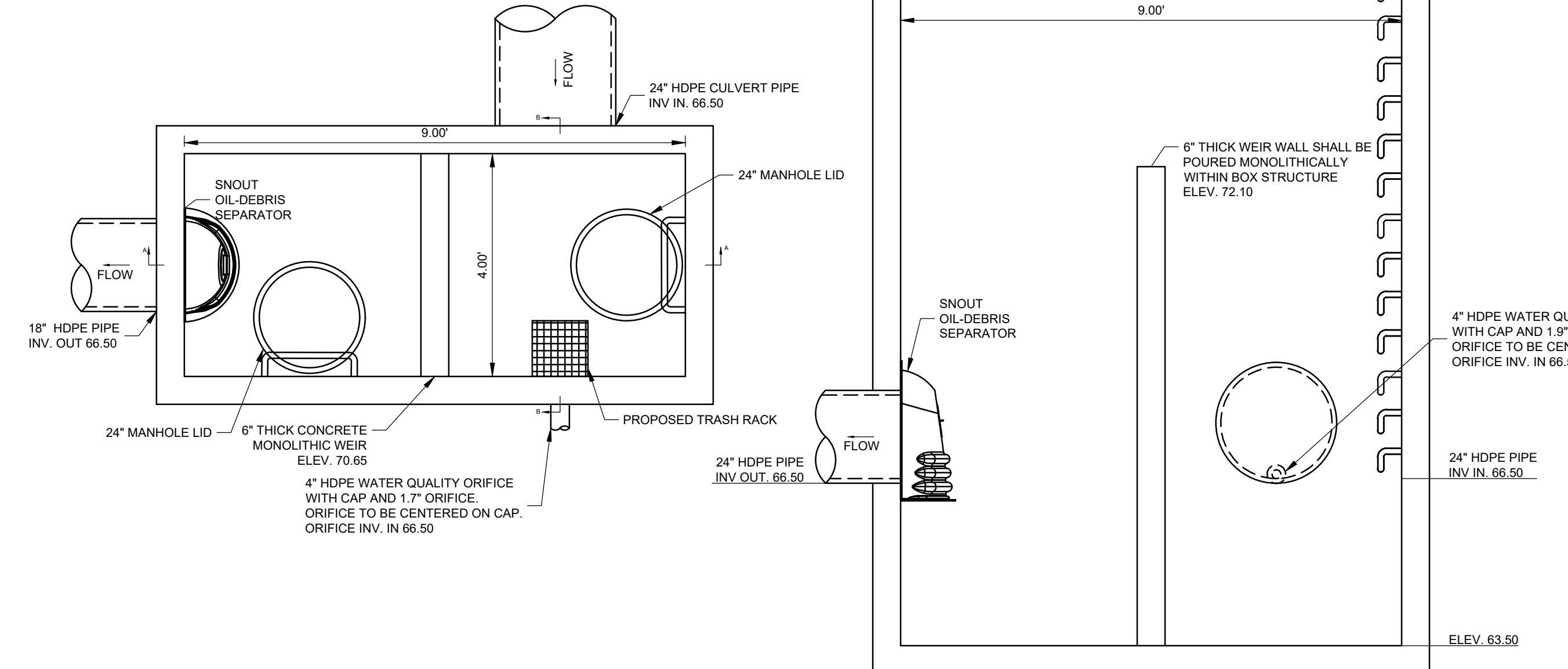
PACKAGE 2 - IFB
NOT FOR CONSTRUCTION
08/28/23
NPDES PERMIT #PAC510302
PWD TRACKING #FY22-KING-6800-01

NOTE: ALL DIMENSIONS AND CONDITIONS SHALL BE VERIFIED BY THE CONTRACTOR AT THE SITE BEFORE PROCEEDING WITH THE WORK.

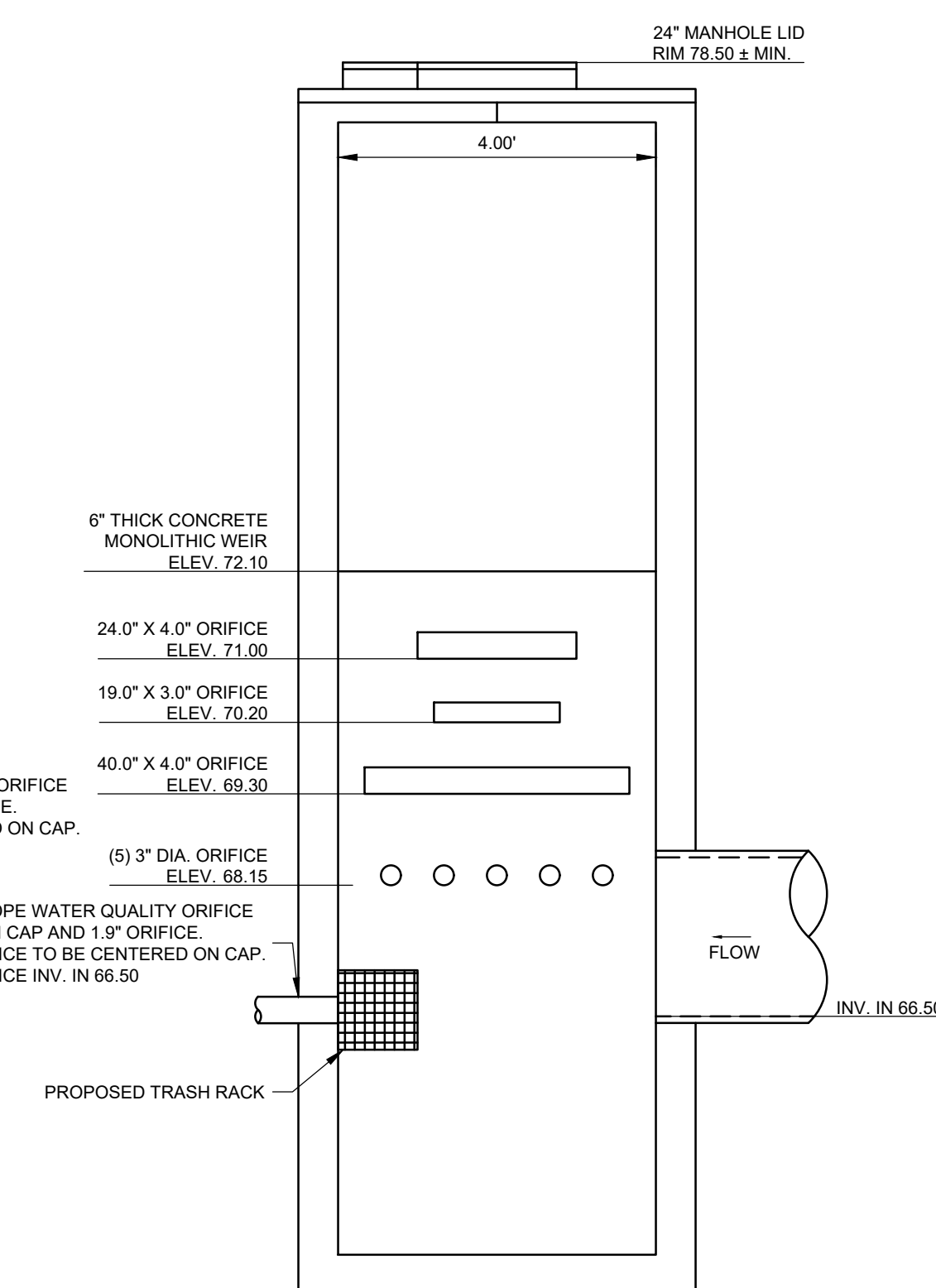


- WYE BRANCH INSTALLATION PROCEDURE:**
- CONTRACTOR IS RESPONSIBLE FOR CONTROLLING THE FLOW DURING THE CONNECTION.
 - THE CONTRACTOR IS TO OBTAIN INFORMATION ABOUT THE FLOW RATE PRIOR TO COMMENCING THE CONNECTION. THIS MAY BE OBTAINED BY OPENING A MANHOLE AND MEASURING THE FLOW (FLOW CAN VARY).
 - FOR HIGH FLOW RATE THE CONTRACTOR SHALL SUBMIT A BY-PASS METHOD FOR APPROVAL.
 - FOR LOW FLOW RATE THE CONTRACTOR MAY BE ABLE TO DIRECT THE FLOW THROUGH THE OPENED SECTION.
 - THE CONTRACTOR MUST SELECT THE PROPER SIZE WYE AND ALIGN THE INVERTS.
 - EXCAVATE SAFELY TO EXPOSE THE EXISTING SEWER PIPE WITHOUT DAMAGING IT.
 - EXCAVATE A DITCH WIDE ENOUGH TO ACCOMMODATE THE WYE AND THE CUT-OFF WALLS.
 - CONTRACTORS SHOULD PERFORM THE CONNECTION IN A SAFE MANNER AND PER OSHA REGULATIONS.
 - SHORING SHALL BE INSTALLED IN DITCHES AND TRENCHES AS PER OSHA REGULATIONS OR AS REGULATED BY PWD. VIOLATION OF THIS PROVISION WILL RESULT IN A STOP WORK ORDER AND/OR PENALTIES PRESCRIBED BY LAW. (REFER TO PHILADELPHIA PLUMBING CODE 2004 SECTION P-1503.2 SHORING).
 - FOR CONNECTIONS TO BRICK SEWER, CONSTRUCT A 2-FOOT WIDE BY 8-INCH HIGH, CONCRETE CUTOFF WALL TO THE SPRING LINE, ON BOTH SIDES OF THE WYE, 12-INCH FROM THE LOCATION TO CUT THE SEWER PIPE.
 - USE A MINIMUM OF 3,500 PSI CONCRETE FOR THE CONCRETE CUTOFF WALL.
 - ALLOW AT LEAST 24 HOURS FOR THE CONCRETE TO CURE BEFORE CUTTING THE SEWER PIPE.
 - CUT THE SEWER PIPE SO THAT THE WYE SECTION WILL FIT IN TIGHTLY.
 - PREVENT ANY DEBRIS FROM FLOWING INTO THE SEWER.
 - CLEAN THE BOTTOM OF THE OPENED AREA FROM LOOSE AND SOFT SOIL IF NO CRADLE, AND PLACE STONE IN THE MIDDLE LEAVING 12 INCH BELOW THE JOINTS FOR CONCRETE COLLAR.
 - INSERT THE WYE SECTION IN PLACE IMMEDIATELY, SEAL THE JOINTS AND CONSTRUCT A 24-INCH CONCRETE COLLAR AROUND BOTH JOINTS.
 - USE A MINIMUM OF 3,500 PSI CONCRETE COLLAR, 12 INCH DEEP AND EXTENDING 24 INCH WIDE AROUND THE JOINT.
 - ALLOW AT LEAST 24 HOURS FOR THE CONCRETE TO CURE BEFORE BACKFILLING.
 - ANY OTHER PIPELINE EXPOSED AND UNDERMINED DURING THIS OPERATION MUST BE SUPPORTED IMMEDIATELY AND BACKFILLED WITH CONTROLLED LOW STRENGTH MATERIAL (CLSM) AFTER THE COMPLETION OF THE CONNECTION.

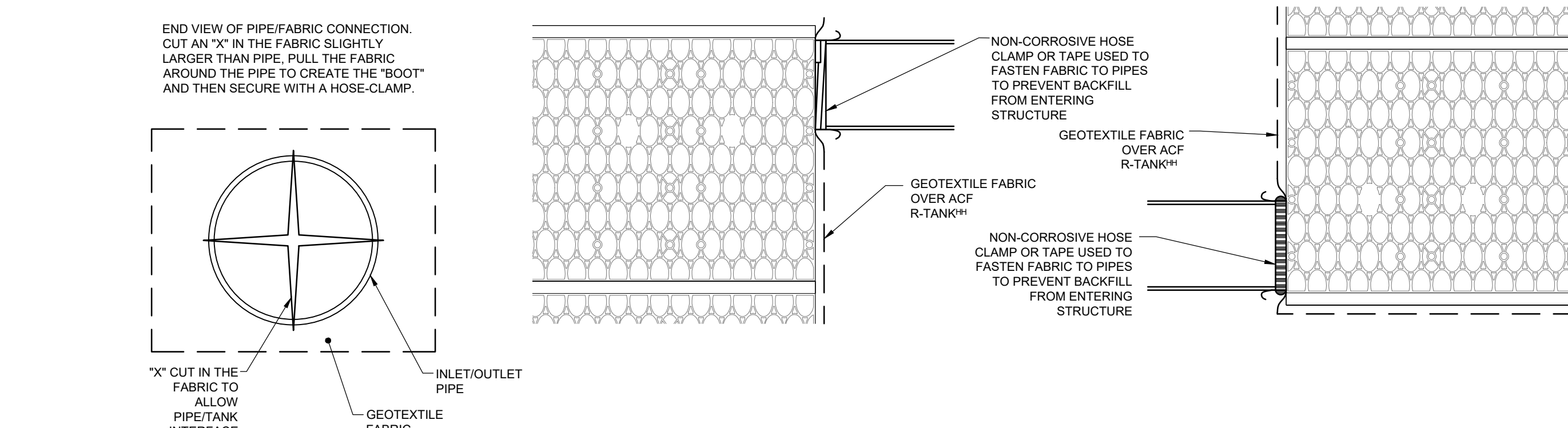
DETAIL- WYE CONNECTION
N.T.S.



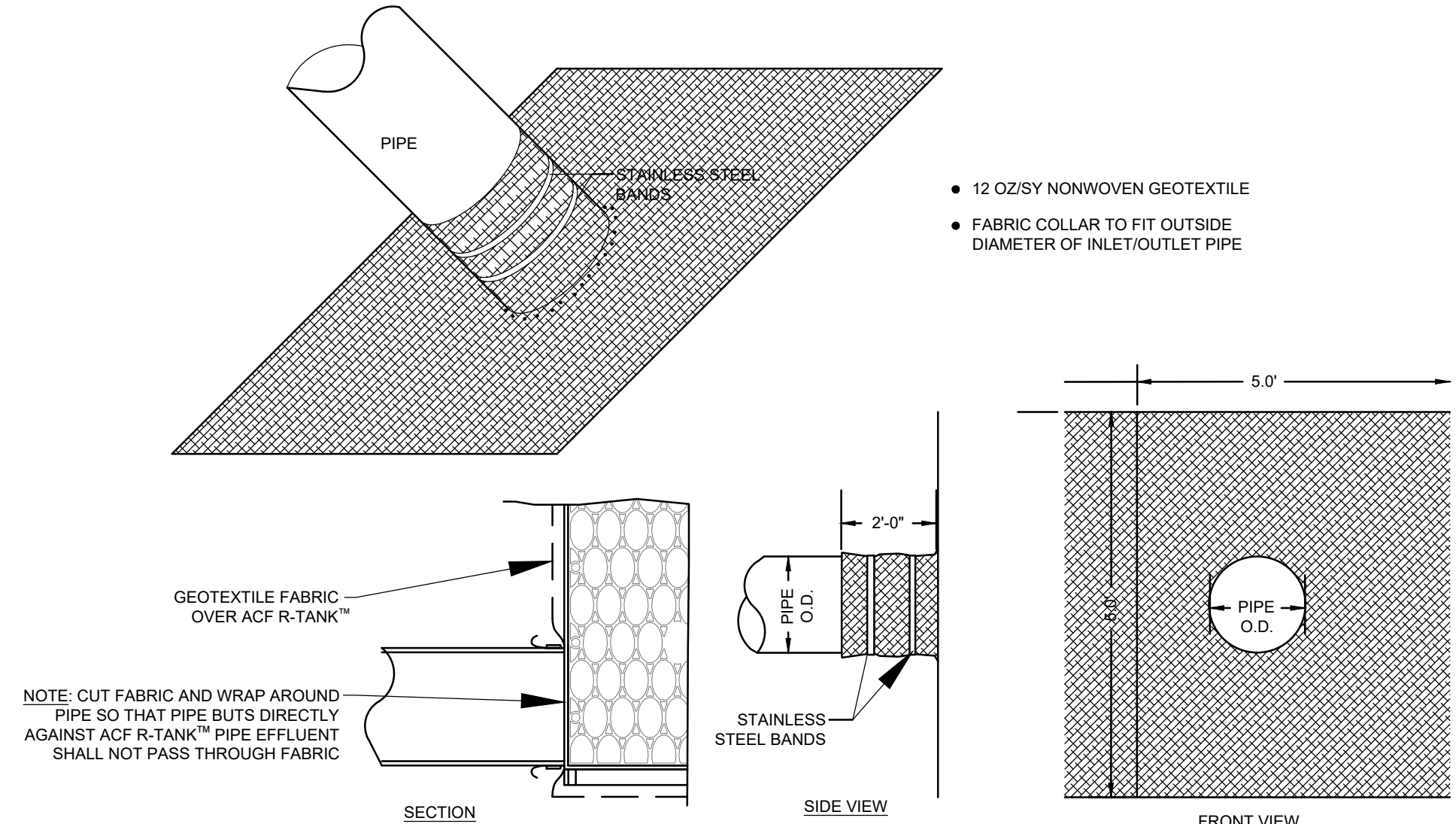
DETAIL- UNDERGROUND BASIN - OUTLET CONTROL STRUCTURE
1" = 2'



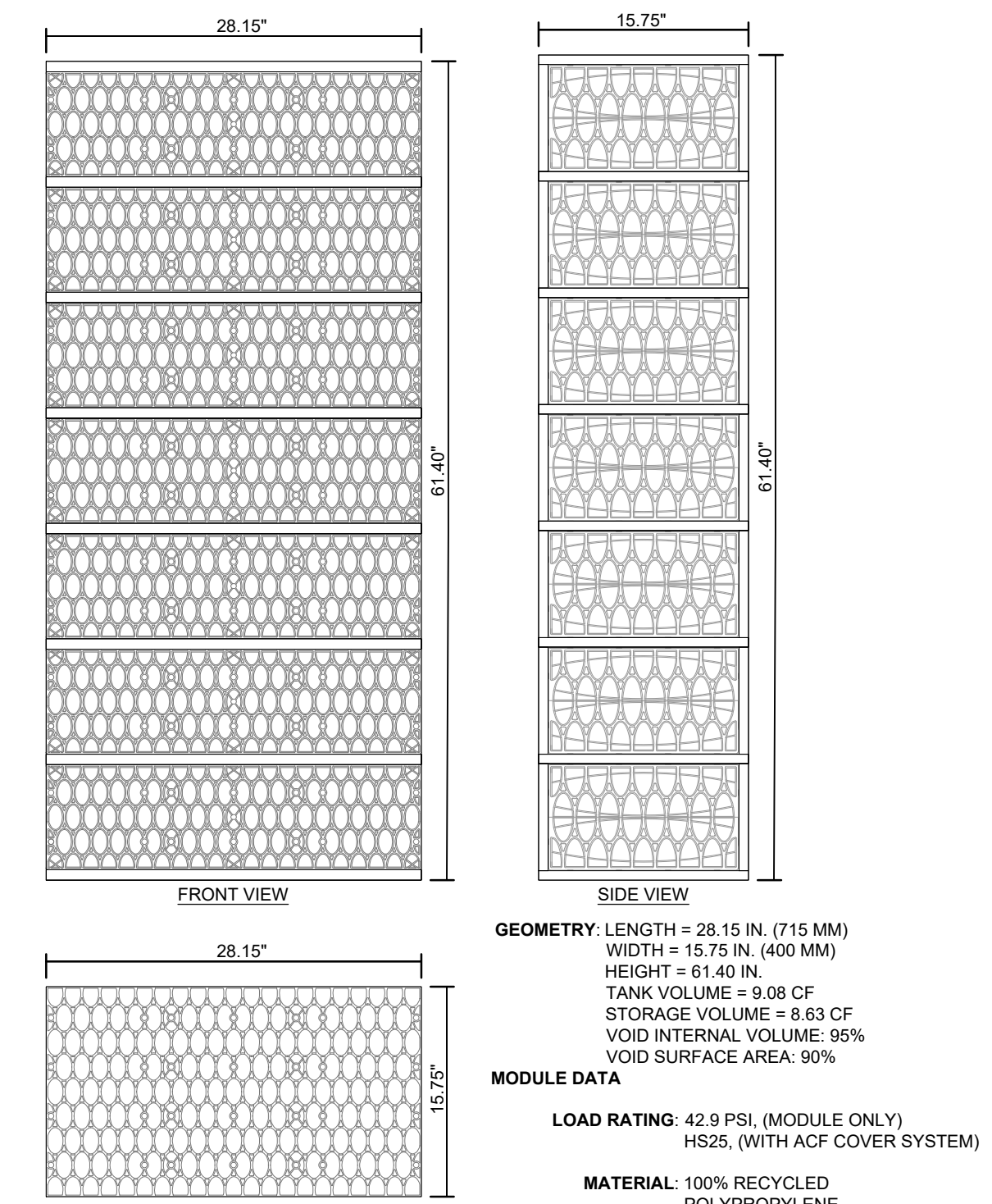
SECTION B-B
N.T.S.



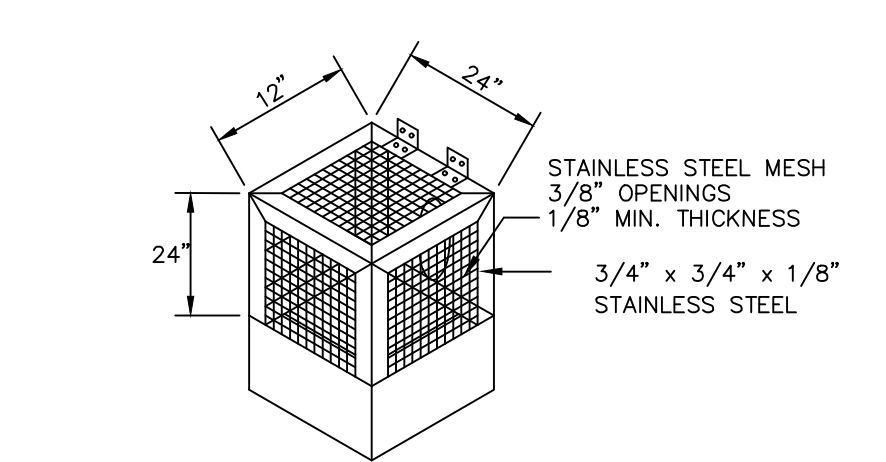
DETAIL- R-TANK INLET/OUTLET
N.T.S.



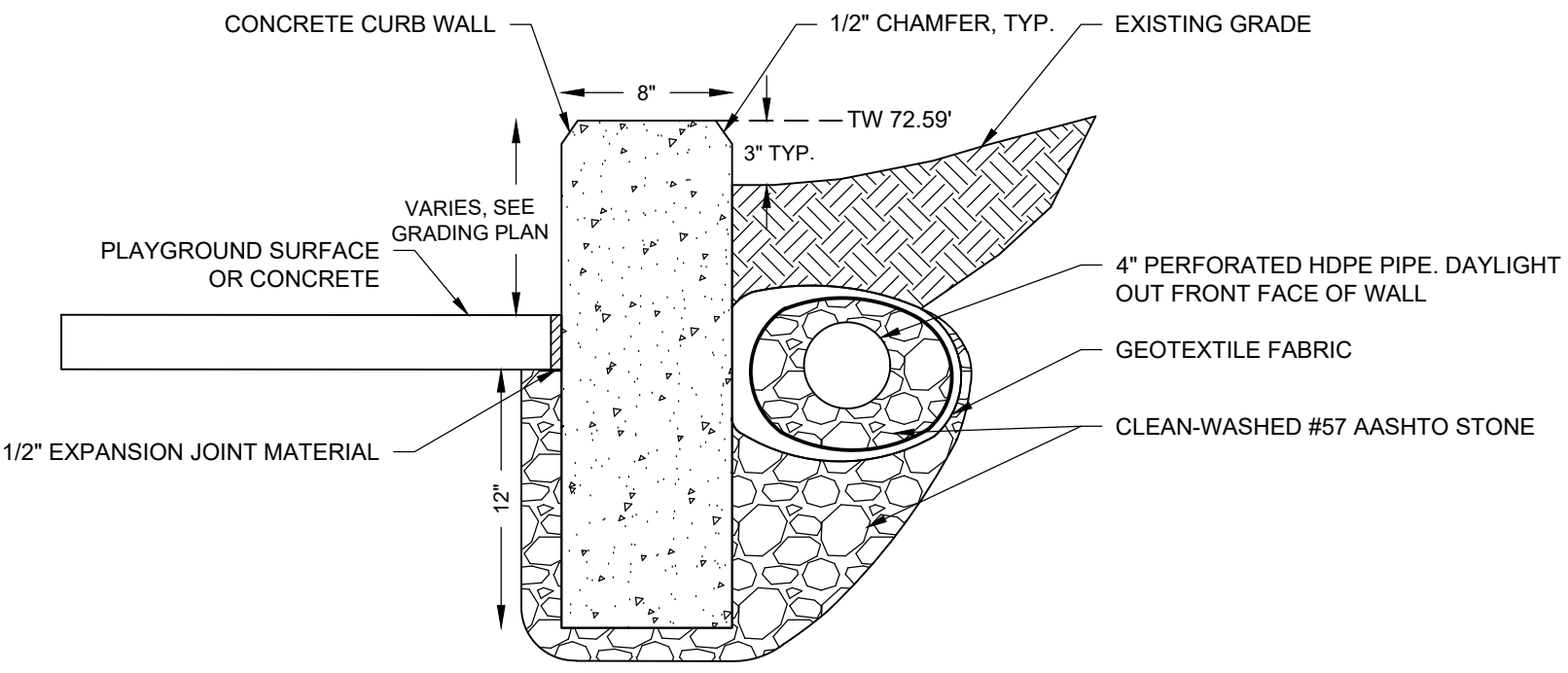
DETAIL- R-TANK PIPE BOOT
N.T.S.



DETAIL- RTANK SD 7 MODULE
N.T.S.



DETAIL- ORIFICE PROTECTION
N.T.S.



DETAIL- CONCRETE CURB WALL AT PLAYGROUND
N.T.S.

REVISIONS

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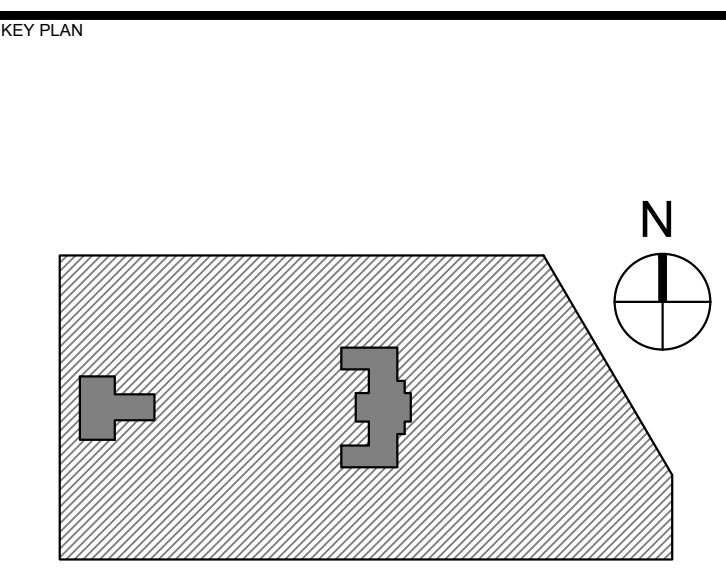
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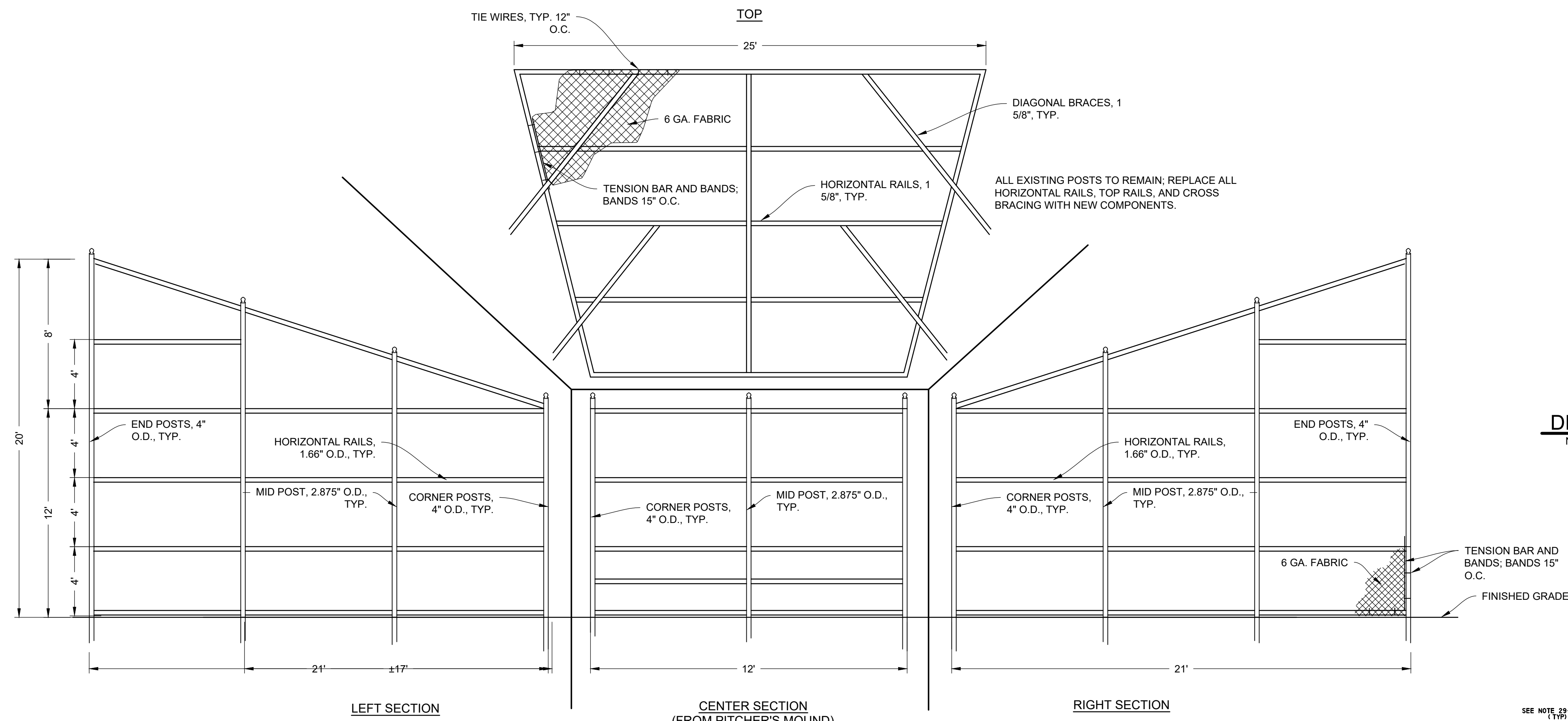


SITE DETAILS

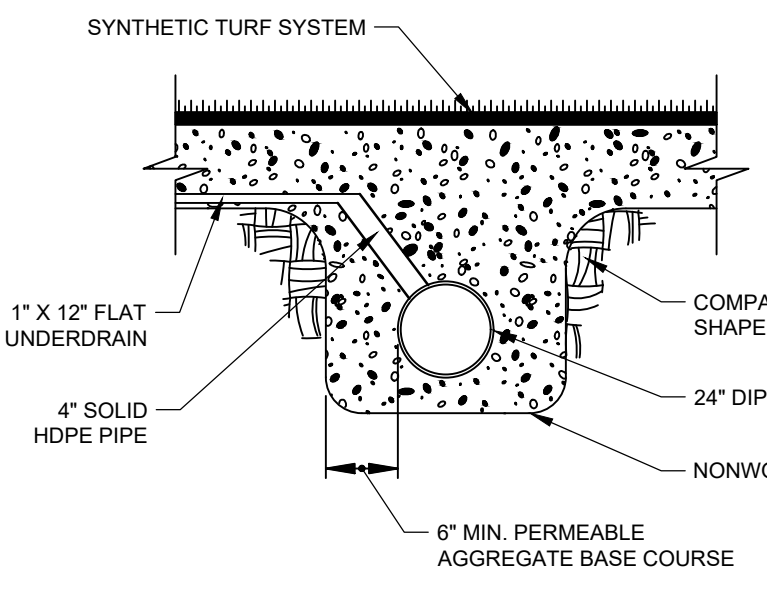
PROJECT NO. KLMLX21003	DRAWING NO. CS603-R.2
DATE 08/28/23	SCALE AS NOTED
DRAWN BY SDL	CHECKED BY MJM

PACKAGE 2 - IFB
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NPDES PERMIT #PAC510302
PWD TRACKING #FY22-KING-6800-01

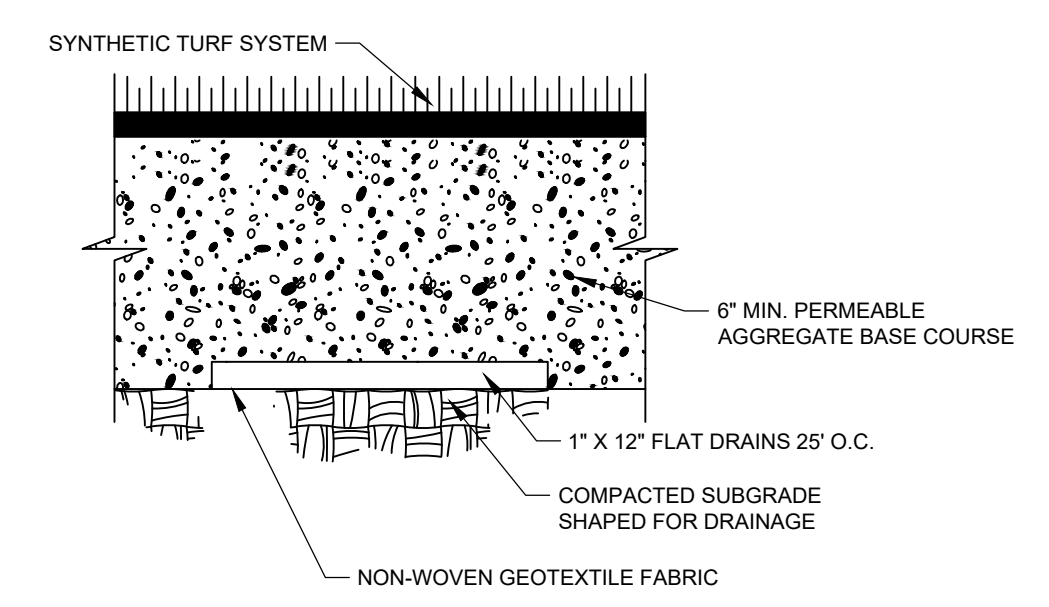
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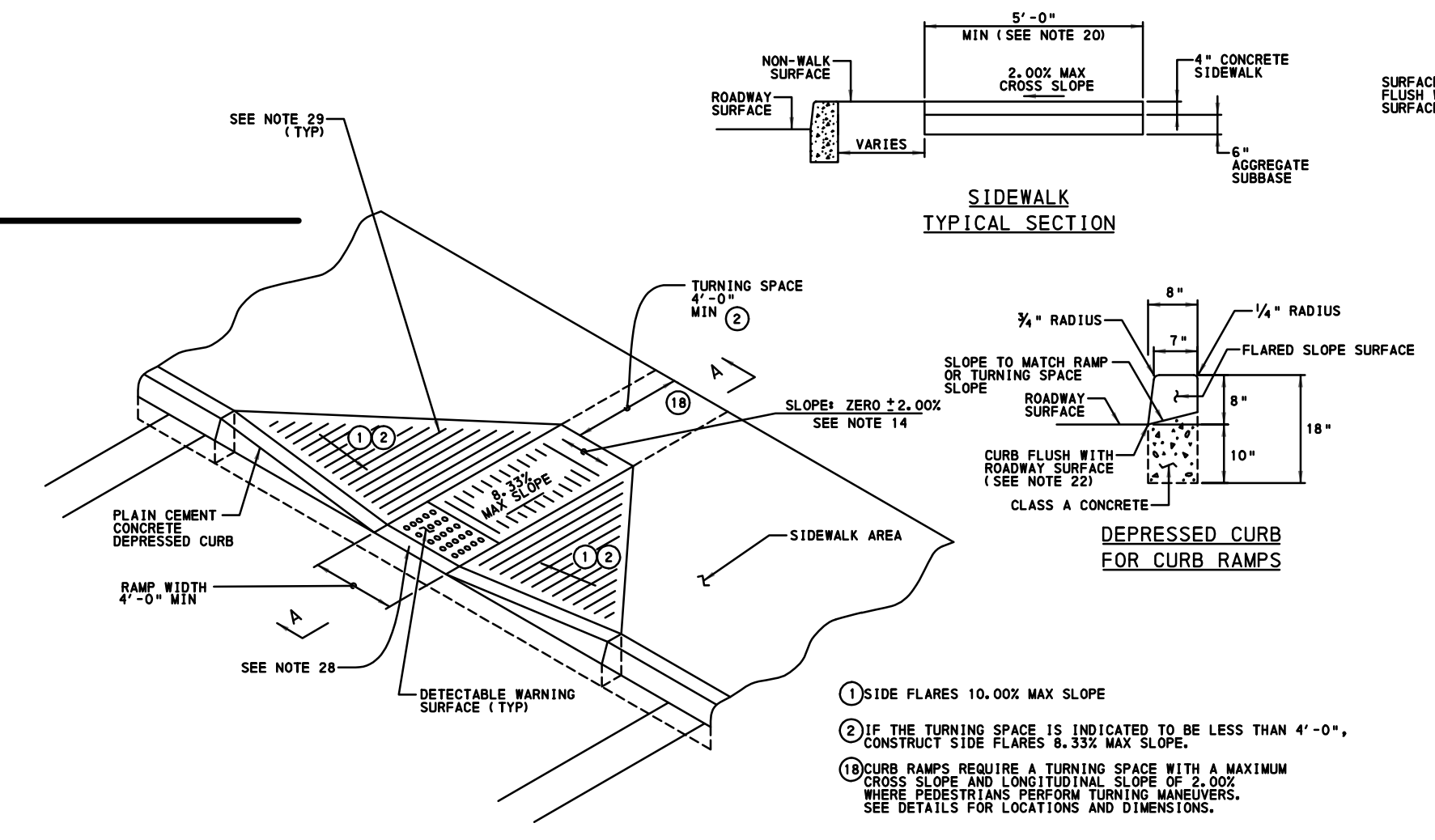
DETAIL- CHAIN LINK BACKSTOP FENCE
N.T.S.



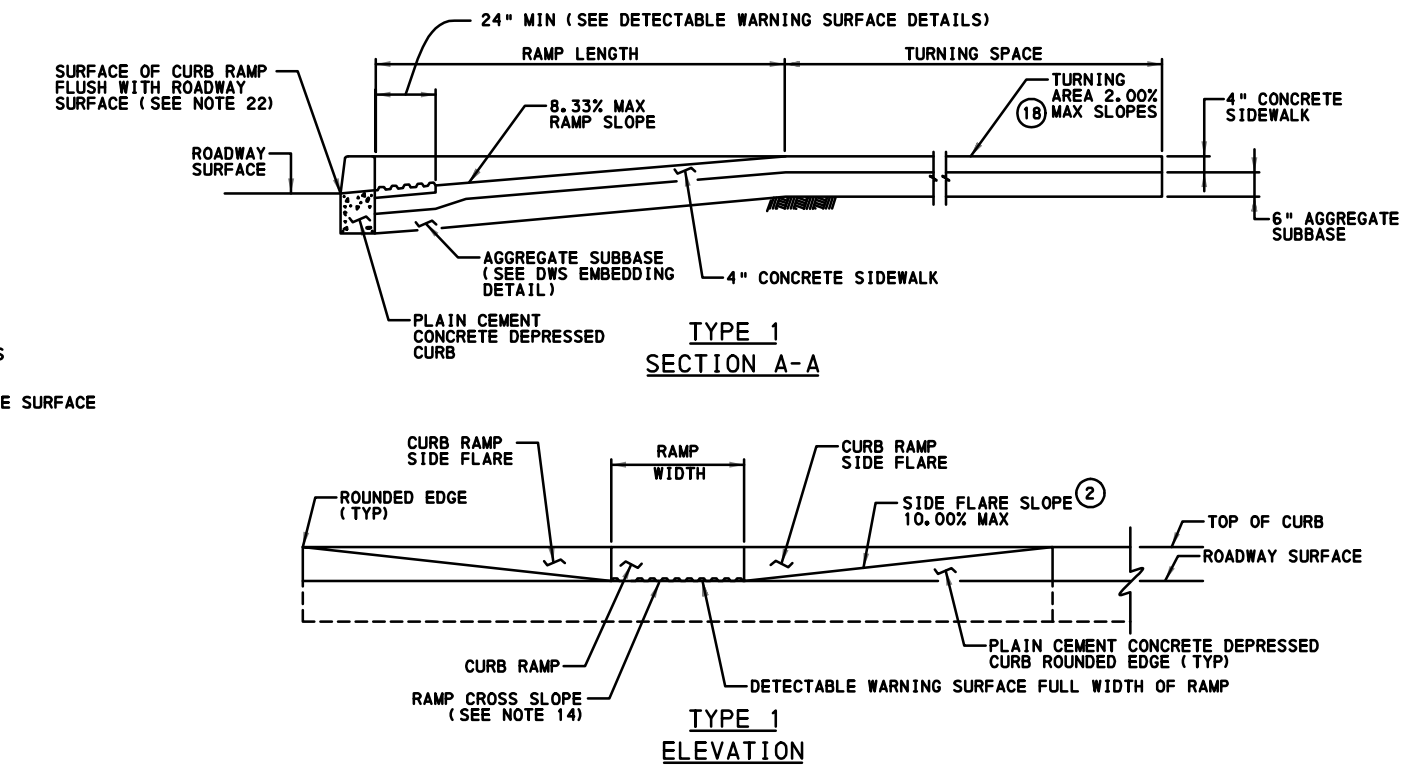
DETAIL- SYNTHETIC TURF FIELD COLLECTOR DRAIN
N.T.S.



DETAIL- SYNTHETIC TURF FIELD FLAT UNDER DRAIN
N.T.S.

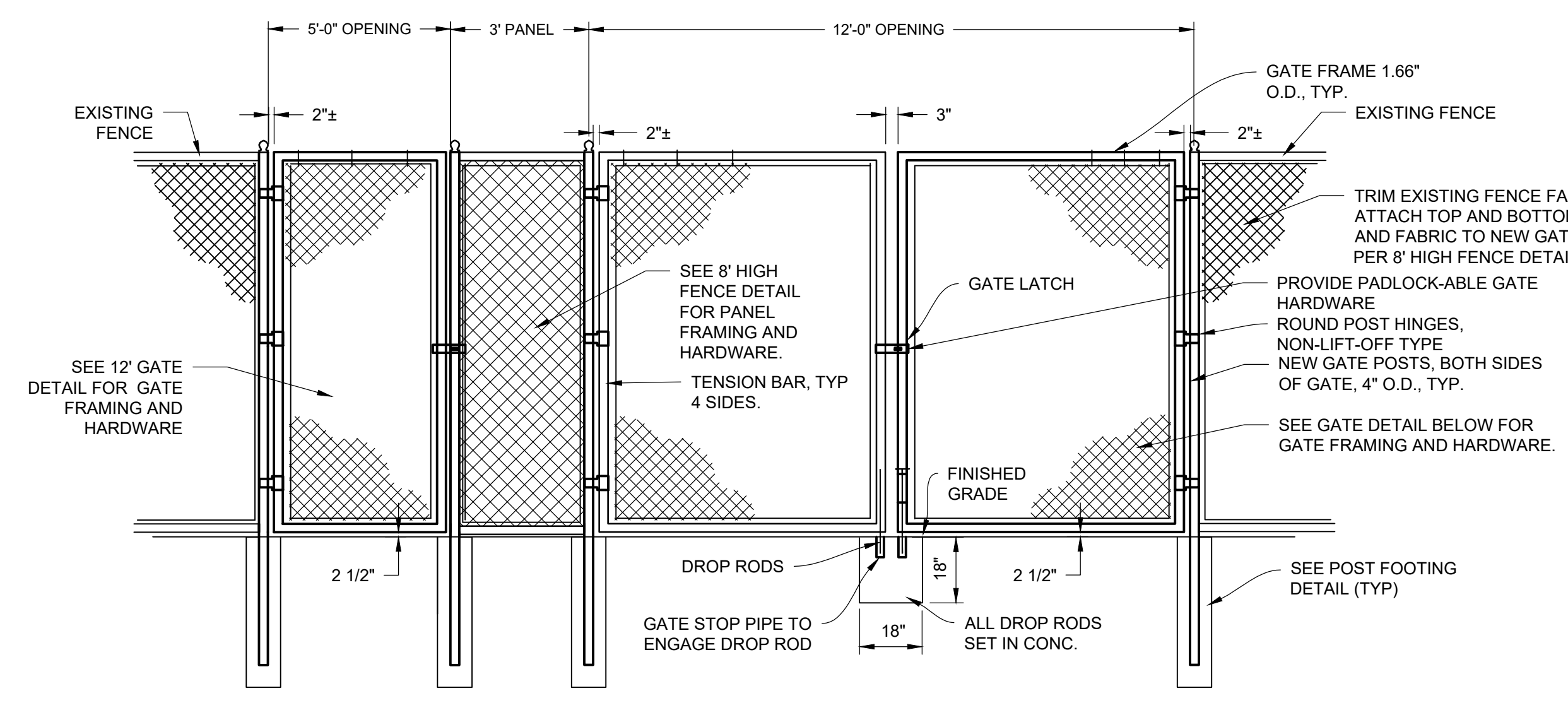


DETAIL- TYPE 1 CURB RAMP
N.T.S.

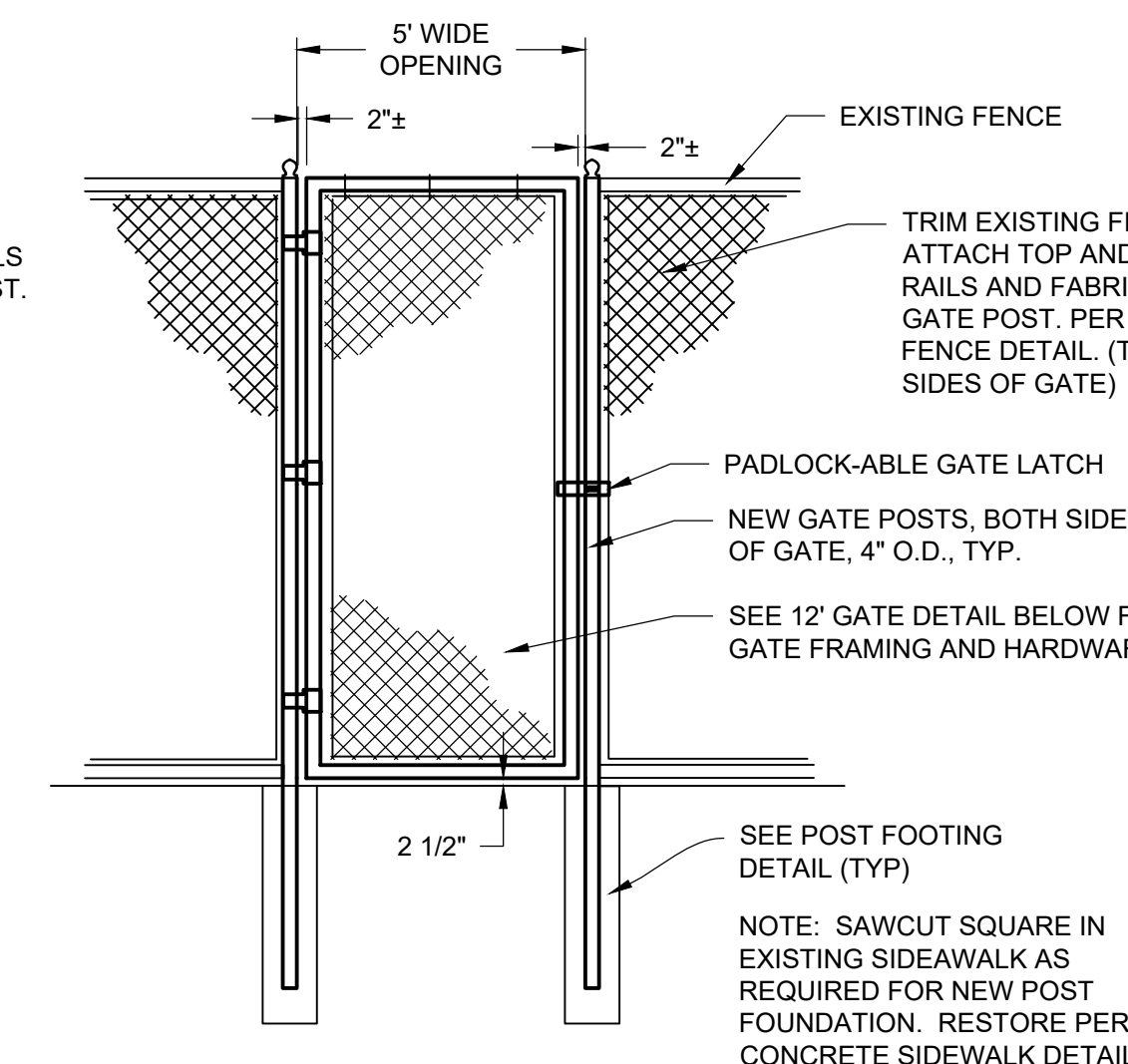


PERCENT SLOPE	EQUIVALENT SLOPE
10.000	1:10
8.333	1:12
7.143	1:14
6.000	1:17
5.000	1:20
4.000	1:25
3.000	1:33

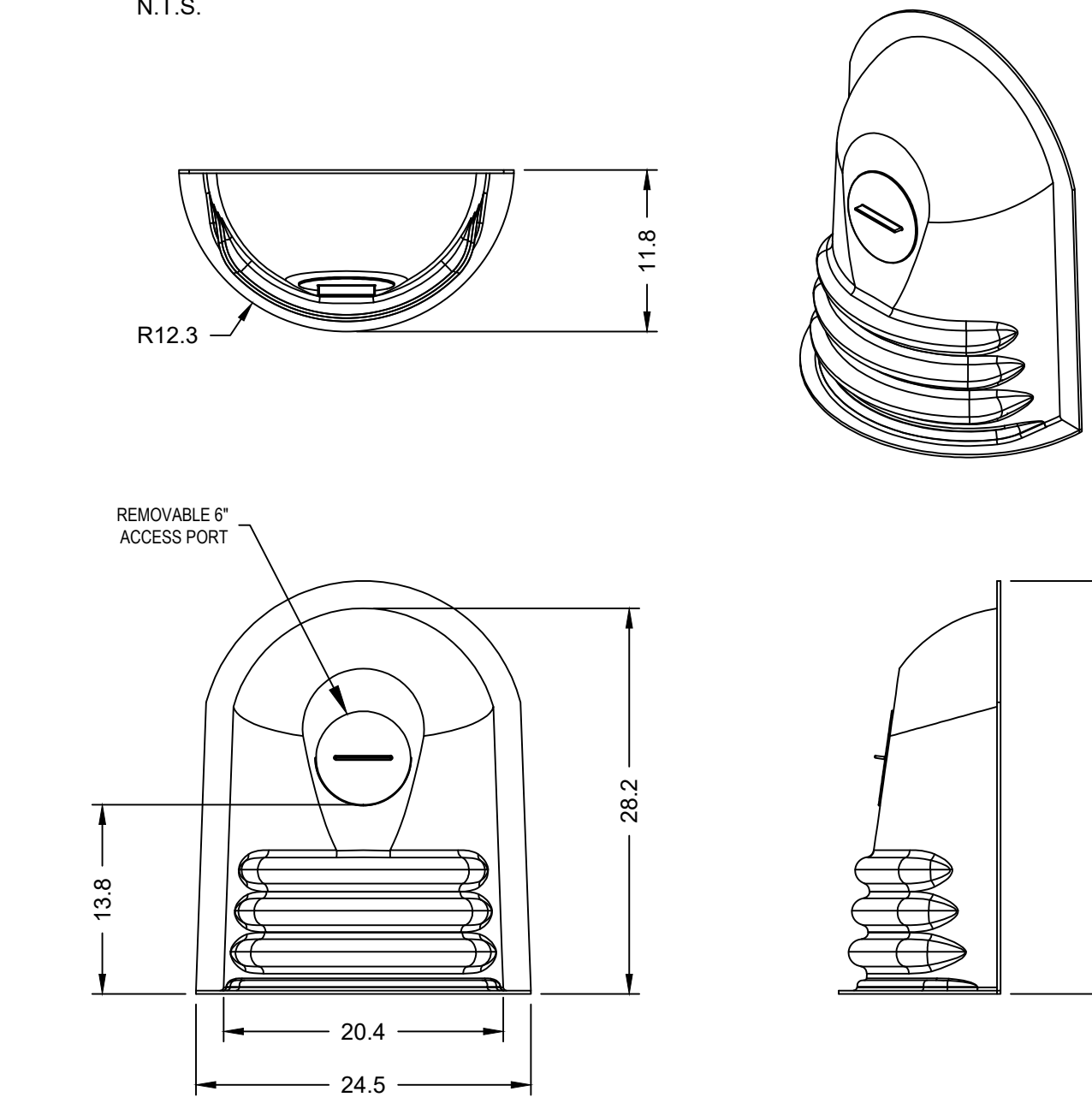
EQUIVALENT SLOPES



DETAIL- 12\"/>



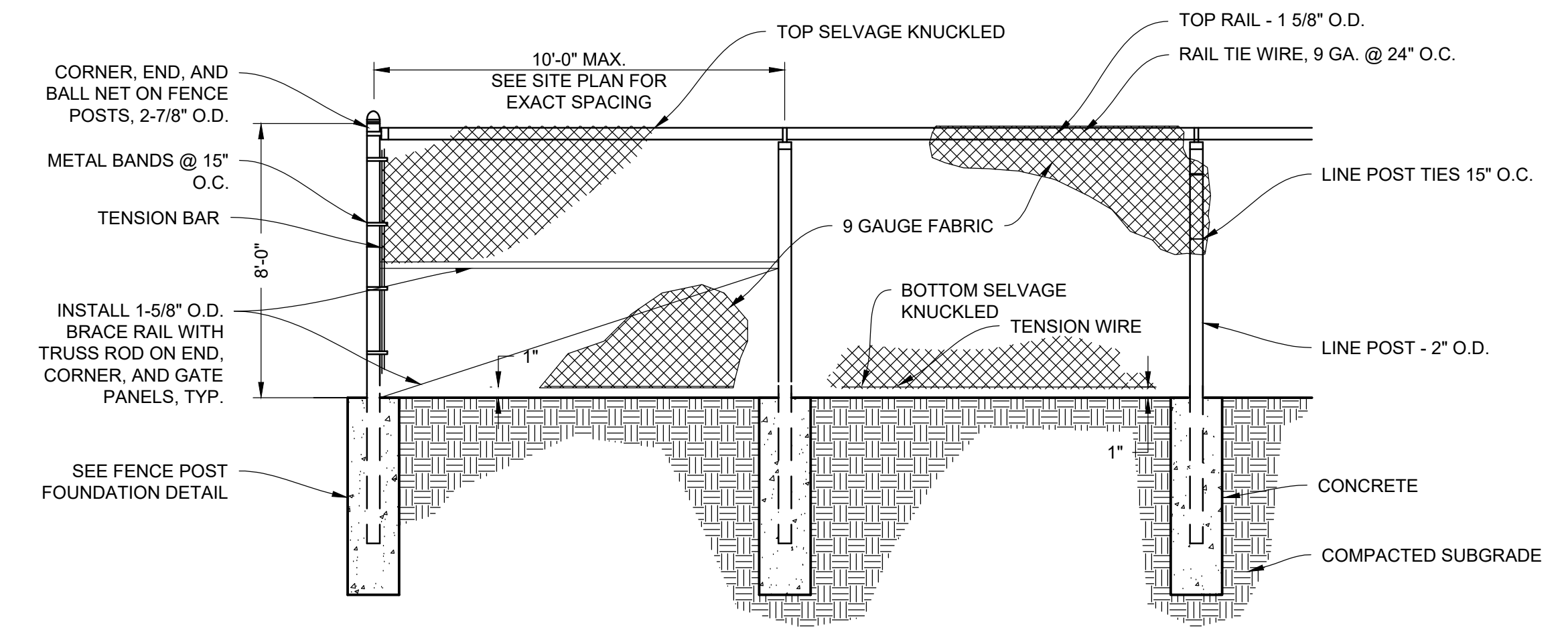
DETAIL- 5\"/>



DETAIL- 18\"/>

- FENCING NOTES:**
- FENCE FABRIC TO BE CLASS 2B VINYL COATED, 2\"/>
 - ALL NEW FRAMING MEMBERS (INCLUDING POSTS) AND HARDWARE TO BE POWDER COATED (BLACK) PER SPECIFICATIONS.
 - GATE FRAME JOINTS TO BE WELDED TO MAKE A RIGID FRAME.
 - GATE DROP RODS TO BE SET IN CONCRETE.
 - FASTEN FENCE FABRIC TO PLAYING FIELD SIDE OF POSTS.
 - TOP AND BOTTOM SELVAGE TO BE KNUCKLED.
 - PROVIDE COMPLETE SHOP DRAWINGS FOR FENCES, GATES, AND BACKSTOP.

DETAIL- CHAIN LINK FENCE
N.T.S.



DETAIL- CHAIN LINK FENCE
N.T.S.

REVISIONS

ISSUE	DATE	DESCRIPTION
1	01/09/23	PWD PCSM/NPDES SUBMISSION
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REVIEWED BY:

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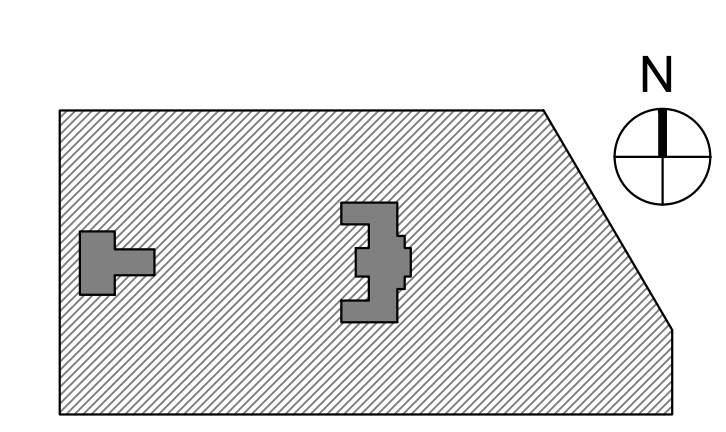
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CITY OF PHILADELPHIA
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1515 ARCH STREET
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PHILADELPHIA PENNSYLVANIA

PROJECT TITLE
KINGSESSING RECREATION CENTER

KEY PLAN



DRAWING TITLE
SITE DETAILS

PROJECT NO. KLMLX21003	DRAWING NO. CS604-R.2
DATE 08/28/23	SCALE AS NOTED
DRAWN BY SDL	CHECKED BY MJM

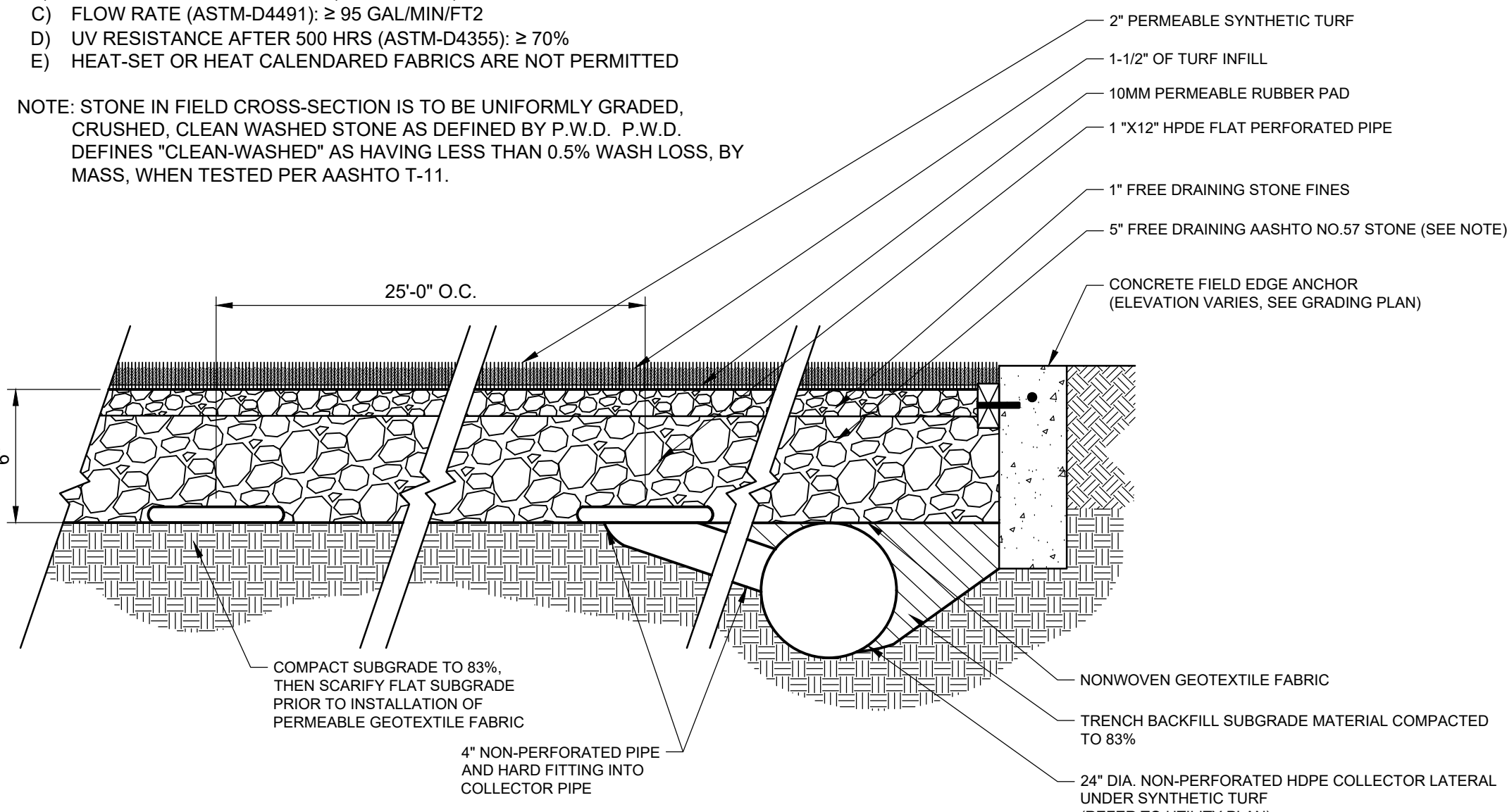
PACKAGE 2 - IFB
NOT FOR CONSTRUCTION
08/28/23
NPDES PERMIT #PAC510302
PWD TRACKING #FY22-KING-6800-01

NOTE: ALL DIMENSIONS AND CONDITIONS SHALL BE VERIFIED BY THE CONTRACTOR AT THE SITE BEFORE PROCEEDING WITH THE WORK.

NOTE: GEOTEXTILE IS TO CONSIST OF POLYPROPYLENE FIBERS AND TO MEET THE FOLLOWING SPECIFICATIONS:

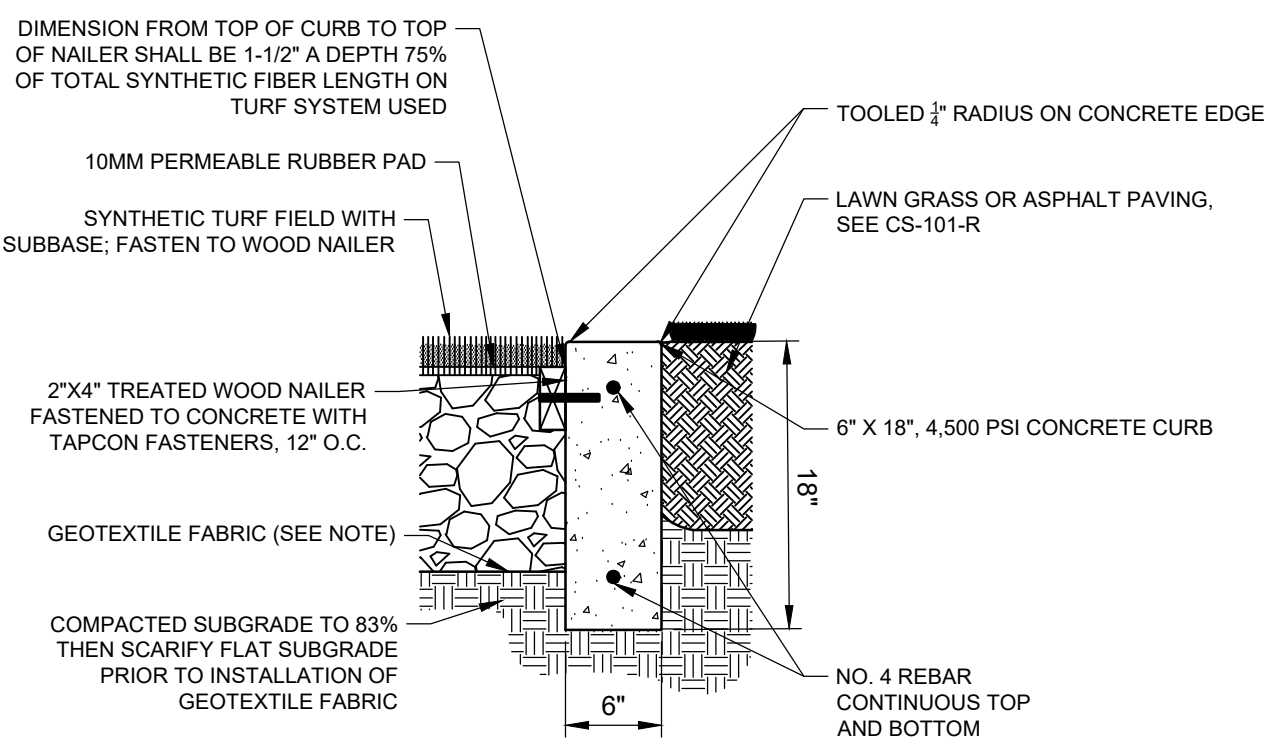
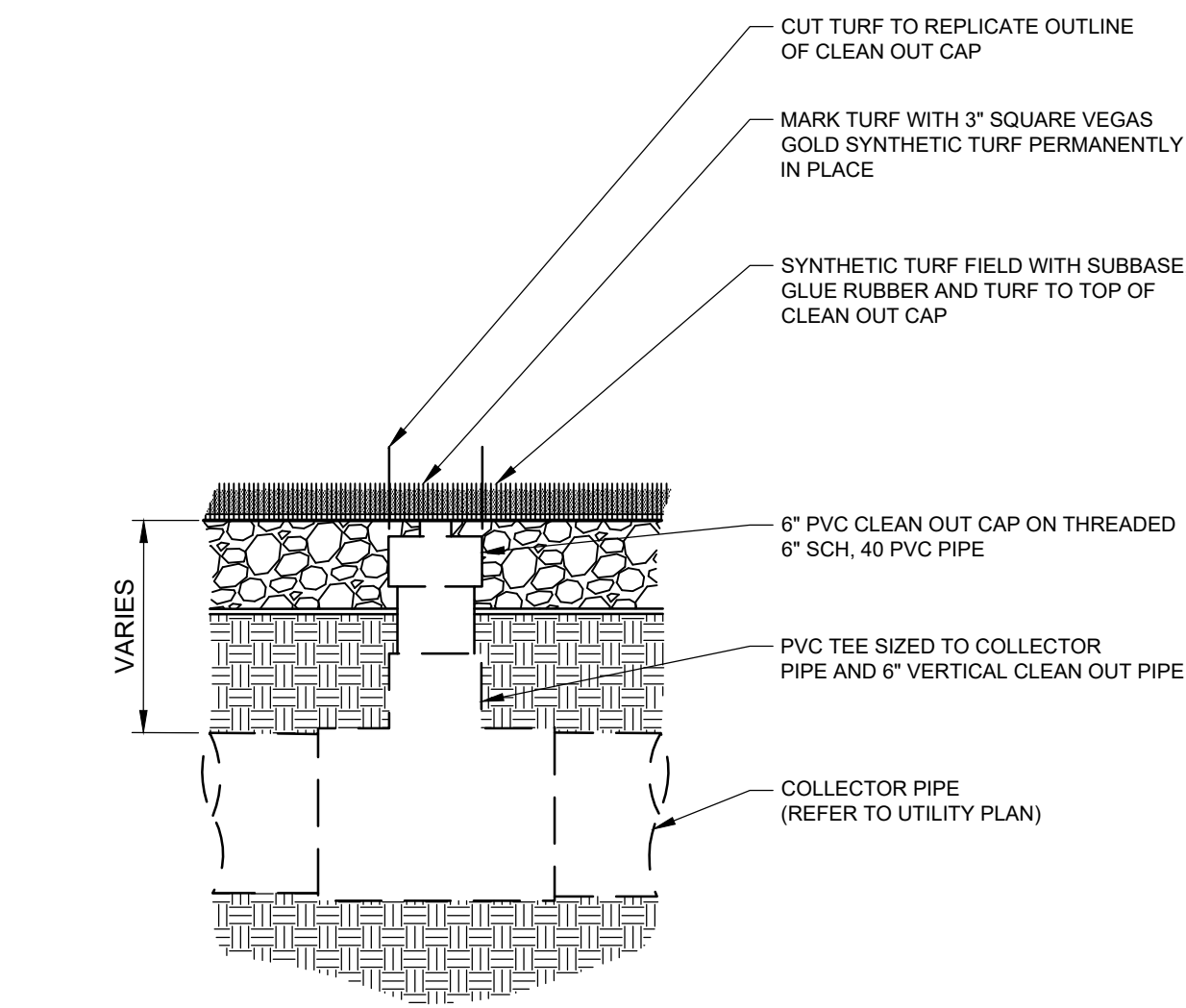
- A) GRAB TENSILE STRENGTH (ASTM-D4632): ≥ 120 LBS
- B) MULLEN BURST STRENGTH (ASTM-D3786): ≥ 225 PSI
- C) FLOW RATE (ASTM-D4491): ≥ 95 GAL/MIN/FT²
- D) UV RESISTANCE AFTER 500 HRS (ASTM-D4355): ≥ 70%
- E) HEAT-SET OR HEAT CALENDARED FABRICS ARE NOT PERMITTED

NOTE: STONE IN FIELD CROSS-SECTION IS TO BE UNIFORMLY GRADED, CRUSHED, CLEAN WASHED STONE AS DEFINED BY P.W.D. P.W.D. DEFINES "CLEAN-WASHED" AS HAVING LESS THAN 0.5% WASH LOSS, BY MASS, WHEN TESTED PER AASHTO T-11.

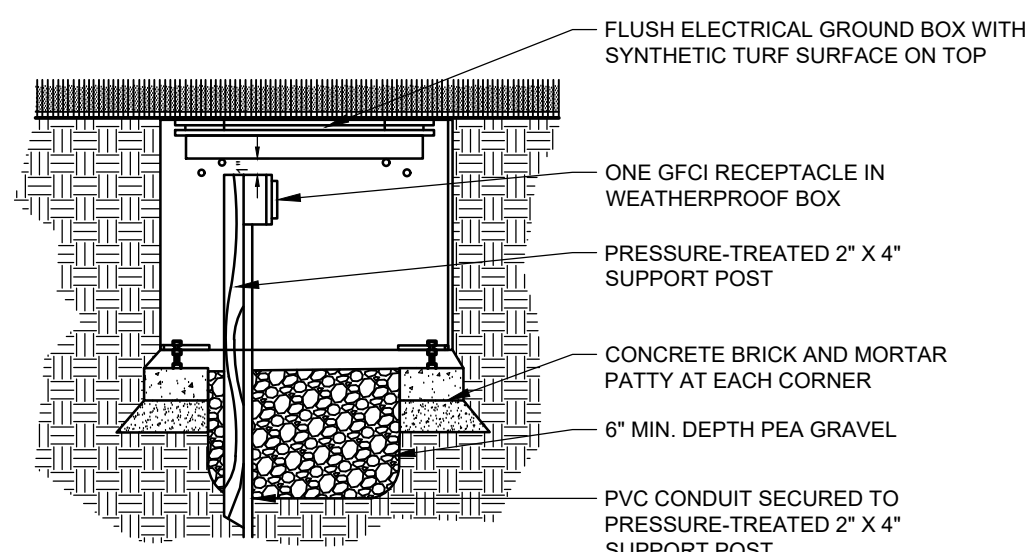


DETAIL- SYNTHETIC FIELD UNDERDRAINAGE
N.T.S.

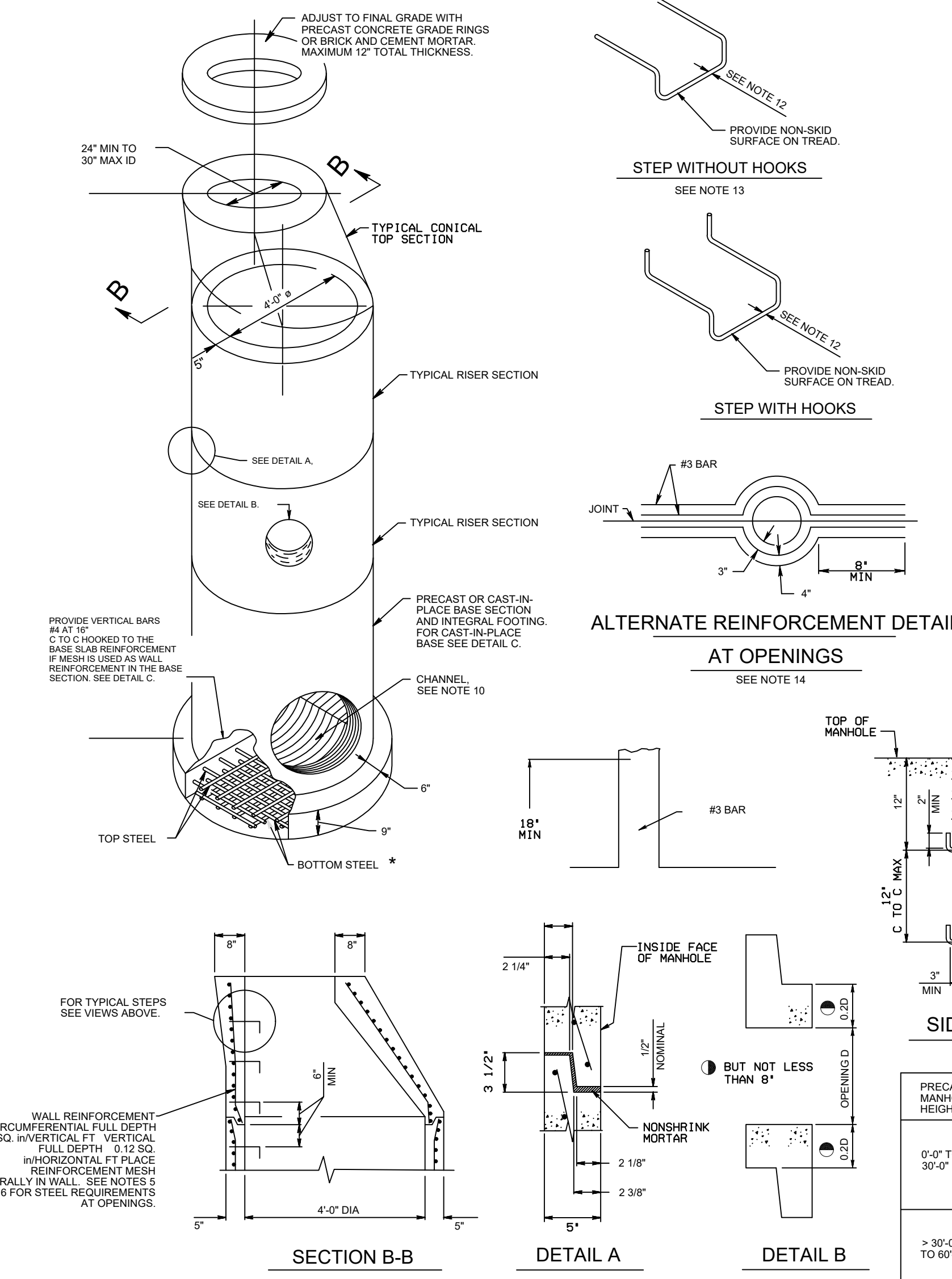
DETAIL- CLEANOUT PORTAL IN SYNTHETIC TURF
N.T.S.



DETAIL- CONCRETE FIELD EDGE ANCHOR
N.T.S.



DETAIL- OUTDOOR GROUND BOX RECEPTACLE
N.T.S.



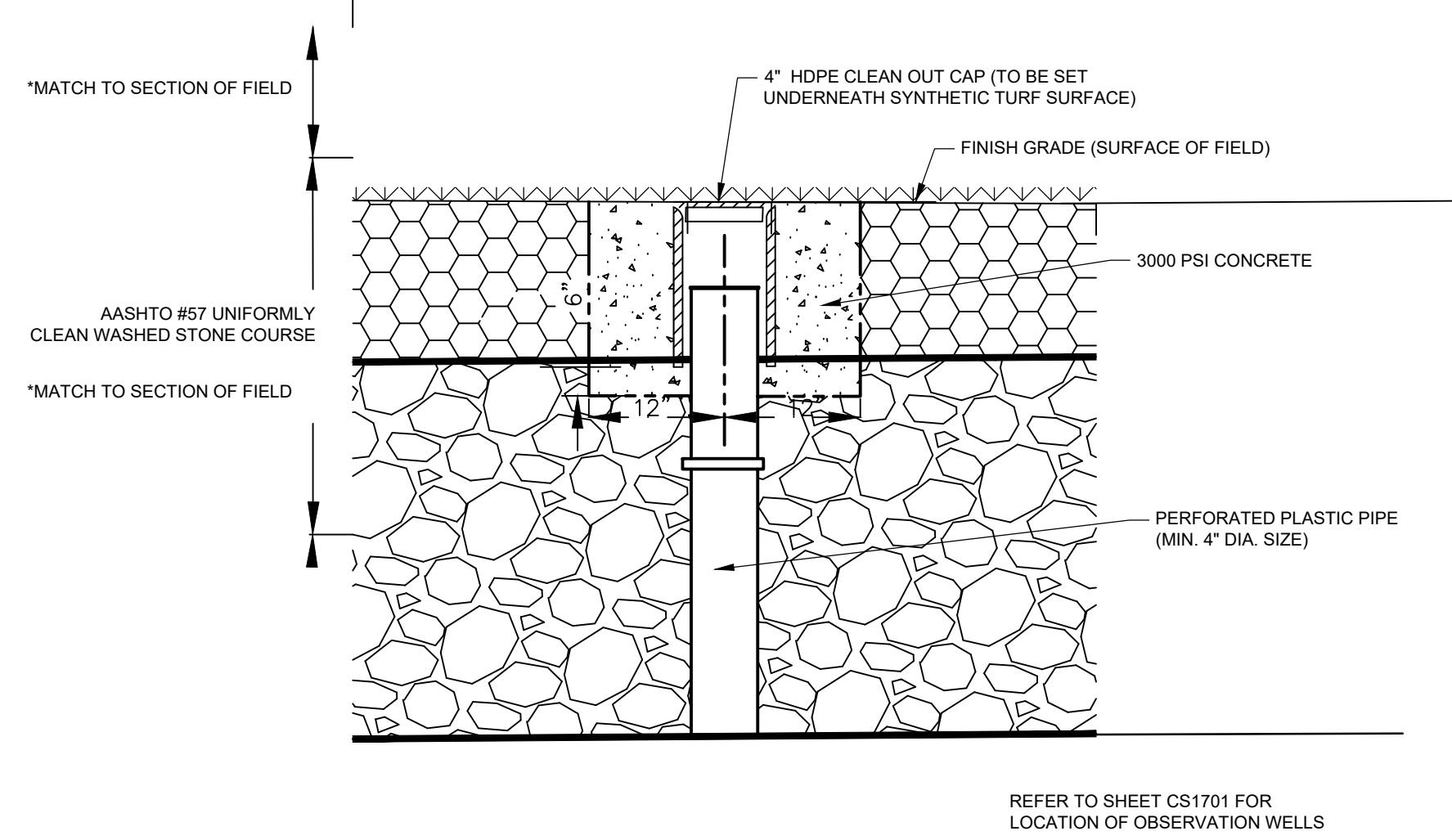
DETAIL- PRECAST STORM MANHOLE
N.T.S.

NOTE: ALL TREATED WOOD TO BE NON-CCA. TOP OF TURF INFILL TO BE FLUSH WITH TOP OF CONCRETE CURB. EXPANSION JOINTS TO BE INSTALLED AT INTERVALS NOT TO EXCEED 160" - SAWCUT CONTROL JOINTS SHALL OCCUR EVERY 20".

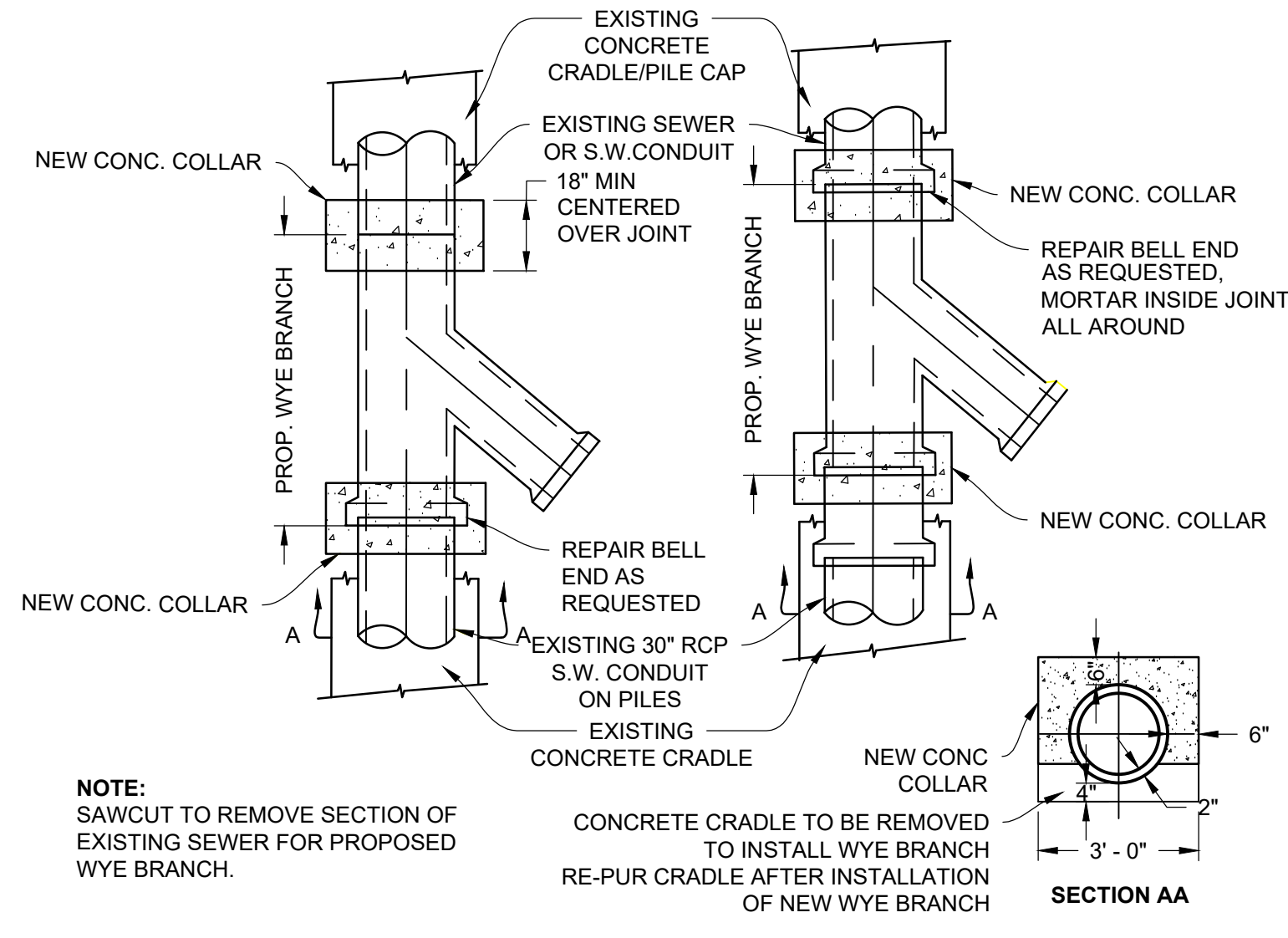
NOTE: NON-WOVEN GEOTEXTILE IS TO CONSIST OF POLYPROPYLENE FIBERS AND TO MEET THE FOLLOWING SPECIFICATIONS:

- A) GRAB TENSILE STRENGTH (ASTM-D4632): ≥ 120 LBS
- B) MULLEN BURST STRENGTH (ASTM-D3786): ≥ 225 PSI
- C) FLOW RATE (ASTM-D4491): ≥ 95 GAL/MIN/FT²
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- E) HEAT-SET OR HEAT CALENDARED FABRICS ARE NOT PERMITTED

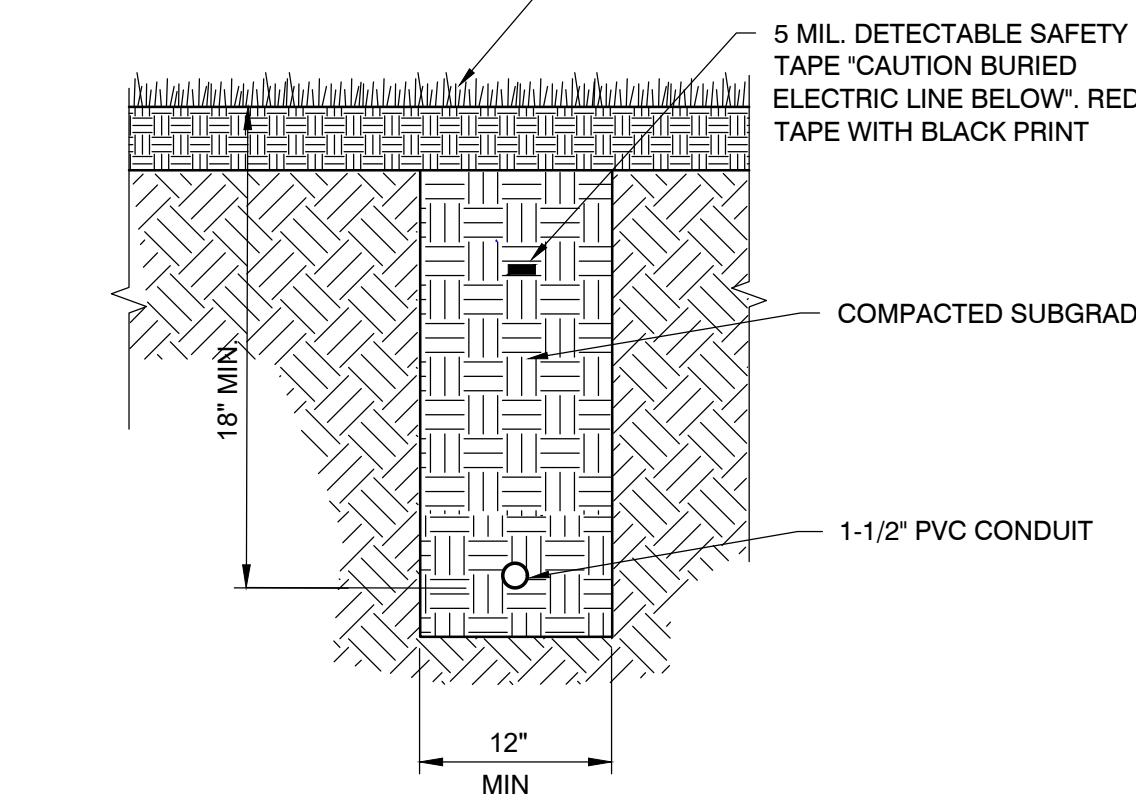
NOTE: STONE IN FIELD CROSS-SECTION IS TO BE UNIFORMLY GRADED, CRUSHED, CLEAN WASHED STONE AS DEFINED BY P.W.D. P.W.D. DEFINES "CLEAN-WASHED" AS HAVING LESS THAN 0.5% WASH LOSS, BY MASS, WHEN TESTED PER AASHTO T-11.



DETAIL- OBSERVATION WELL IN SYNTHETIC TURF
N.T.S.



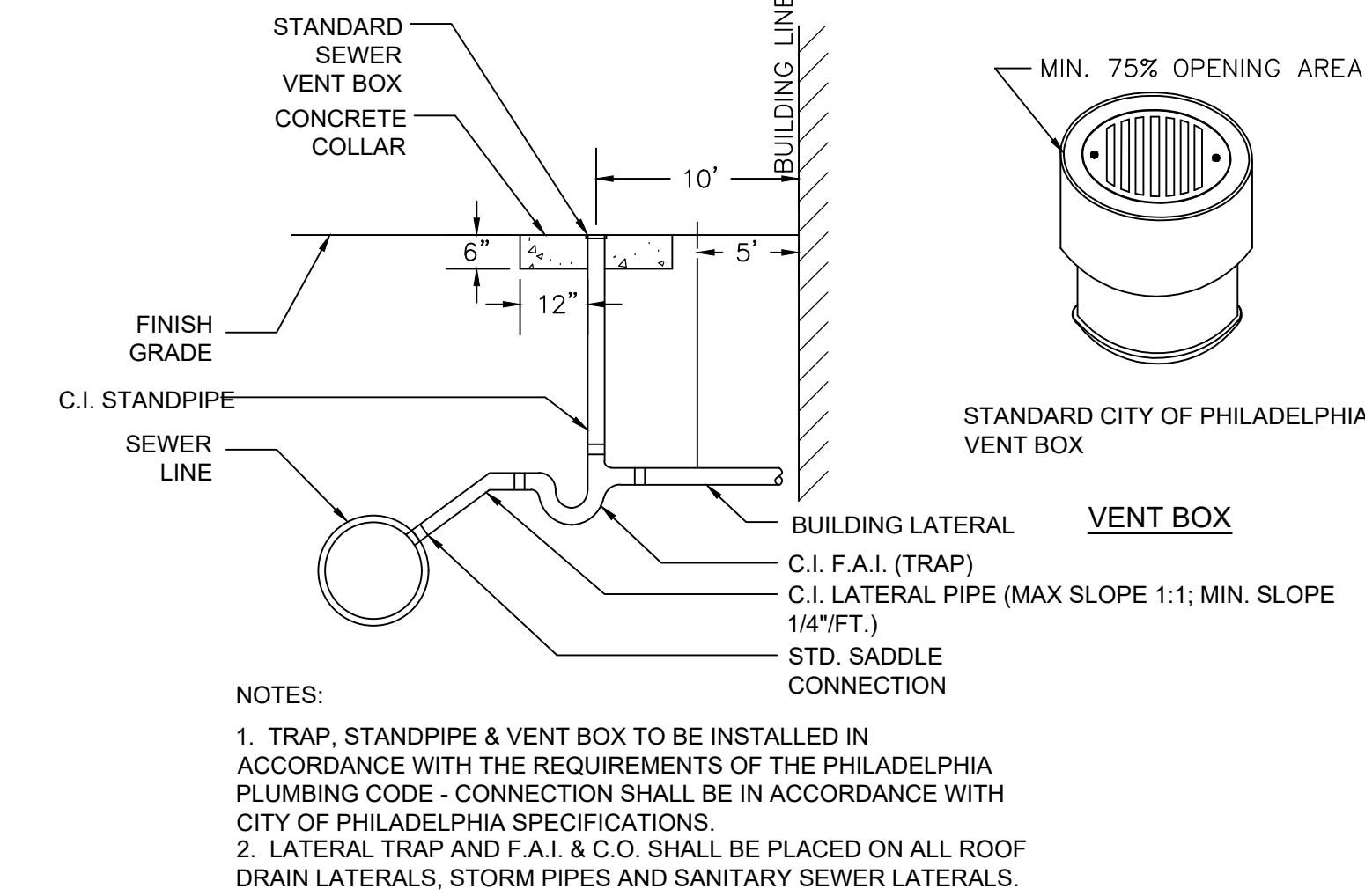
DETAIL- WYE CONNECTION
N.T.S.



ELECTRICAL CONDUIT NOTES:

- PROVIDE 1-1/2" PVC CONDUIT, MINIMUM 18" BELOW GRADE.
- REFER TO MEP PLANS FOR ELECTRICAL CONNECTION AT EXISTING AND RELOCATED BUILDINGS.
- PROVIDE APPROPRIATE UNDERGROUND WARNING TAPE.
- AFTER INSTALLATION, PROVIDE ENGINEER AND CITY AS-BUILT DRAWINGS INDICATING EXACT LOCATION, VERTICAL AND HORIZONTAL, OF CONDUIT.

DETAIL- ELECTRICAL CONDUIT
N.T.S.



DETAIL- LATERAL TRAP, FAI, AND CLEANOUT
N.T.S.

REVISIONS

ISSUE	DATE	DESCRIPTION
1	01/09/23	PWD PCSM/NPDES SUBMISSION
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REVIEWED BY:
PROJECT COORDINATOR:
REALS:



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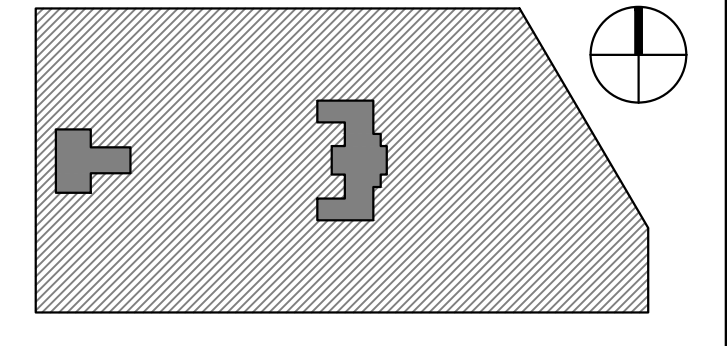
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1515 ARCH STREET
11TH FLOOR, ONE PARKWAY BUILDING
PHILADELPHIA, PENNSYLVANIA

KINGSESSING RECREATION CENTER



SITE DETAILS

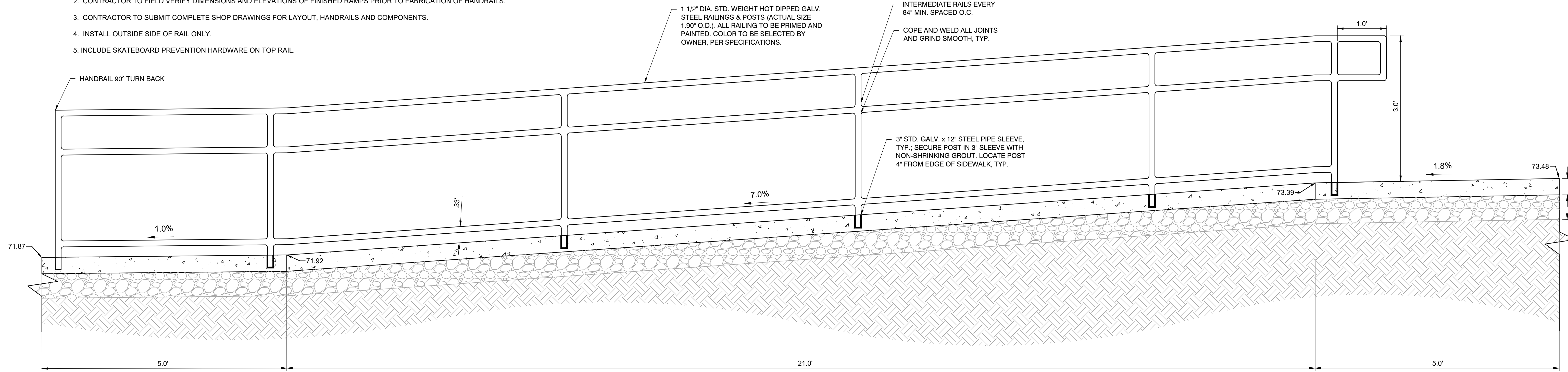
PROJECT NO.	KLMLX21003	DRAWING NO.	CS605-R.2
DATE	08/28/23	SCALE	AS NOTED
DRAWN BY	SDL	CHECKED BY	MJM

NOTE: ALL DIMENSIONS AND CONDITIONS SHALL BE VERIFIED BY THE CONTRACTOR AT THE SITE BEFORE PROCEEDING WITH THE WORK.

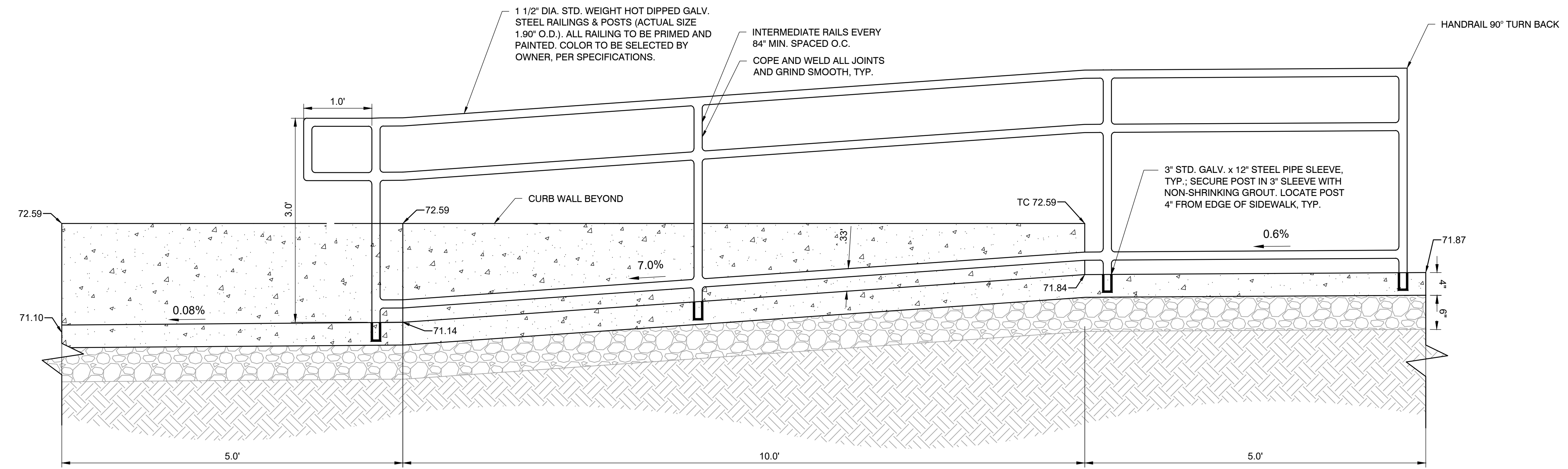
PACKAGE 2 - IFB
NOT FOR CONSTRUCTION
08/28/23
NPDES PERMIT #PAC510302
PWD TRACKING #FY22-KING-6800-01

HANDRAIL NOTES:

1. ALL HANDRAILS AND HARDWARE TO BE HOT DIPPED GALVANIZED, PRIMED AND PAINTED. COLOR TO BE DETERMINED BY OWNER, PER SPECIFICATIONS.
2. CONTRACTOR TO FIELD VERIFY DIMENSIONS AND ELEVATIONS OF FINISHED RAMPS PRIOR TO FABRICATION OF HANDRAILS.
3. CONTRACTOR TO SUBMIT COMPLETE SHOP DRAWINGS FOR LAYOUT, HANDRAILS AND COMPONENTS.
4. INSTALL OUTSIDE SIDE OF RAIL ONLY.
5. INCLUDE SKATEBOARD PREVENTION HARDWARE ON TOP RAIL.



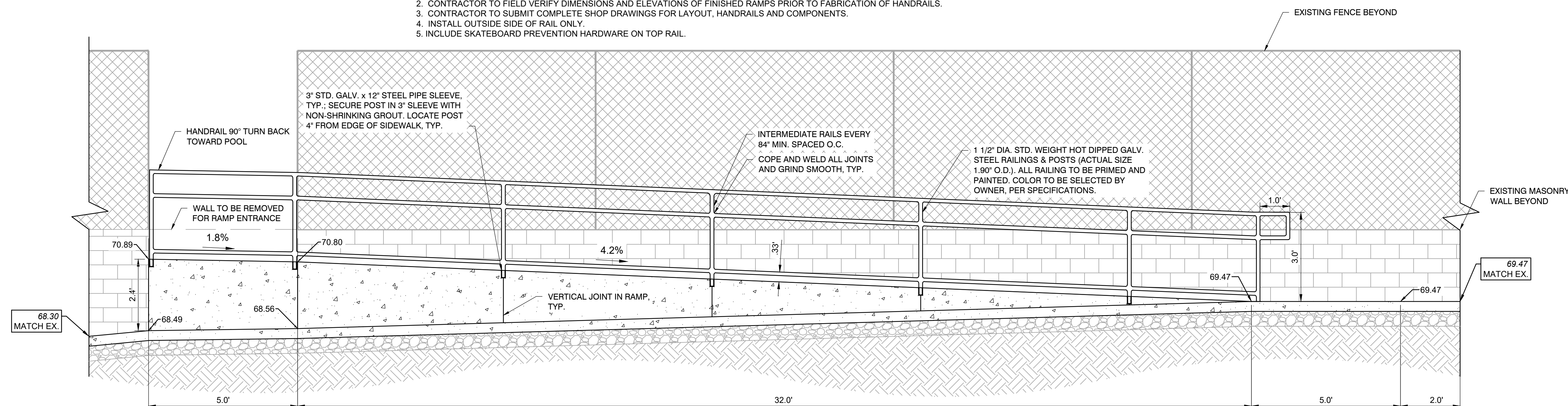
DETAIL- PLAYGROUND RAMP AND RAILING
1"=1'



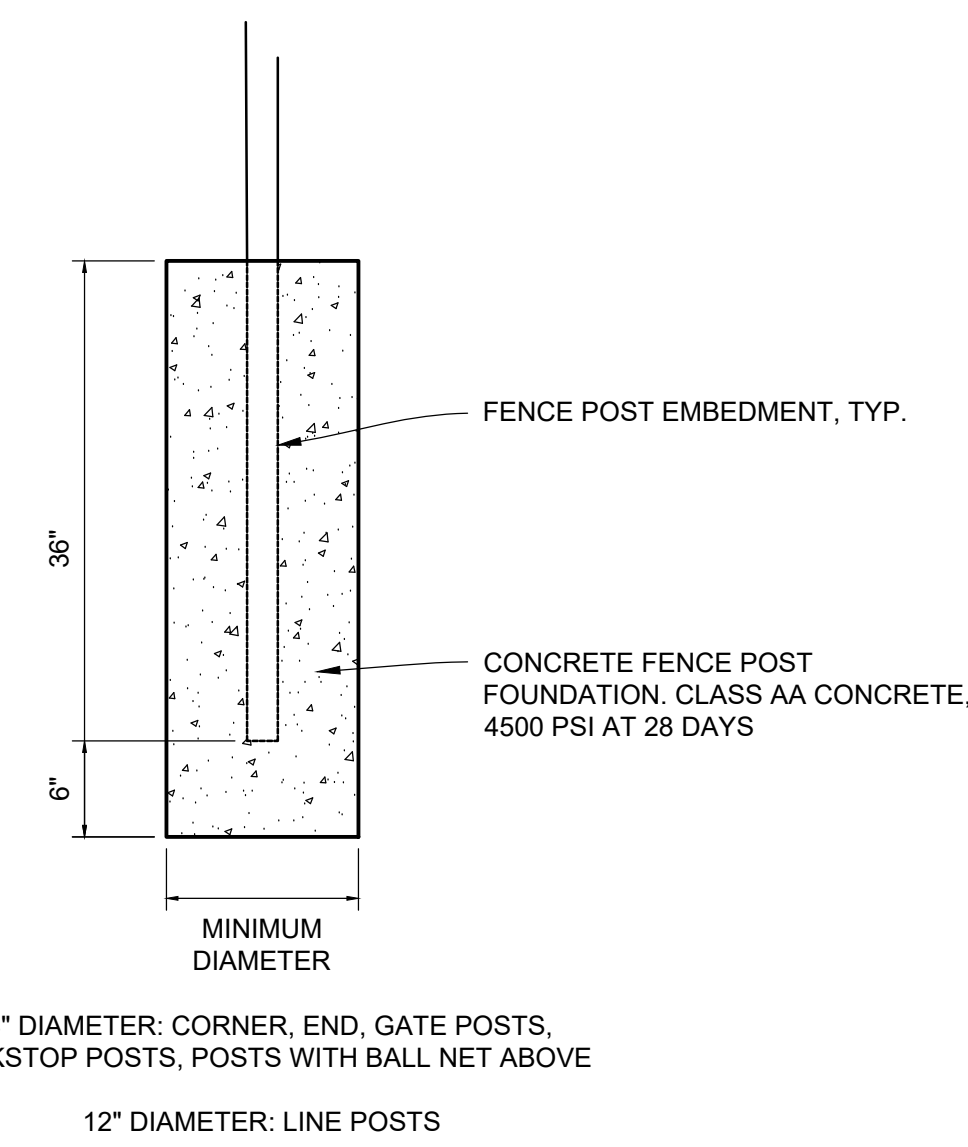
DETAIL- PLAYGROUND RAMP AND RAILING
1"=1'

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3. CONTRACTOR TO SUBMIT COMPLETE SHOP DRAWINGS FOR LAYOUT, HANDRAILS AND COMPONENTS.
4. INSTALL OUTSIDE SIDE OF RAIL ONLY.
5. INCLUDE SKATEBOARD PREVENTION HARDWARE ON TOP RAIL.



DETAIL- POOL DECK RAMP AND RAILING
1"=2'



DETAIL- FENCE POST FOUNDATION
N.T.S.

REVISIONS

ISSUE	DATE	DESCRIPTION
1	01/09/23	PWD PCSM/NPDES SUBMISSION
2	02/14/23	PWD PCSM RESUBMISSION
3	05/24/23	PWD PCSM RESUBMISSION



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PROJECT COORDINATOR:

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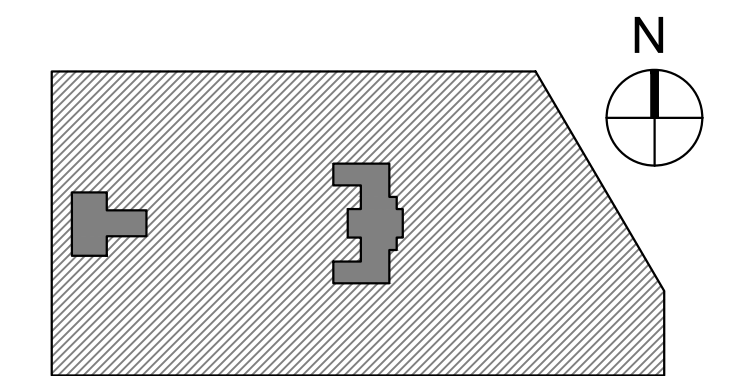
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CITY OF PHILADELPHIA
DEPARTMENT OF PUBLIC PROPERTY
1515 ARCH STREET
11TH FLOOR, ONE PARKWAY BUILDING
PHILADELPHIA PENNSYLVANIA

PROJECT TITLE
KINGSESSING RECREATION CENTER

KEY PLAN



DRAWING TITLE
SITE DETAILS

PROJECT NO.
KLMLX21003

DATE
08/28/23

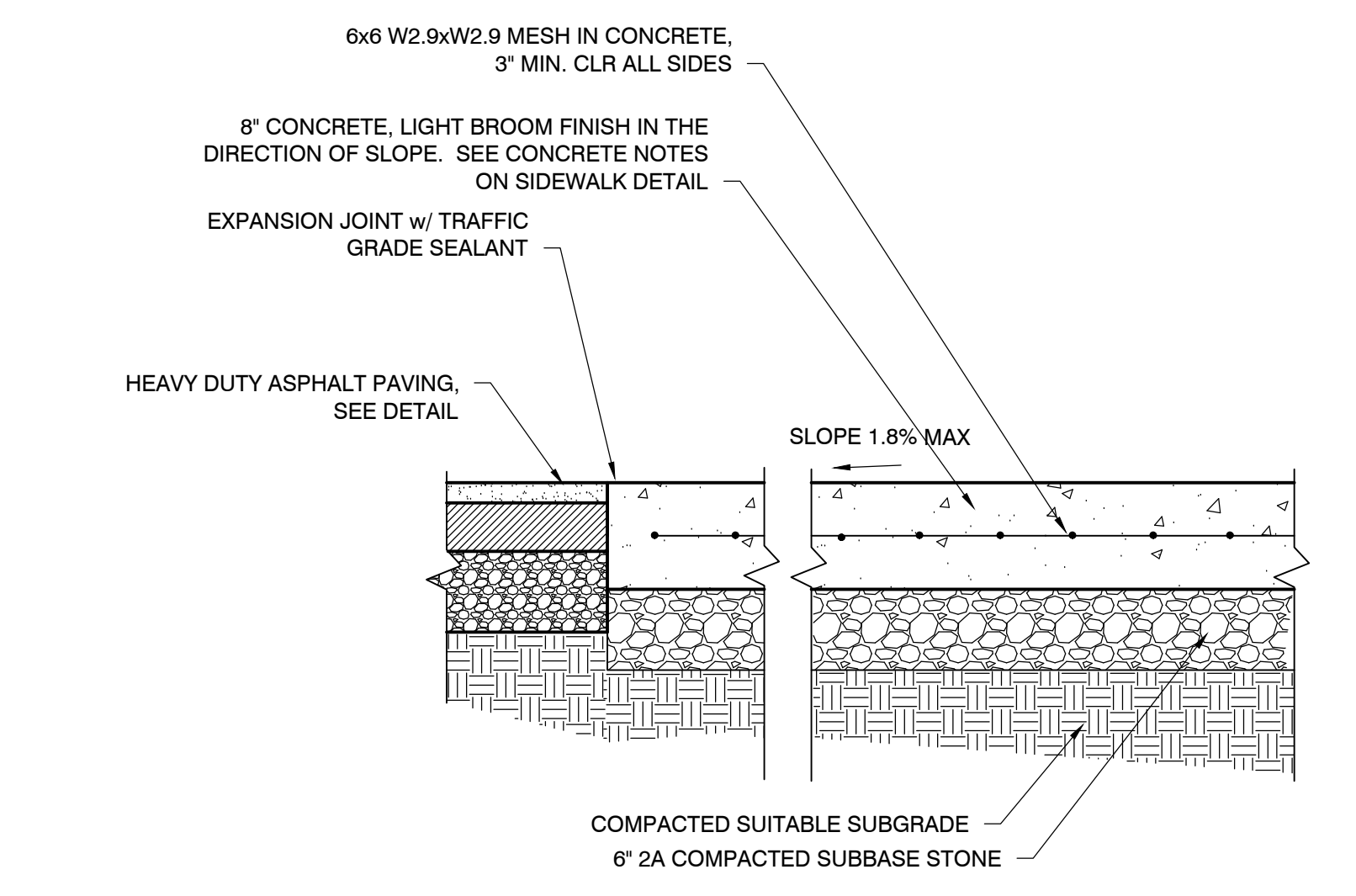
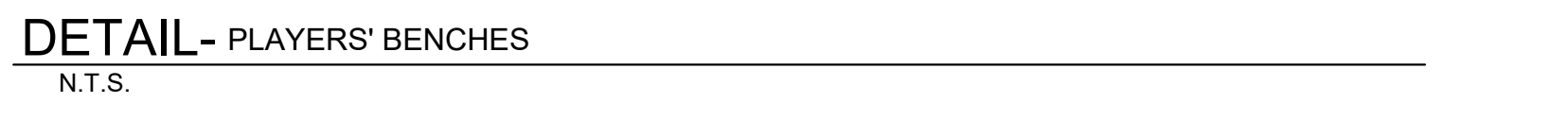
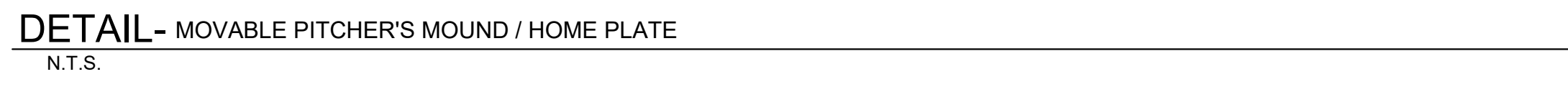
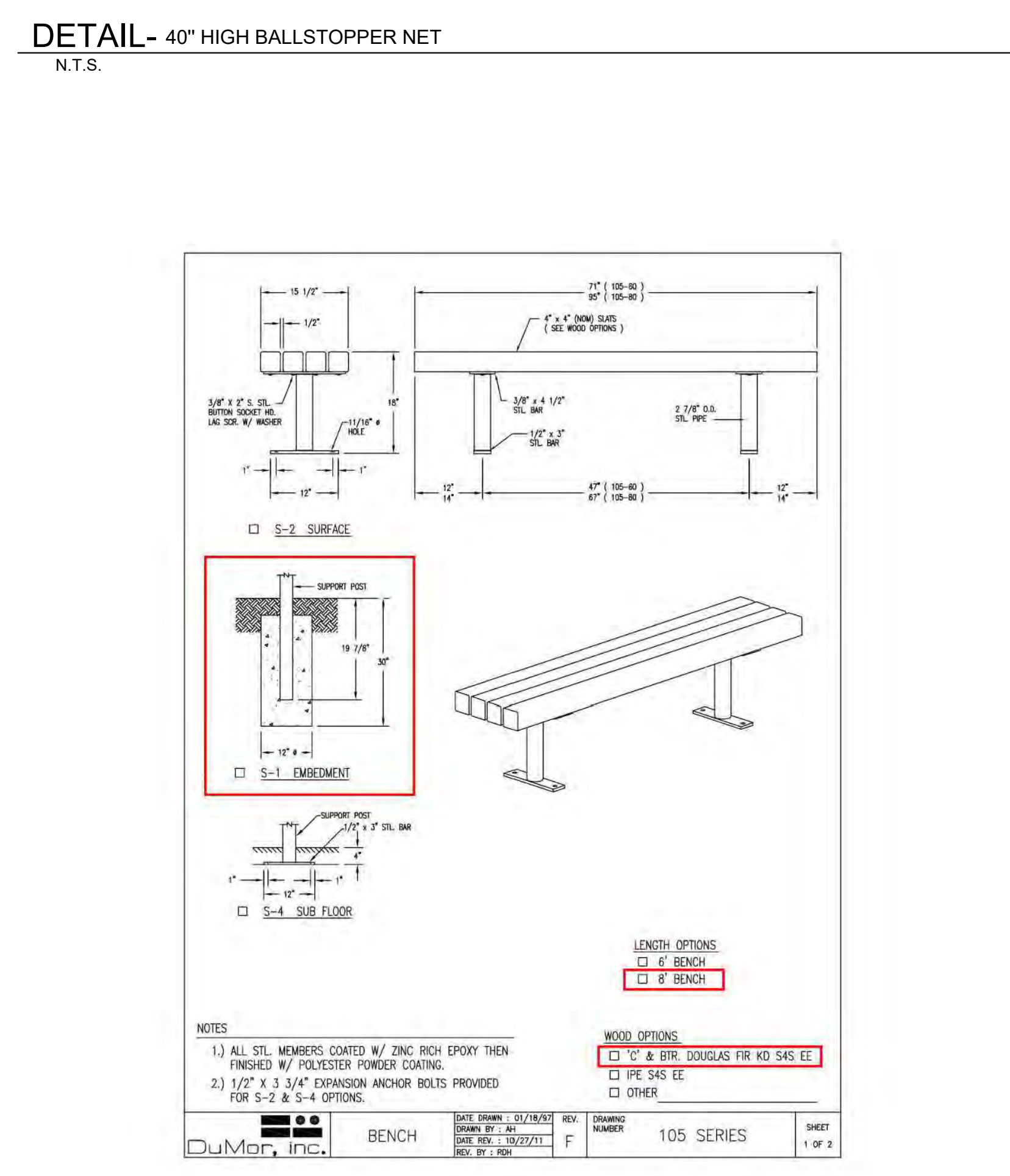
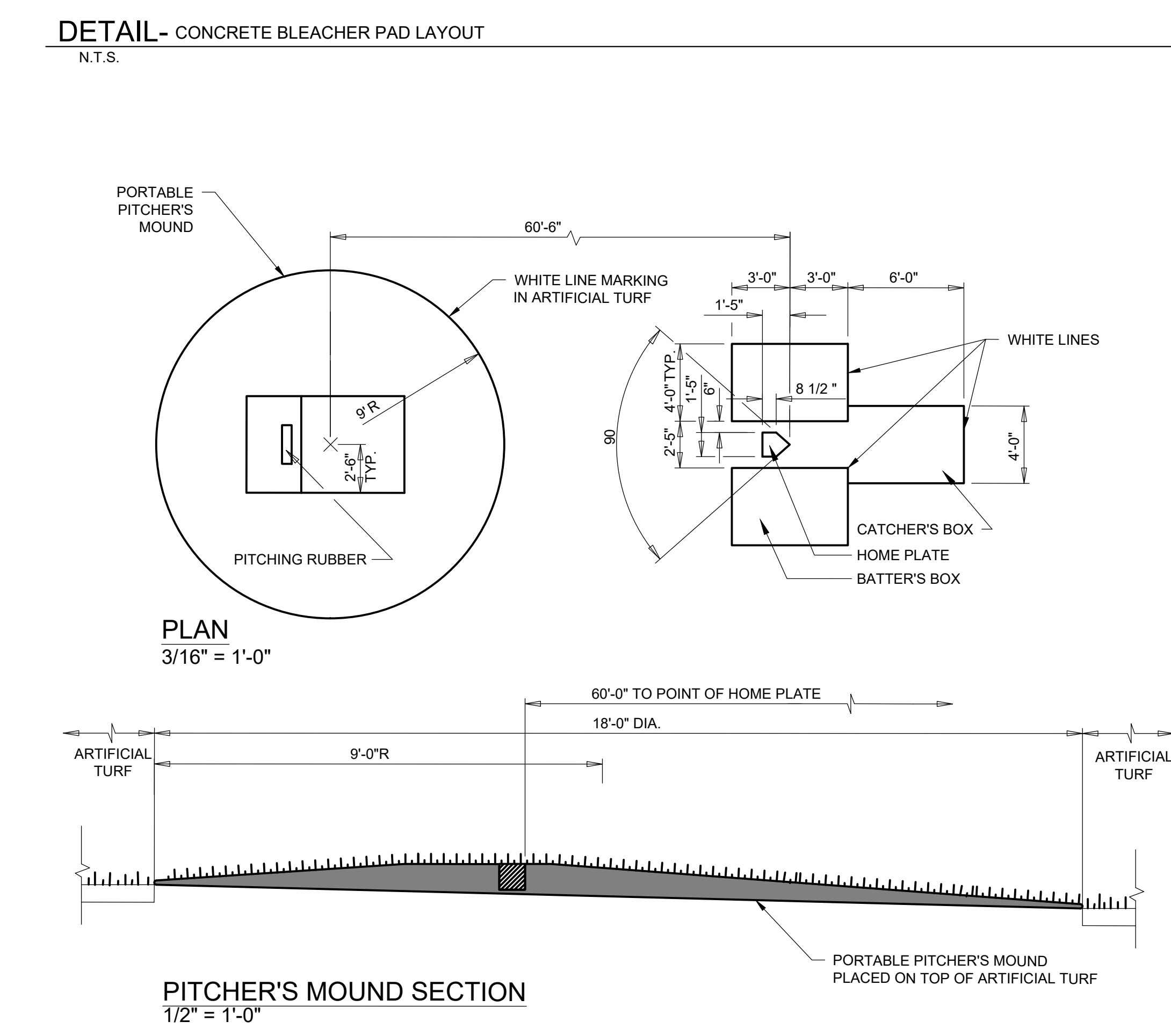
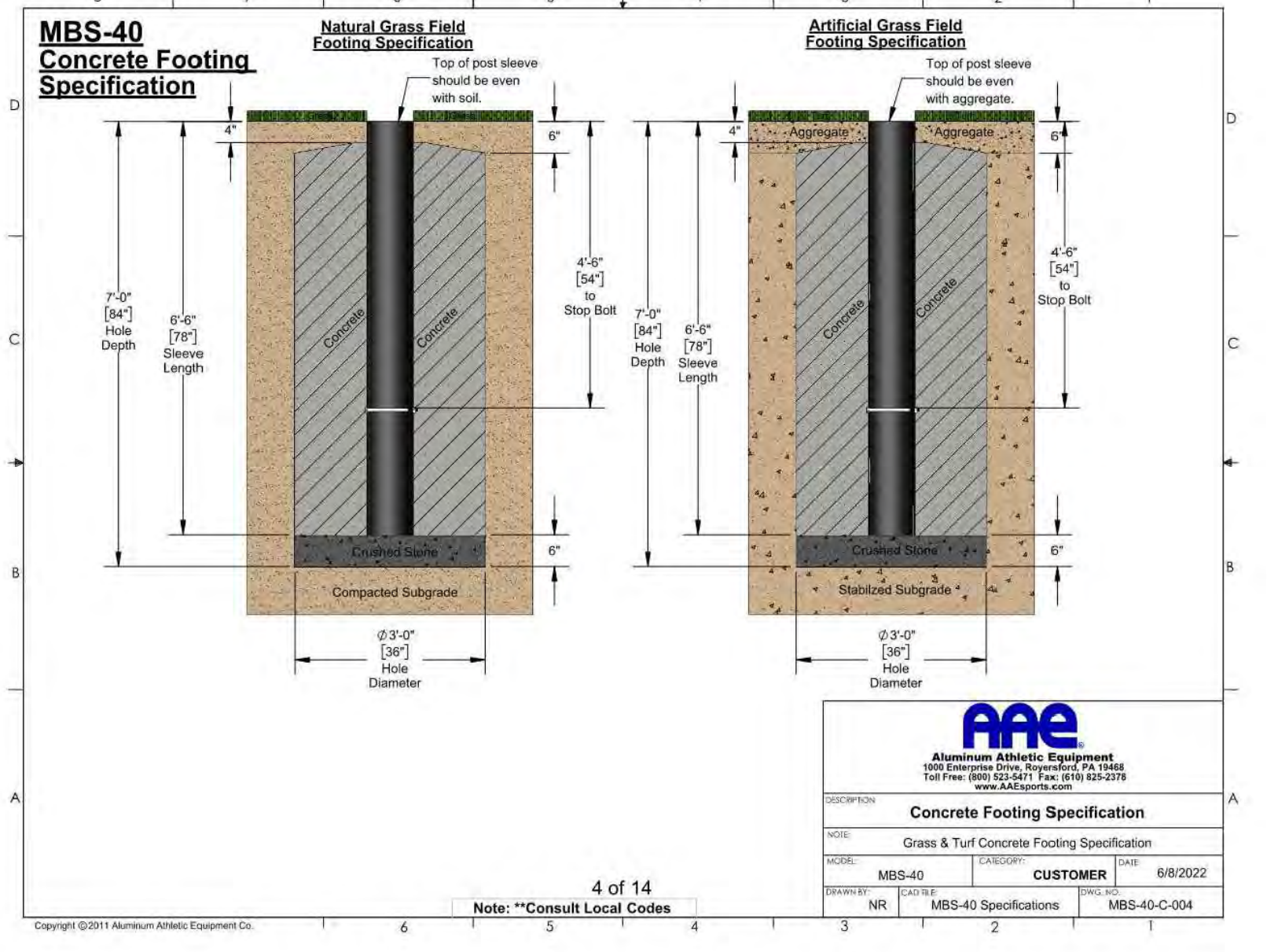
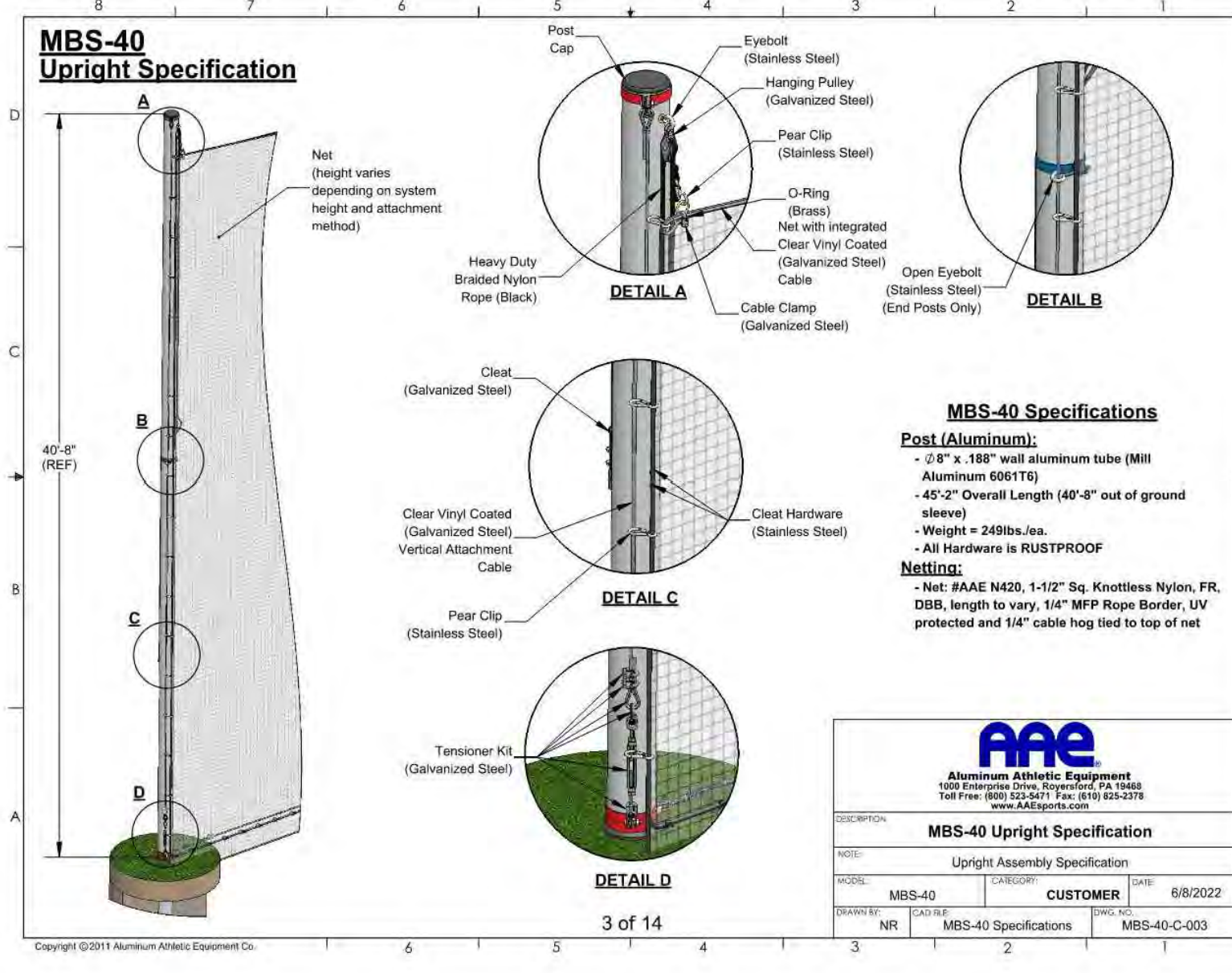
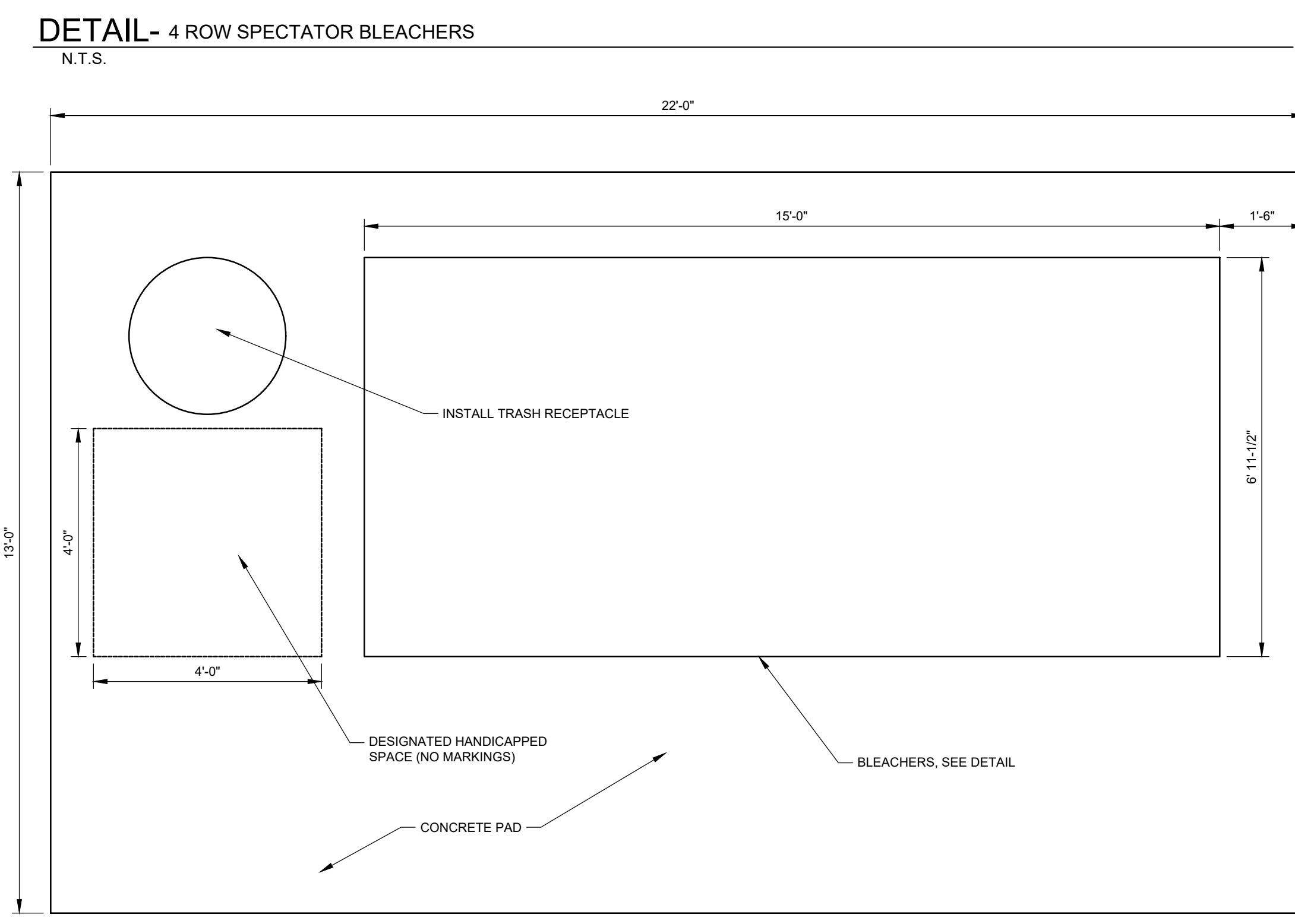
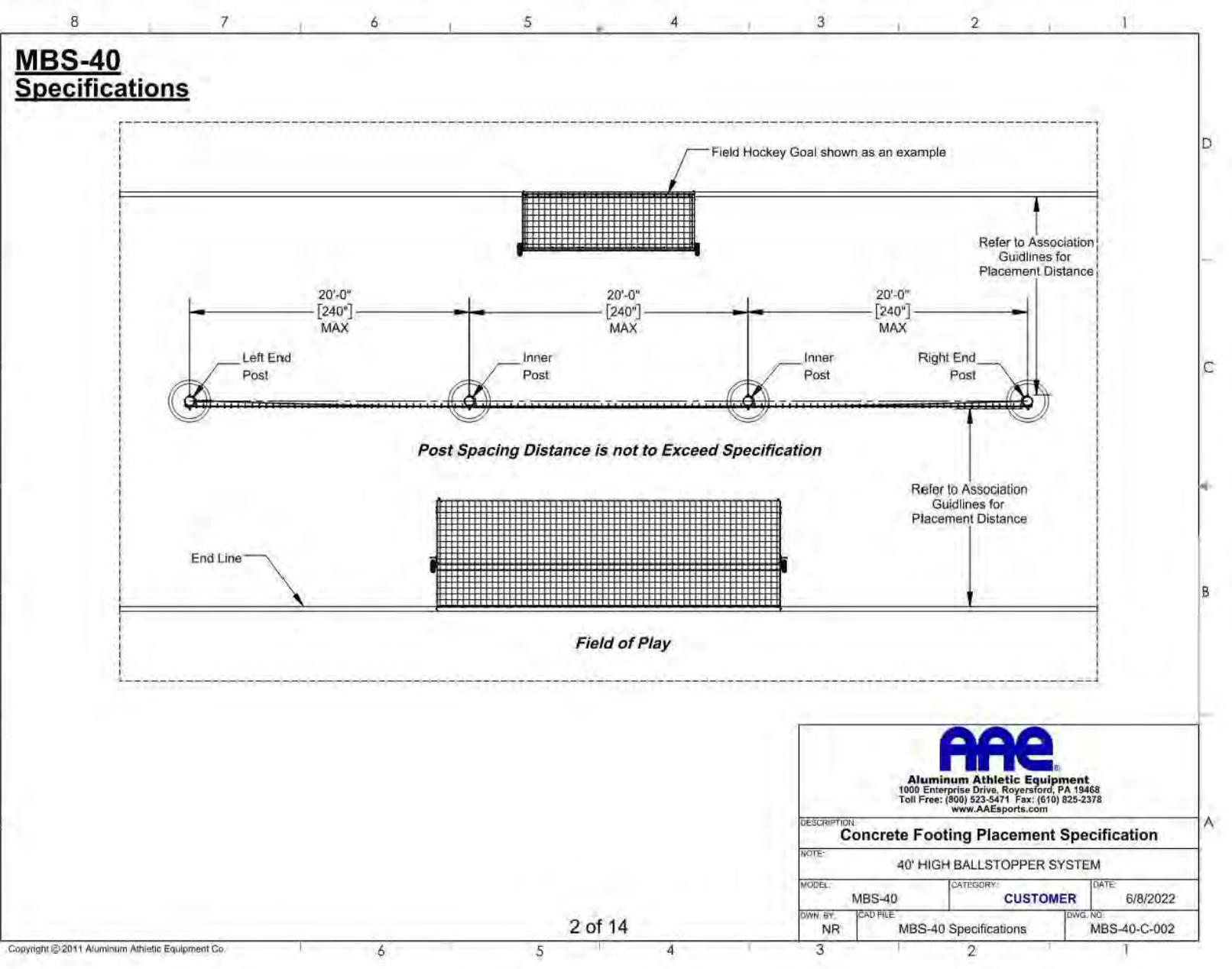
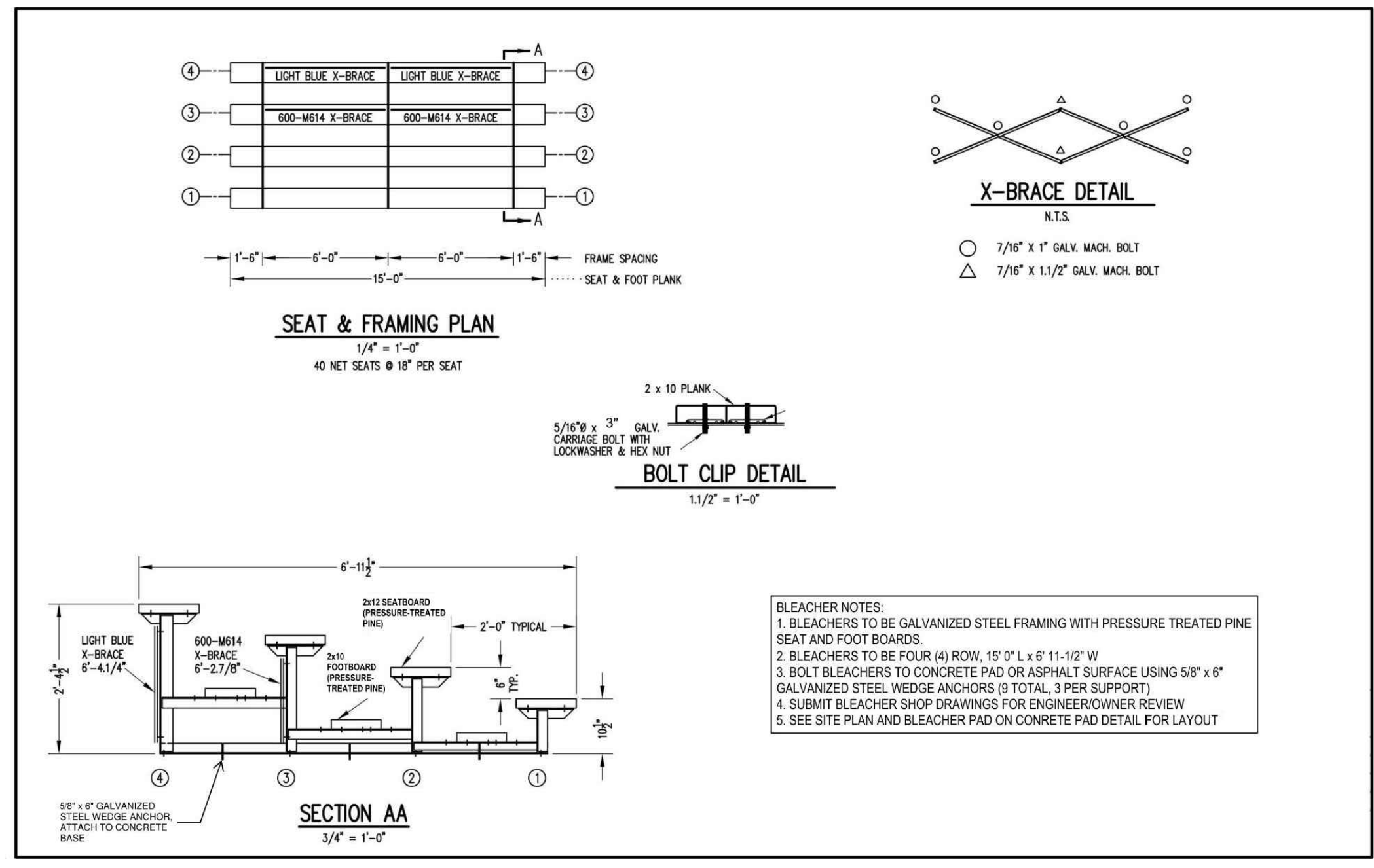
SCALE
AS NOTED

DRAWN BY: SDL
CHECKED BY: MJM

DRAWING NO.
CS607-R.2

PACKAGE 2 - IFB
NOT FOR CONSTRUCTION
08/28/23
NPDES PERMIT #PAC510302
PWD TRACKING #FY22-KING-6800-01

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ISSUE	DATE	DESCRIPTION
1	01/09/23	PWD PCSMNPDES SUBMISSION
2	02/14/23	PWD PCSM RESUBMISSION
3	05/24/23	PWD PCSM RESUBMISSION

REVIEWED BY: _____

PROJECT COORDINATOR: _____

SCALE: _____

KMA

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PROJECT TITLE
KINGSESSING RECREATION CENTER

KEY PLAN

DRAWING TITLE
SITE DETAILS

PROJECT NO.
 KLMLX21003

DATE
 08/28/23

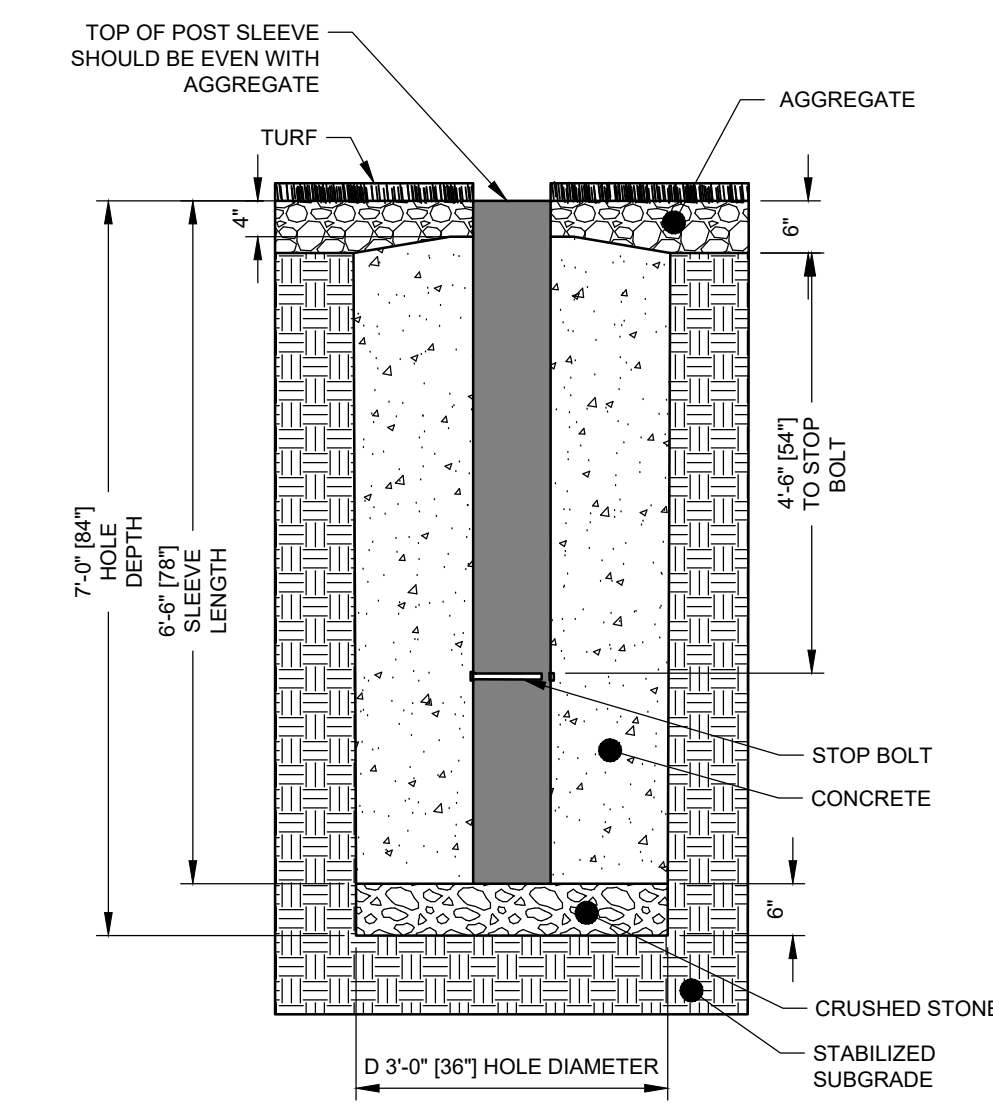
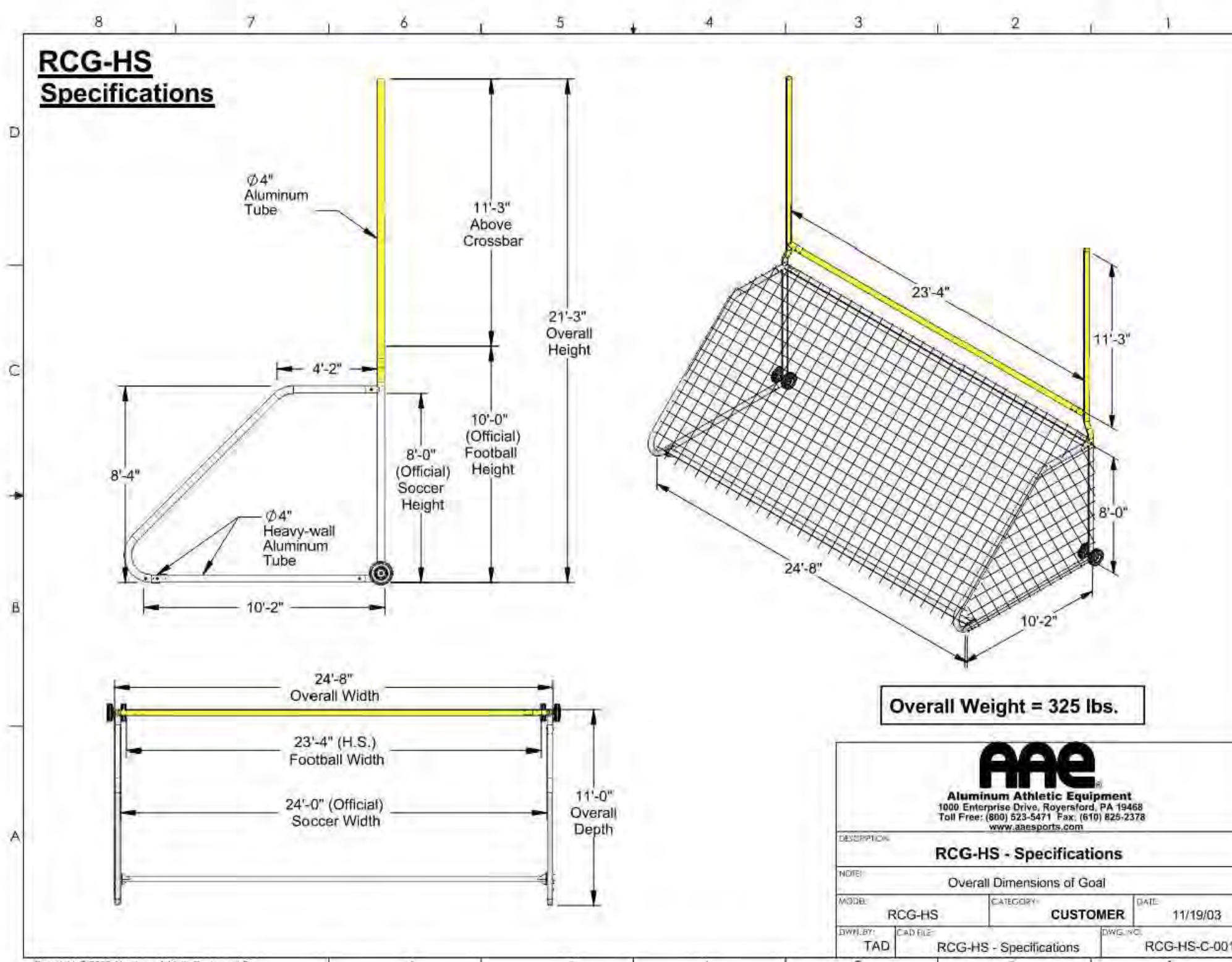
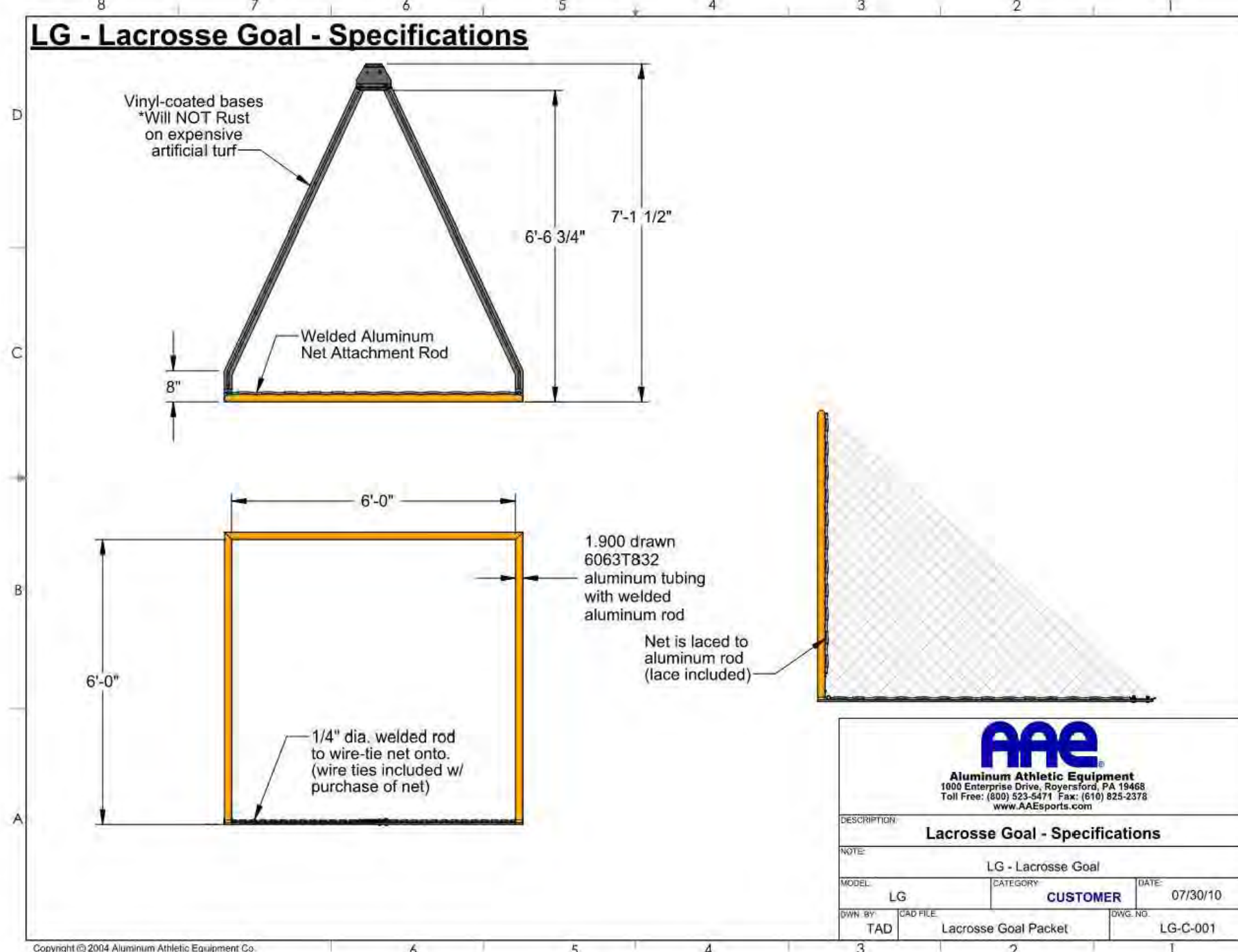
SCALE
 AS NOTED

DRAWN BY
 SDL

CHECKED BY
 MJM

DRAWING NO.
CS608-R.2

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DETAIL- LACROSS GOAL DETAIL
N.T.S.

DETAIL- COMBINATION FOOTBALL/SOCCER GOAL
N.T.S.

DETAIL- 40' HEIGHT BALL CONTROL NET CONCRETE FOOTING DETAIL
N.T.S.

REVISIONS

ISSUE	DATE	DESCRIPTION
1	01/09/23	PWD PCSM/NPDES SUBMISSION
2	02/14/23	PWD PCSM RESUBMISSION
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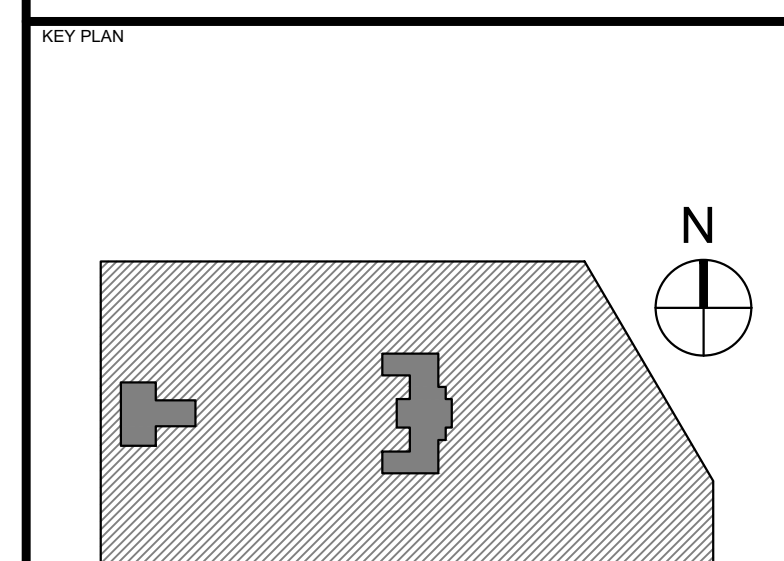
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PROJECT TITLE
KINGSESSING RECREATION CENTER

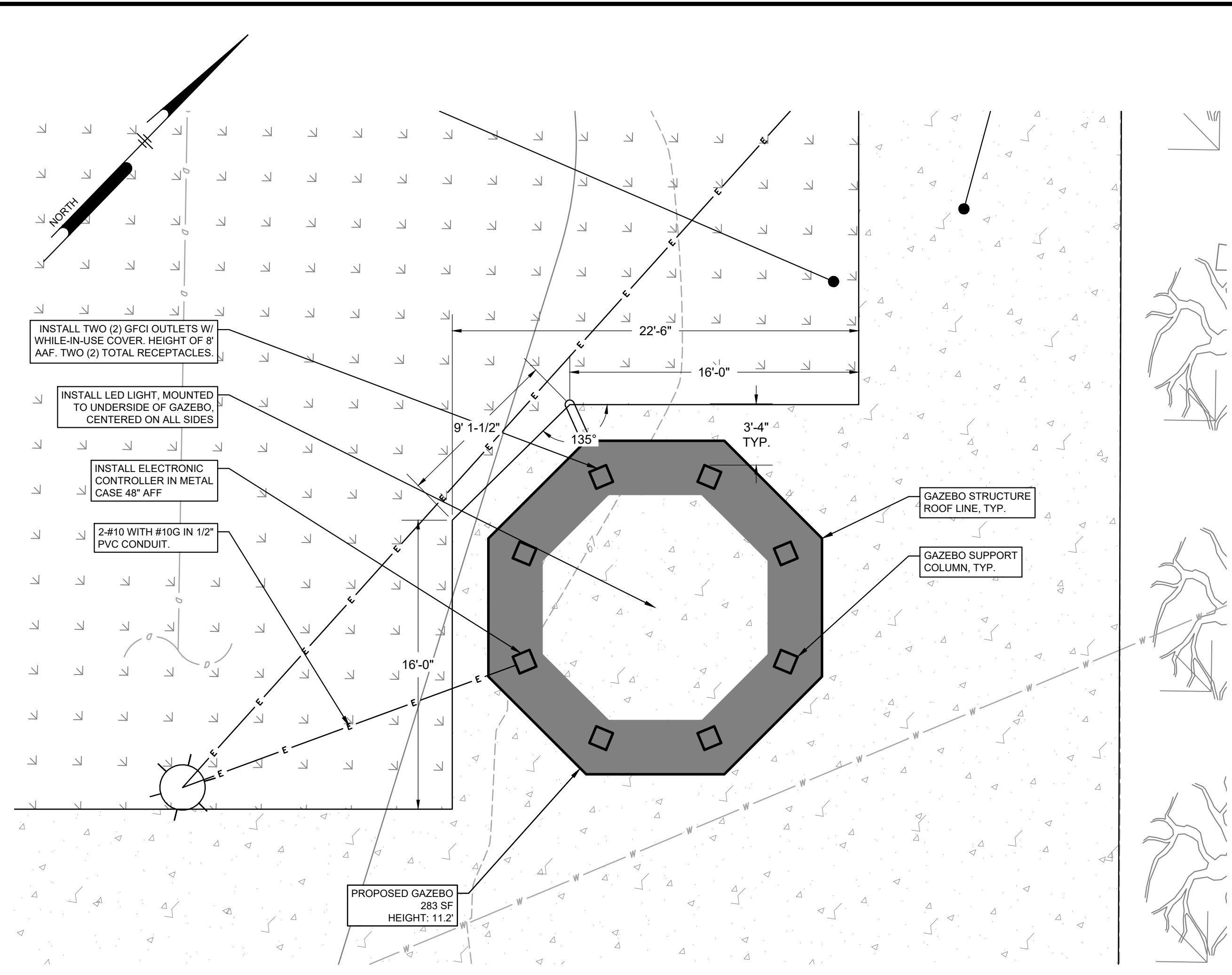


DRAWING TITLE
SITE DETAILS

PROJECT NO.	DRAWING NO.
KLMLX21003	CS609-R.2
DATE	08/28/23
SCALE	AS NOTED
DRAWN BY	SDL
CHECKED BY	MJM

PACKAGE 2 - IFB
NOT FOR CONSTRUCTION
08/28/23
NPDES PERMIT #PAC510302
PWD TRACKING #FY22-KING-6800-01

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DETAIL PLAN - SHADE STRUCTURE

SCALE: 1"=5'-0"

SHADE STRUCTURE NOTES:

- COMFORT STATION SHADE STRUCTURE IS TO BE 20' OCTAGON SHELTER AS MANUFACTURED BY POLYGON (POLYGON.COM), MODEL #OTC20. SINGLE TIER - STANDING SEAM ROOF.
- THE LAYOUT PROVIDED IS FOR INFORMATION ONLY. REFER TO WRITTEN INSTALLATION INSTRUCTIONS FROM POLYGON FOR DETAILED LAYOUT PLAN, COLUMN SPACING, TEMPLATES, AND LAYOUT INSTRUCTIONS FOR THE COLUMNS AND ANCHORS.
- STEEL POWDERCOAT AND ROOF COLORS TO BE SELECTED BY OWNER.

Electronic Timer Control - ET175C



Electronic Timer Control - 7-Day 2-Circuit Electronic Control, 120-277 VAC, 2-SPST, Indoor Metal Enclosure

Item ET175C



PRODUCT DESCRIPTION
This series offers an easy way to upgrade from a basic mechanical time switch to an electronic time switch. These timers will allow for set-to-the-minute programming, battery backup for power loss, up to 28 events total, and automatic daylight saving time corrections without the need of user interaction. They come in standard 24-hour, 7-day and 7-day astronomical versions.

- FEATURES**
- Select switch to determine input voltage between 120-277 VAC
 - Up to 28 events total
 - To-the-minute accuracy
 - Temporary override or permanent manual override
 - Automatic Daylight Saving Time adjustment
 - Astronomic modes enable dusk-to-dawn scheduling

- APPLICATIONS**
- Indoor Lighting Control
 - Timing/Scheduling ON/OFF
 - Machinery & Pump Controls

TECHNICAL DATA

General	
Model Number	ET175C
Description	7-Day 2-Circuit Electronic Control, 120-277 VAC, 2-SPST, Indoor Metal Enclosure
ULC Code	ET175C109865
Brand	Intermatic
Country of Origin (Intermatic)	USA
Warranty Period	1 Year Limited
Control Specifications	
Minimum ON/OFF Times	1 min
Minimum Pulse Time	2 sec
Maximum Pulse Time	2 sec
Maximum ON/OFF Times	Indefinite
Maximum ON/OFF Operations	28
Setpoint Program Count	28
ON/OFF Operations	28
Operation Mode	7 day
Daylight Savings Adjustment	Automatic
Backup Type	Battery
Battery Type	AAA
Battery Service Type	Field Serviceable
Mechanical Specifications	
Enclosure Type	Indoor type 1 metal
Dimensions	

Technical specifications and other information are subject to change without notice. Images can vary from original. © 02/27/2023 1/4

Electronic Timer Control - ET175C



Electronic Timer Control - 7-Day 2-Circuit Electronic Control, 120-277 VAC, 2-SPST, Indoor Metal Enclosure

Product Dimensions (H x W x D) in	7.875 x 5.125 x 3.4375 in
Wire Size Min	#14 AWG
Wire Size Max	#8 AWG
Knockout Dimensions Bottom	(2) diameters 1/2" - 3/4"
Knockout Dimensions Back	(1) diameter 1/2" - 3/4"

Load Ratings	
Turbofan Ranges	5 A, 120-240 VAC
Electronic Ballast Load Ratings Ranges	1 A, 120-277 VAC
Magnetic Ballast (M) Ranges	20 A, 120-277 VAC
Resistive (R) Ranges	30 A, 28 VDC; 30 A, 120-240 VAC
Inductive Load Ratings (I) Ranges	30 A, 120/240 VAC
Inductive Load Ratings (I) Ranges	20 A, 28 VDC; 30 A, 120/240 VAC
Turbofan (M) Ranges	5 A, 120/240 VAC
Motor Load Ratings (M) Ranges	1 HP, 120 VAC; 2 HP, 240 VAC
Motor Load Ratings (M) Ranges	1 HP, 120 VAC; 2 HP, 240 VAC

Electrical Specifications	
Voltage Selection Type	Selecter Switch
Wiring Option	Terminal
Input Voltage Ranges	120-277 VAC, 50/60 Hz
Number of Circuits	2
Switch Type	2SPST, 1hpPST or Pulse
Maximum Power Consumption (W)	6 W
Electronic Series	ET1700 Series

Packaging	
Unit Carton Dimensions (H x W x L) in	3.131 x 5.251 x 8.001 in

Environmental Specifications	
Temperature operation	-40 °F to 104 °F (-40 °C to 40 °C)

Standards and Certifications	
CSA Certification	cCSAus
Other Certifications and Composites	UL 20
California Proposition 65	Lead

Technical specifications and other information are subject to change without notice. Images can vary from original. © 02/27/2023 2/4

PRODUCT CUT SHEET - LIGHT TIMER/DIMMER

VCPG LED Parking Garage

Specifications:
Diameter: 19"
Height: 3.75" (with up-light)
Weight: 18 lbs (with up-light)
1/2" PVC CONDUIT

Introduction:
The all new VCPG LED (Visually Comfortable Parking Garage) luminaire is designed to bring glare control, optical performance and energy savings into one package. The recessed lens design of VCPG LED minimizes high angle glare, while its precision modulated acrylic lens eliminates LED pollution and delivers the required minimum, verticals and uniformity. The dedicated up-light module option reduces the contrast between the luminaire and the ceiling creating a more visually comfortable environment.

The VCPG LED delivers up to 87% in energy savings when replacing 175W metal halide luminaires. With over 100,000 hour life expectancy (12+ years of 24/7 continuous operation), the VCPG LED luminaire provides significant maintenance savings over traditional luminaires.

Ordering Information

EXAMPLE: VCPG LED V4 P4 40K 70CRI TSM MVOLT SRM DNAXD

Series	LED Light Engine	Fixture	Color Temperature	Color Rendering Index	Photometric	Photopic	Mounting	Shipping
VCPG LED	V4 18x18x18 Tri-View	P4	40K	70CRI	TSM	Medium	MVOLT	For ordering with top
	V4 18x18x18 Tri-View	P4	40K	70CRI	TSM	Medium	MVOLT	345 328
	V4 18x18x18 Tri-View	P4	40K	70CRI	TSM	Medium	MVOLT	480 208
	V4 18x18x18 Tri-View	P4	40K	70CRI	TSM	Medium	MVOLT	240 248
	V4 18x18x18 Tri-View	P4	40K	70CRI	TSM	Medium	MVOLT	345 328
	V4 18x18x18 Tri-View	P4	40K	70CRI	TSM	Medium	MVOLT	345 328

Shipping & Packaging

Shipping included:
P4: Request standard (24 inch length supply leads)
SRM: Surface mount (24 inch length supply leads)
AAA: Amazon.com (use SRM/AAA accessory request for wall)

Shipping separately:
YK: Mechanical mount

LITHONIA LIGHTING
One Lithonia Way • Conyers, Georgia 30012 • Phone: 800-705-5ERV (5378) • Fax: 770-932-2633
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PRODUCT CUT SHEET - LED LIGHT FIXTURE

Ordering Information Cont.

Accessories

VCPG LED V4 P4 40K 70CRI TSM MVOLT SRM DNAXD: Best suited for PM (specify finish)
VCPG LED V4 P4 40K 70CRI TSM MVOLT SRM DNAXD: Best suited for PM (specify finish)
VCPG LED V4 P4 40K 70CRI TSM MVOLT SRM DNAXD: Best suited for PM (specify finish)
VCPG LED V4 P4 40K 70CRI TSM MVOLT SRM DNAXD: Best suited for PM (specify finish)
VCPG LED V4 P4 40K 70CRI TSM MVOLT SRM DNAXD: Best suited for PM (specify finish)
VCPG LED V4 P4 40K 70CRI TSM MVOLT SRM DNAXD: Best suited for PM (specify finish)

NOTES

- P4 not available with VE P7 not available with VE
- See available with P7
- Only vertical height adjustment. No angle adjustment. Use P4 and VCPG LED for mounting to angled ceiling or
- Not available with 347V or 480V.
- Finish and color can vary up to 10% tolerance.
- SRM: 24" long supply leads with P4 and SRM.
- SRM: 24" long supply leads with P4 and SRM.
- SRM: 24" long supply leads with P4 and SRM.

Performance Data

Lumen Output

Mounting	Height	Beam	Area	Footcandle	Footcandle	Footcandle	Footcandle	Footcandle	Footcandle	
P1	27'	27'	15W	3,638	136	1,778	140	1,256	142	1,877
			30W	7,276	272	3,556	280	2,512	365	3,754
			45W	10,914	408	5,334	420	3,768	540	5,631

Up-light Lumen Output

Beam	Area	Footcandle	Footcandle
SR1	6.58	119	187
SR2	8.58	175	275

Lumen Multiplier for 80CRI

Beam	Area	Footcandle
SR1	6.58	119
SR2	8.58	175

Lumen Ambient Temperature (LAT) Multipliers

Data references the temperature performance and color for the luminaire based on 25°C ambient. Based on 1000 hours of 100% duty cycle. For more information, please refer to the product data sheet.

Temp	Multiplier	Color Shift
25°C	1.00	0.00
30°C	0.95	0.05
35°C	0.90	0.10
40°C	0.85	0.15
45°C	0.80	0.20

Projected LED Lumen Maintenance

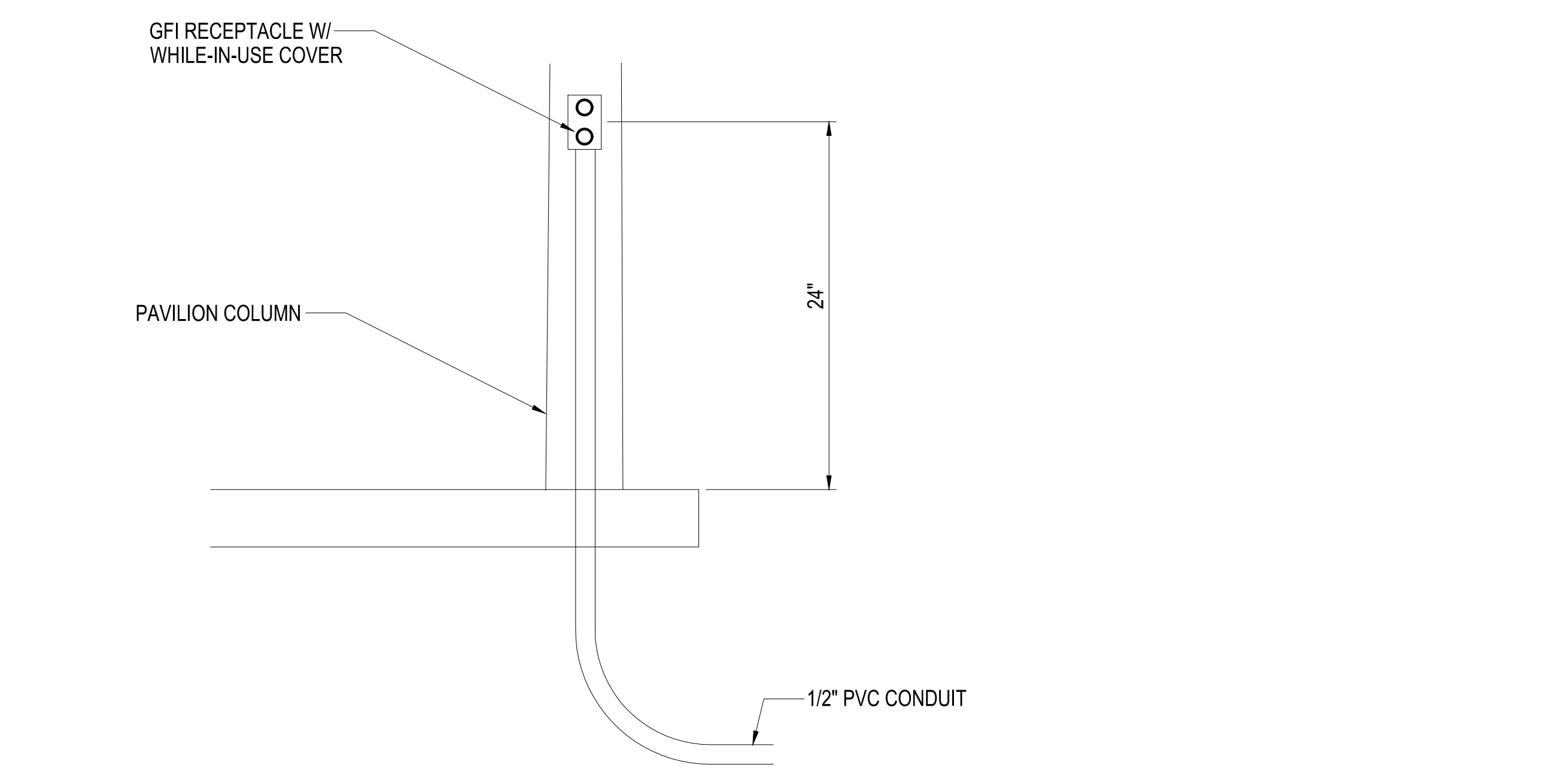
Data references the projected lumen maintenance for the luminaire based on 1000 hours of 100% duty cycle. For more information, please refer to the product data sheet.

Temp	Hours	Lumen Maintenance
25°C	10,000	90%
30°C	10,000	85%
35°C	10,000	80%
40°C	10,000	75%
45°C	10,000	70%

Electrical Load

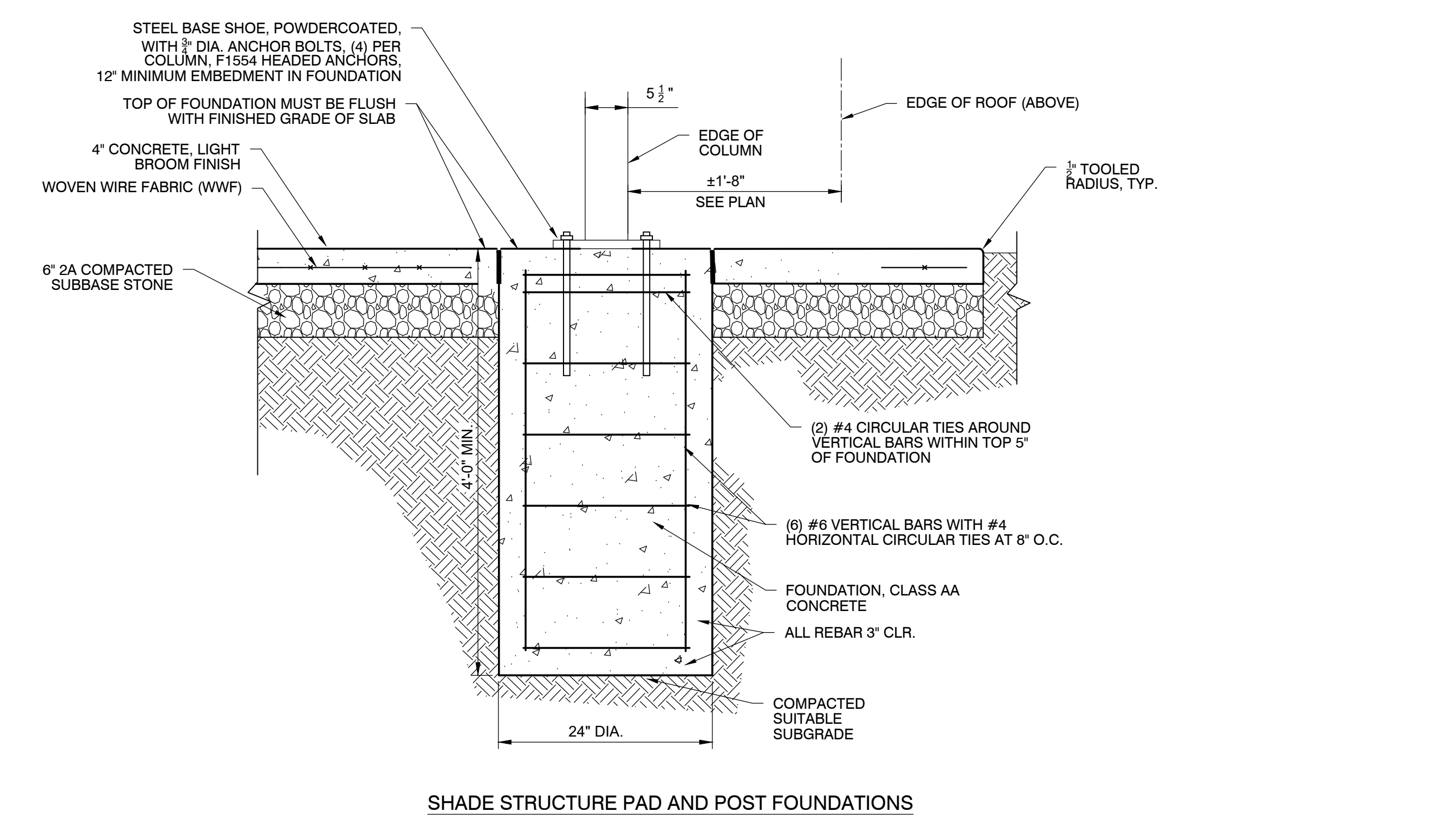
Beam	Area	Footcandle	Footcandle
SR1	6.58	119	187
SR2	8.58	175	275

LITHONIA LIGHTING
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PAVILION RECEPTACLE MOUNTING DETAIL

SCALE: N.T.S.



SHADE STRUCTURE PAD AND POST FOUNDATIONS

REVISIONS

ISSUE	DATE	DESCRIPTION
1	01/09/23	PWD PCSMNPDES SUBMISSION
2	02/14/23	PWD PCSM RESUBMISSION
3	05/24/23	PWD PCSM RESUBMISSION
7	06/28/23	ADDENDUM #1



REVIEWED BY:

PROJECT COORDINATOR

REALS



KELLY MAIELLO ARCHITECTS
1420 Walnut Street, 15th Floor
Philadelphia, PA 19102
www.kmarchitects.com

LANDSCAPE ARCHITECT:
Salt Design Studio
4100 Main Street Suite 200
Philadelphia PA 19127
www.saltdesignstudio.com

M.E.P./F.P./SITE CIVIL ENGINEERS:
Pennoni Associates
1900 Market Street Suite 300
Philadelphia PA 19103
www.pennoni.com

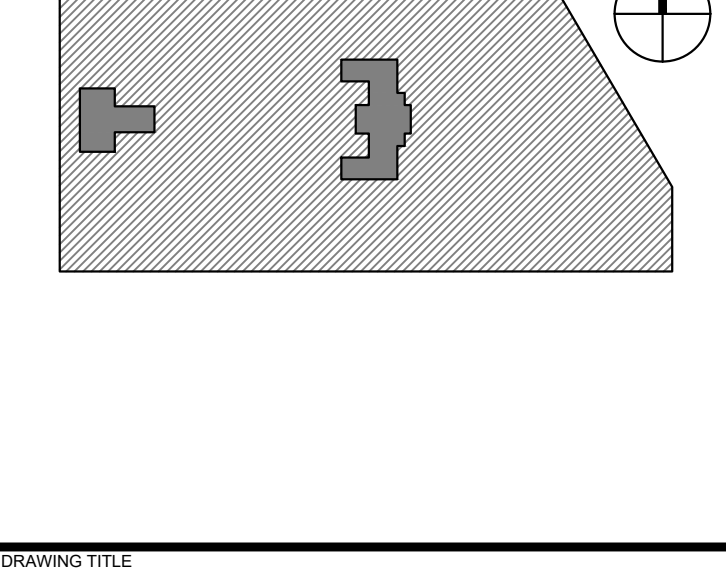
LEED CONSULTANT:
Verde Architecture Consulting
1635 Market Street Suite 1600
Philadelphia PA 19103

CITY OF PHILADELPHIA
DEPARTMENT OF PUBLIC PROPERTY
1515 ARCH STREET
11TH FLOOR, ONE PARKWAY BUILDING
PHILADELPHIA PENNSYLVANIA

KINGSESSING RECREATION CENTER

PROJECT TITLE

KEY PLAN



SITE IMPROVEMENT PLAN SOUTH

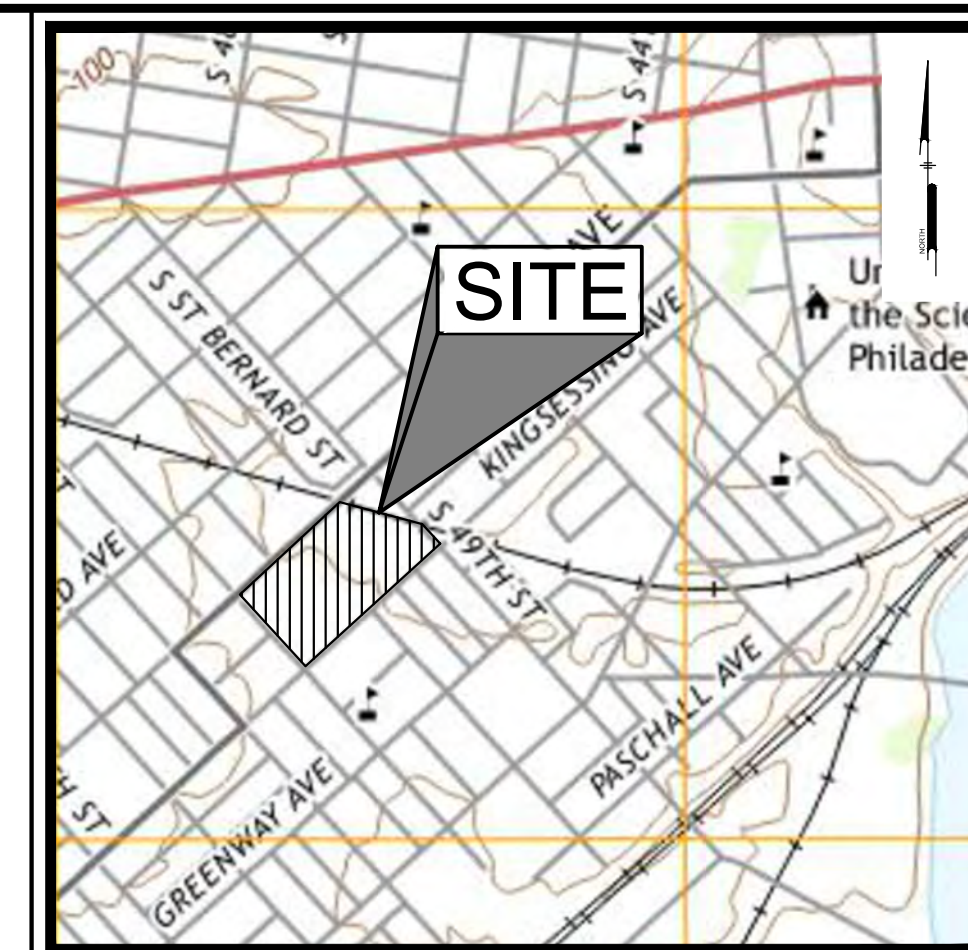
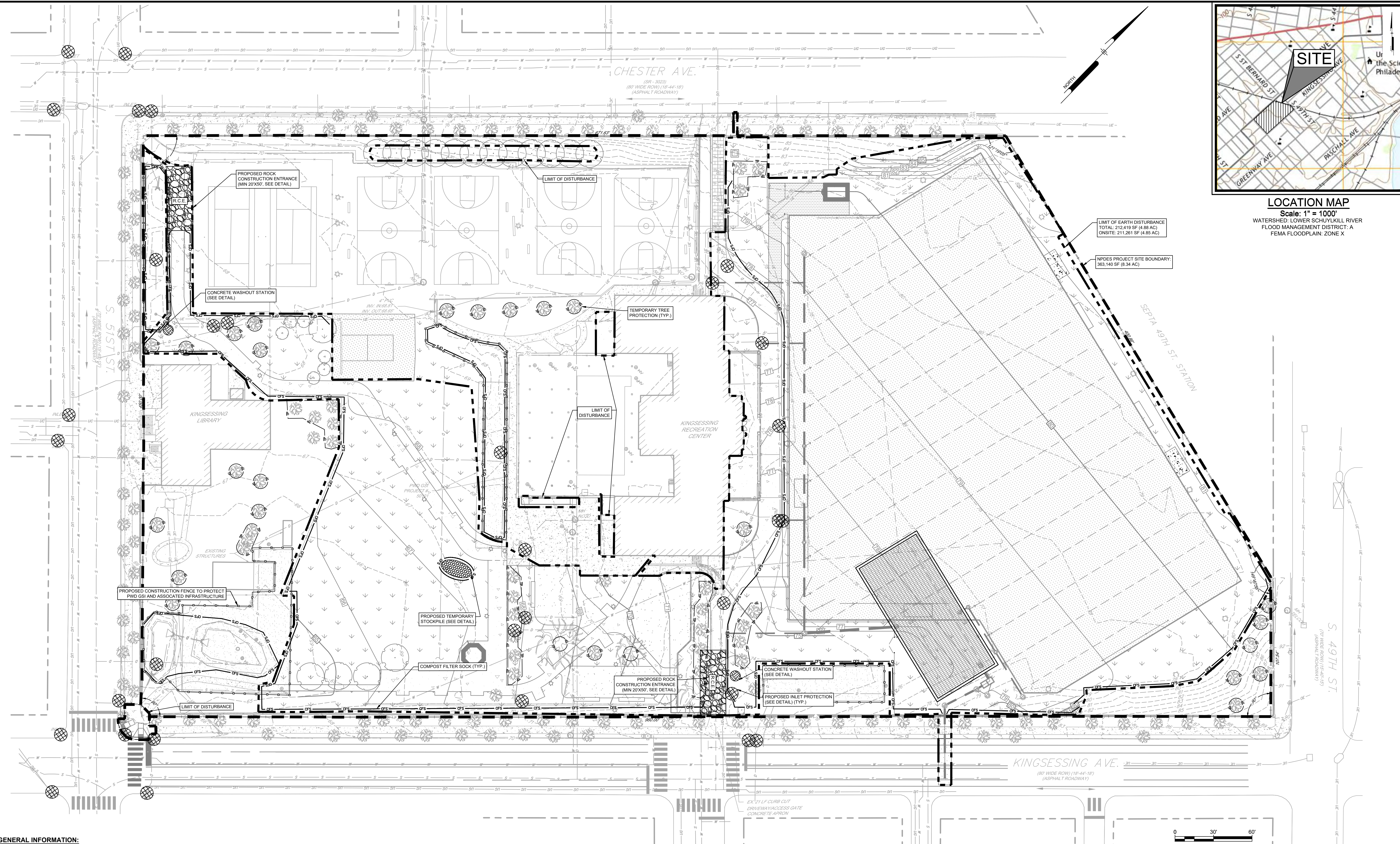
PROJECT NO. KLMLX21003 DRAWING NO. C-610-R.2

DATE 08/28/23 SCALE 1" = 5'

DRAWN BY SDL CHECKED BY MJM

NOTE: ALL DIMENSIONS AND CONDITIONS SHALL BE VERIFIED BY THE CONTRACTOR AT THE SITE BEFORE PROCEEDING WITH THE WORK.

PACKAGE 2 - IFB
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08/28/23
NPDES PERMIT #PAC510302
PWD TRACKING #FY22-KING-6800-01



LOCATION MAP
Scale: 1" = 100'
WATERSHED: LOWER SCHUYLKILL RIVER
FLOOD MANAGEMENT DISTRICT: A
FEMA FLOODPLAIN: ZONE X

REVISIONS		
ISSUE	DATE	DESCRIPTION
1	01/09/23	PWD PCSM/NPDES SUBMISSION
2	02/14/23	PWD PCSM RESUBMISSION
3	05/24/23	PWD PCSM RESUBMISSION



REVIEWED BY:
PROJECT COORDINATOR:
REALS:



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www.kmarchitects.com

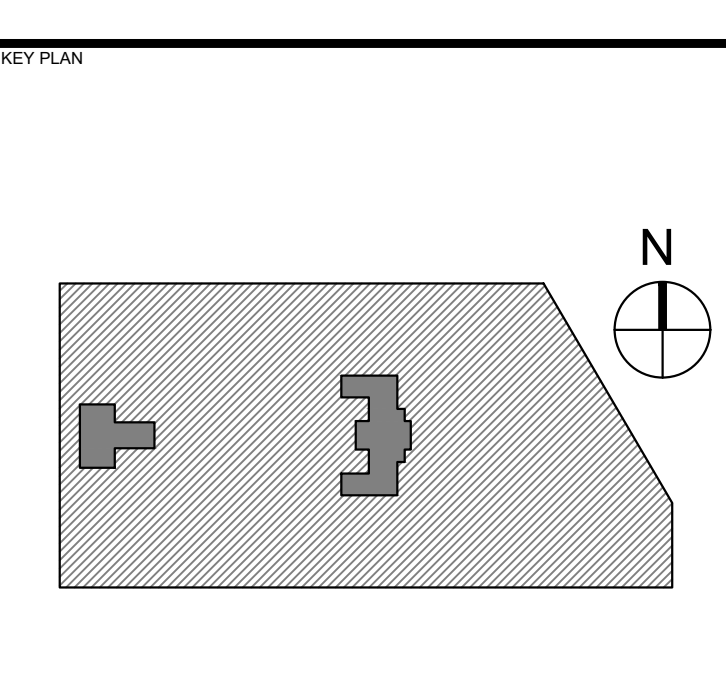
LANDSCAPE ARCHITECT:
Salt Design Studio
4100 Main Street Suite 200
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LEED CONSULTANT:
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1635 Market Street Suite 1600
Philadelphia PA 19103

CITY OF PHILADELPHIA
DEPARTMENT OF PUBLIC PROPERTY
1515 ARCH STREET
11TH FLOOR, ONE PARKWAY BUILDING
PHILADELPHIA, PENNSYLVANIA

PROJECT TITLE
KINGSESSING RECREATION CENTER



GENERAL INFORMATION:

SITE:
4901 KINGSESSING AVE
PHILADELPHIA, PA 19143
OPA#783249500

CLIENT:
CITY OF PHILADELPHIA
PARKS AND RECREATION
1515 ARCH STREET, 10TH FLOOR
PHILADELPHIA, PA 19102

ENGINEER:
PENNONI ASSOCIATES
1900 MARKET STREET, SUITE 300
PHILADELPHIA, PA 19103

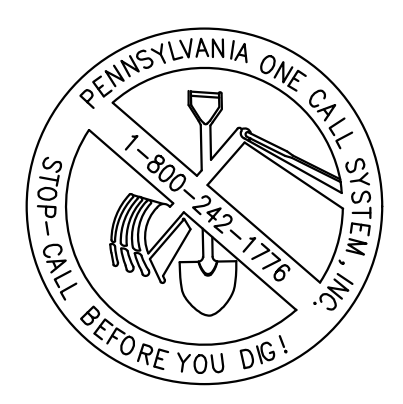
REFERENCES:

1. BOUNDARY AND TOPOGRAPHIC INFORMATION TAKEN FROM A PLAN BY AMERICAN ENGINEERS GROUP, LLC ENTITLED "KINGSESSING RECREATION CENTER TOPOGRAPHIC SURVEY", SHEET NO. 1, DATED 03/12/2021.

EXISTING LEGEND	
---	EXISTING PROPERTY LINE (APPROXIMATE)
---	EXISTING RIGHT OF WAY LINE (APPROXIMATE)
---	EXISTING BUILDING
---	EXISTING CURB
---	EXISTING SIDEWALK
---	EXISTING EDGE OF MACADAM/GRAVEL
---	EXISTING TRAFFIC MARKING
---	EXISTING FENCE
○	EXISTING TREE
---	EXISTING MAJOR CONTOUR
---	EXISTING MINOR CONTOUR
---	EXISTING SIGN
---	EXISTING BOLLARD
---	EXISTING UTILITY POLE
---	EXISTING LIGHT
---	EXISTING INLET
---	EXISTING SEWER
---	EXISTING STORM SEWER
---	EXISTING WATER LINE
---	EXISTING UNDERGROUND TELEPHONE LINE
---	EXISTING GAS LINE
---	EXISTING UNDERGROUND ELECTRIC
---	EXISTING OVERHEAD WIRES
---	EXISTING UTILITY STRUCTURES

PROPOSED LEGEND	
---	PROPOSED BUILDING
---	PROPOSED DOOR
---	PROPOSED CONCRETE PAVEMENT
---	PROPOSED ASPHALT PAVEMENT
---	PROPOSED LANDSCAPED AREA
---	PROPOSED UNIT PAVERS
---	PROPOSED POROUS PLAYGROUND SAFETY SURF
---	PROPOSED SYNTHETIC TURF FIELD
---	PROPOSED ADA RAMP
---	PROPOSED CURB
---	PROPOSED DEPRESSED CURB
---	PROPOSED WALL
---	PROPOSED STOP BAR/PAVEMENT MARKING
---	PROPOSED FENCE
---	PROPOSED SIGN
---	PROPOSED MAJOR CONTOUR
---	PROPOSED MINOR CONTOUR
---	PROPOSED STORM INLET
---	PROPOSED STORM MANHOLE
---	PROPOSED STORM CLEANOUT
---	PROPOSED STORM SEWER
---	PROPOSED SANITARY SEWER
---	PROPOSED WATER LINE
---	PROPOSED FIRE LINE
---	PROPOSED NATURAL GAS LINE
---	PROPOSED CABLE/TELEPHONE LINE
---	PROPOSED ELECTRIC LINE

EROSION AND SEDIMENT CONTROL LEGEND	
---	PROPOSED LIMIT OF DISTURBANCE
---	PROPOSED NPDES PROJECT SITE BOUNDARY
---	PROPOSED ROCK CONSTRUCTION ENTRANCE
---	PROPOSED STOCKPILE
---	PROPOSED INLET PROTECTION
---	PROPOSED CONCRETE WASHOUT
---	PROPOSED TREE PROTECTION
---	PROPOSED COMPOST FILTER SOCK
---	PROPOSED CONSTRUCTION FENCE



CALL BEFORE YOU DIG
BEFORE YOU DIG ANYWHERE IN PENNSYLVANIA
CALL 1-800-242-1776
PA. ACT 287 OF 1974 REQUIRES THREE WORKING DAYS NOTICE TO UTILITIES BEFORE YOU EXCAVATE, DRILL OR BLAST PENNSYLVANIA ONE-CALL SYSTEM, INC. SERIAL NUMBER(S): 20212583952

PACKAGE 2 - IFB
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08/28/23
NPDES PERMIT #PAC510302
PWD TRACKING #FY22-KING-6800-01

EROSION & SEDIMENTATION CONTROL PLAN

PROJECT NO. KLMXLX21003	DRAWING NO. C-801-R.2
DATE 08/28/23	SCALE 1" = 30'
DRAWN BY SDL	CHECKED BY MJM

NOTE: ALL DIMENSIONS AND CONDITIONS SHALL BE VERIFIED BY THE CONTRACTOR AT THE SITE BEFORE PROCEEDING WITH THE WORK.

PWD STANDARD EROSION AND SEDIMENTATION CONTROL NOTES:

- AN INDUSTRIAL WASTE PERMIT WILL BE REQUIRED SHOULD PUMPING TO CITY-OWNED INFRASTRUCTURE BECOME NECESSARY DURING CONSTRUCTION. ALL PUMPING OF WATER FROM ANY WORK AREA SHALL BE DONE IN ACCORDANCE WITH THE PLAN OVER UNDISTURBED AREAS.
- INLET PROTECTION SHOULD BE PROVIDED FOR ALL INLETS OWNED BY PWD THAT ARE LOCATED WITHIN ONE BLOCK OF THE PROJECT SITE.
- PWD IS NOT RESPONSIBLE FOR ANY CLEANUP OR REPAIRS NEEDED ON CITY-OWNED INFRASTRUCTURE DUE TO FAILURE OF ANY EROSION AND SEDIMENT CONTROL PRACTICES. CONTRACTOR SHALL BE RESPONSIBLE FOR ANY CLEANUP OR REPAIRS NEEDED.
- INSPECTION AND MAINTENANCE OF ALL EROSION AND SEDIMENT BEST MANAGEMENT PRACTICES SHALL OCCUR ON A WEEKLY BASIS, BEFORE ANY ANTICIPATED PRECIPITATION EVENTS, AND AFTER ALL PRECIPITATION EVENTS.
- THE MAXIMUM HEIGHT FOR STOCKPILES AREAS SHALL BE 20 FEET. THE MAXIMUM SIDE SLOPE FOR STOCKPILE AREAS SHALL NOT EXCEED 2:1.
- THE ROCK CONSTRUCTION ENTRANCE THICKNESS SHALL BE CONSTANTLY MAINTAINED ON-SITE. A STOCKPILE SHALL BE MAINTAINED ON-SITE FOR THIS PURPOSE. AT THE END OF EACH CONSTRUCTION DAY, ALL SEDIMENT DEPOSITED ON PAVED ROADWAYS SHALL BE REMOVED AND RETURNED TO THE CONSTRUCTION SITE. IN NO CASE SHALL THE SEDIMENT BE WASHED, SHOVELLED, OR SWEEP INTO ANY ROADSIDE DITCH, STORM SEWER, OR SURFACE WATER.
- FILTER FABRIC FENCE AND/OR COMPOST FILTER SOCK SHALL BE INSTALLED AT LEVEL GRADE. BOTH ENDS OF EACH FENCE/STOCK SECTION SHOULD BE EXTENDED AT LEAST 8 FEET UPSLOPE AT 45 DEGREES TO THE MAIN BARRIER ALIGNMENT. SUPPORT STAKES SHOULD BE SPACED AT A MAXIMUM OF 15 FEET. OBJECTS OF CONSIDERABLE MASS (E.G. CONCRETE BLOCKS, SAND BAGS, ETC.) SHALL BE USED IMMEDIATELY DOWNSLOPE OF COMPOST FILTER SOCKS PLACED ON PAVED SURFACES IN LIEU OF STAKES TO HOLD THE SOCK IN PLACE. OBJECTS TO BE PLACED AT INTERVALS PER THE COMPOST FILTER SOCK MANUFACTURER'S SPECIFICATION OR PA DEP MAXIMUM SPACING OF 10' ON CENTER. SEDIMENT MUST BE REMOVED WHEN ACCUMULATIONS REACH 1/2 THE ABOVE GROUND HEIGHT OF THE FILTER FENCE/FILTER SOCK.
- ANY FENCE/STOCK SECTION WHICH HAS BEEN UNDERMINED OR TOPPED MUST BE IMMEDIATELY REPLACED WITH A ROCK FILTER OUTLET. SEDIMENT MUST BE REMOVED WHEN ACCUMULATIONS REACH 1/3 THE HEIGHT OF THE OUTLET.
- EROSION CONTROL BLANKETING SHALL BE INSTALLED ON ALL SLOPES 3H:1V OR STEEPER WITHIN 90 FEET OF A SURFACE WATER AND ON ALL OTHER DISTURBED AREAS SPECIFIED ON THE PLAN MAPS AND/OR DETAIL SHEETS.
- IMMEDIATELY UPON DISCOVERING UNFORESEEN CIRCUMSTANCES POSING THE POTENTIAL FOR ACCELERATED EROSION AND/OR SEDIMENT POLLUTION, THE OPERATOR SHALL IMPLEMENT APPROPRIATE BEST MANAGEMENT PRACTICES TO MINIMIZE THE POTENTIAL FOR EROSION AND SEDIMENT POLLUTION AND NOTIFY PWD AND PA DEP.
- UNTIL THE SITE IS STABILIZED, ALL E&S BMPs SHALL BE MAINTAINED PROPERLY. MAINTENANCE SHALL INCLUDE INSPECTIONS OF ALL E&S BMPs PRIOR TO ANY ANTICIPATED STORM EVENT. AFTER EACH RAINFALL EVENT AND ON A WEEKLY BASIS, ALL PREVENTATIVE AND REMEDIAL MAINTENANCE WORK, INCLUDING CLEAN OUT, REPAIR, REPLACEMENT, REGRADING, RESEEDING, REMULCHING, AND RENETTING, MUST BE PERFORMED IMMEDIATELY. IF THE E&S BMPs FAIL TO PERFORM AS EXPECTED, REPLACEMENT BMPs, OR MODIFICATIONS OF THOSE INSTALLED, WILL BE REQUIRED.
- ON ALL EARTH DISTURBANCES, INCLUDING CLEARING AND GRUBBING, AS WELL AS CUTS AND FILLS, SHALL BE DONE IN ACCORDANCE WITH THE APPROVED E&S PLAN. A COPY OF THE APPROVED DRAWINGS MUST BE AVAILABLE AT THE PROJECT SITE AT ALL TIMES. PWD SHALL BE NOTIFIED OF ANY CHANGES TO THE APPROVED PLAN PRIOR TO IMPLEMENTATION OF THOSE CHANGES. PWD MAY REQUIRE A WRITTEN SUBMITTAL OF THOSE CHANGES FOR REVIEW AND APPROVAL AT ITS DISCRETION.
- AT LEAST THREE (3) DAYS PRIOR TO STARTING ANY EARTH DISTURBANCE ACTIVITIES, OR EXPANDING INTO AN AREA PREVIOUSLY UNMARKED, THE PENNSYLVANIA ONE CALL SYSTEM, INC. SHALL BE NOTIFIED AT 1-800-242-1776 FOR THE LOCATION OF EXISTING UNDERGROUND UTILITIES.
- ALL EARTH DISTURBANCE ACTIVITIES SHALL PROCEED IN ACCORDANCE WITH THE SEQUENCE PROVIDED ON THE PLAN DRAWINGS. DEVIATION FROM THAT SEQUENCE MUST BE APPROVED IN WRITING BY PWD AND THE PA DEP PRIOR TO IMPLEMENTATION.
- AREAS TO BE FILLED ARE TO BE CLEARED, GRUBBED, AND STRIPPED OF TOPSOIL TO REMOVE TREES, VEGETATION, ROOTS AND OTHER OBJECTIONABLE MATERIAL.
- CLEARING, GRUBBING, AND TOPSOIL STRIPPING SHALL BE LIMITED TO THE AREAS DESCRIBED IN EACH STAGE OF THE CONSTRUCTION SEQUENCE. GENERAL SITE CLEARING, GRUBBING, AND TOPSOIL STRIPPING MAY NOT COMMENCE IN ANY STAGE OF THE PROJECT UNTIL THE E&S BMPs SPECIFIED BY THE BMP SEQUENCE FOR THAT STAGE HAVE BEEN INSTALLED AND ARE FUNCTIONING AS DESCRIBED IN THIS PLAN.
- AT NO TIME SHALL CONSTRUCTION VEHICLES BE ALLOWED TO ENTER AREAS OUTSIDE THE LIMIT OF DISTURBANCE BOUNDARIES SHOWN ON THE PLAN MAPS. THESE AREAS MUST BE CLEARLY MARKED AND FENCED OFF BEFORE CLEARING AND GRUBBING OPERATIONS BEGIN.
- A LOG SHOWING DATES THAT E&S BMPs WERE INSPECTED AS WELL AS ANY DEFICIENCIES FOUND AND THE DATE THEY WERE CORRECTED SHALL BE MAINTAINED ON THE SITE AND BE MADE AVAILABLE TO PWD AT THE TIME OF INSPECTION.
- ALL SEDIMENT REMOVED FROM BMPs SHALL BE DISPOSED OF IN ACCORDANCE WITH ALL STATE, FEDERAL, AND LOCAL REGULATIONS.
- AREAS WHICH ARE TO BE TOPSOILED SHALL BE SCARIFIED TO A MINIMUM DEPTH OF THREE TO FIVE INCHES – SIX TO 12 INCHES ON COMPACTED SOILS – PRIOR TO PLACEMENT OF TOPSOIL. AREAS TO BE VEGETATED SHALL HAVE A MINIMUM FOUR INCHES OF TOPSOIL. IN PLACE PRIOR TO SEEDING AND MULCHING. FILL OUTSLOPES SHALL HAVE A MINIMUM OF TWO INCHES OF TOPSOIL.
- ALL FILLS SHALL BE COMPACTED AS REQUIRED TO REDUCE EROSION, SLIPPAGE, SETTLEMENT, SUBSIDENCE, OR OTHER RELATED PROBLEMS. FILL INTENDED TO SUPPORT BUILDINGS, STRUCTURES, AND CONDUITS, ETC. SHALL BE COMPACTED IN ACCORDANCE WITH LOCAL REQUIREMENTS OR CODES.
- ALL EARTHEN FILLS SHALL BE PLACED IN COMPACTED LAYERS NOT TO EXCEED NINE INCHES IN THICKNESS.
- FILL MATERIALS SHALL BE FREE OF FROZEN PARTICLES, BRUSH, ROOTS, SOO, OR OTHER FOREIGN OR OBJECTIONABLE MATERIALS THAT WOULD INTERFERE WITH OR PREVENT CONSTRUCTION OF SATISFACTORY FILLS.
- FILL MATERIALS OR SOFT, MUCKY, OR HIGHLY COMPRESSIBLE MATERIALS SHALL NOT BE INCORPORATED INTO FILLS.
- FILL SHALL NOT BE PLACED ON SATURATED OR FROZEN SURFACES.
- SEEPS OR SPRINGS ENCOUNTERED DURING CONSTRUCTION SHALL BE HANDLED IN ACCORDANCE WITH THE STANDARD AND SPECIFICATION FOR SUBSURFACE DRAIN OR OTHER APPROVED METHOD.
- ALL GRADED AREAS SHALL BE PERMANENTLY STABILIZED IMMEDIATELY UPON REACHING FINISHED GRADE. CUT SLOPES IN COMPETENT BEDROCK AND ROCK FILLS NEED NOT BE VEGETATED. SEEDED AREAS WITHIN 50 FEET OF A SURFACE WATER, OR AS OTHERWISE SHOWN ON THE PLAN DRAWINGS, SHALL BE BLANKETED ACCORDING TO THE STANDARDS OF THIS PLAN.
- IMMEDIATELY AFTER EARTH DISTURBANCE ACTIVITIES CEASE IN ANY AREA OR SUBAREA OF THE PROJECT, THE OPERATOR SHALL STABILIZE ALL DISTURBED AREAS. DURING NON-GERMINATING MONTHS, MULCH OR OTHER PROTECTIVE BLANKETING SHALL BE APPLIED AS DESCRIBED IN THE PLAN. AREAS NOT AT FINISHED GRADE, WHICH WILL BE REACTIVATED WITHIN ONE YEAR, MAY BE STABILIZED IN ACCORDANCE WITH THE TEMPORARY STABILIZATION SPECIFICATIONS. THOSE AREAS WHICH WILL NOT BE REACTIVATED WITHIN ONE YEAR SHALL BE STABILIZED IN ACCORDANCE WITH THE PERMANENT STABILIZATION SPECIFICATIONS.
- PERMANENT STABILIZATION IS DEFINED AS A MINIMUM UNIFORM, PERENNIAL, 70% VEGETATIVE COVER OR OTHER PERMANENT NON-VEGETATIVE COVER WITH A DENSITY SUFFICIENT TO RESIST ACCELERATED EROSION. CUT AND FILL SLOPES SHALL BE CAPABLE OF RESISTING FAILURE DUE TO SLUMPING, SLIDING, OR OTHER MOVEMENTS.
- E&S BMPs SHALL REMAIN FUNCTIONAL AS SUCH UNTIL ALL AREAS TRIBUTARY TO THEM ARE PERMANENTLY STABILIZED OR UNTIL THEY ARE REPLACED BY ANOTHER BMP APPROVED BY PWD AND PA DEP.
- AFTER FINAL SITE STABILIZATION HAS BEEN ACHIEVED, TEMPORARY E&S BMPs MUST BE REMOVED OR CONVERTED TO PERMANENT POST-CONSTRUCTION STORMWATER MANAGEMENT PRACTICES. AREAS DISTURBED DURING REMOVAL OR CONVERSION OF THE E&S BMPs SHALL BE STABILIZED IMMEDIATELY. IN ORDER TO ENSURE RAPID REVEGETATION OF DISTURBED AREAS, SUCH REMOVAL/CONVERSIONS ARE TO BE DONE ONLY DURING THE GERMINATING SEASON.
- SEDIMENT BASINS AND/OR TRAPS SHALL BE KEPT FREE OF ALL CONSTRUCTION WASTE, WASH WATER, AND OTHER DEBRIS HAVING POTENTIAL TO CLOG THE BASIN/TRAP OUTLET STRUCTURES AND/OR POLLUTE THE SURFACE WATERS.
- DURING CONSTRUCTION, THE SELECTED CONTRACTOR IS EXPECTED TO FOLLOW THE PCSPM APPROVED BY PWD. NO CHANGE OR DEVIATION FROM THE APPROVED PCSPM IS PERMITTED WITHOUT PRIOR APPROVAL FROM PWD.
- ALL WORK ASSOCIATED WITH PWD WATER CONVEYANCE AND SEWER INFRASTRUCTURE SHALL BE DONE IN ACCORDANCE WITH THE CITY OF PHILADELPHIA WATER DEPARTMENT WATER MAIN STANDARD DETAILS AND CORROSION CONTROL SPECIFICATIONS, 1985 EDITION, AND STANDARD DETAILS AND STANDARD SPECIFICATIONS FOR SEWERS, 1985 EDITION.
- CONTACT PWD WATER TRANSPORT RECORDS (1101 MARKET STREET, 2ND FLOOR, PHONE: 215-685-8271) FOR ADDITIONAL APPROVALS AND PERMITS REQUIRED FOR ALL WATER SERVICES, METERS, AND CONNECTIONS TO THE EXISTING AND/OR PROPOSED PWD FACILITIES.
- ALL BUILDING MATERIALS AND WASTES SHALL BE REMOVED FROM THE SITE AND RECYCLED OR DISPOSED OF IN ACCORDANCE WITH THE DEPARTMENT'S SOLID WASTE MANAGEMENT REGULATIONS AT 25 PA CODE 260.1 ET SEQ. 271.1, AND 287.1 ET SEQ. NO BUILDING MATERIALS OR WASTES OR UNRECYCLED BUILDING MATERIALS SHALL BE BURNED, BURIED, DUMPED, OR DISCHARGED AT THE SITE.
- A DUST CONTROL PERMIT WILL BE REQUIRED WHEN COMPLETELY DEMOLISHING A BUILDING OR STRUCTURE THAT IS MORE THAN THREE (3) STORIES, GREATER THAN 400 (FOOT) FEET TALL, OR ENCOMPASSES MORE THAN TEN THOUSAND (10,000) SQUARE FEET, COMPLETELY OR PARTIALLY DEMOLISHING ANY BUILDING OR STRUCTURE BY IMPLICATION, OR ENGAGING IN EARTHWORKS, DEFINED AS CLEARING, GRUBBING, OR EARTH DISTURBANCE OF ANY LAND IN EXCESS OF 5,000 SQUARE FEET."

DUST CONTROL MEASURES:

- FUGITIVE DUST FROM CONSTRUCTION, DEMOLITION, AND EARTHWORKS ACTIVITIES MAY NOT BE VISIBLE AT THE POINT IT PASSES THE WORK SITE PROPERTY LINE.
- USE OF VACUUM OR SIMILAR SUCTION SYSTEMS TO CAPTURE DUST KICKED UP BY POWER TOOLS WHEN GRINDING / CUTTING
- APPLICATION OF WATER OR APPROVED DUST SUPPRESSANT TO A WORK SITE WITH ONGOING EXCAVATION, LAND CLEARING, DEMOLITION, OR OTHER EARTH DISTURBANCE RELATED ACTIVITIES TO SUPPRESS DUST FORMATION.
- GENERAL PROHIBITION AGAINST DRY ABRASIVE BLASTING OF EXTERIOR SURFACES OPEN TO THE OUTSIDE AIR WHEN TEMPERATURE IS ABOVE FREEZING.
- COVERING AND WETTING OF STOCKPILE EARTH, SAND, GRAVEL, AND OTHER SIMILAR CONSTRUCTION MATERIALS.
- USE OF A MATERIAL CHUTE WHEN DROPPING MATERIAL OR DEBRIS MORE THAN 20 FT OUTSIDE OF THE EXTERIOR WALLS OF A BUILDING OR STRUCTURE. MATERIALS MUST BE WETTED WHEN DROPPED, AND /OR EXIT OF CHUTE MUST BE SEALED AGAINST THE TOP OF THE RECEIVING CONTAINER. DUMPIER.
- ALL TEMPORARY PERIMETER FENCING AROUND MUST HAVE A DUST CONTROL FABRIC; MUST MEASURE A MINIMUM OF 5FT IN HEIGHT FROM THE BOTTOM OF THE FENCING.
- A 10 MPH PER HOUR SPEED LIMIT FOR ALL EQUIPMENT AND TRUCKS TRAVELING WITHIN THE WORK SITE.
- WETTING AND SWEEPING OF ROADWAYS / ACCESS ROAD IN A WORK SITE TO PREVENT DUST FORMATION.
- VEHICLE ACCESS POINTS MUST BE EQUIPPED WITH DUST SUPPRESSION MEASURES (I.E. WHEEL WASH SYSTEMS, RUMBLE GRATES, AND OR GRAVEL PADS).

TEMPORARY EROSION AND SEDIMENT CONTROL:

- INLET PROTECTION SHALL BE APPLIED, AS DETAILED ON THE PLAN, TO EVERY INLET WHICH HAS BEEN CONSTRUCTED TO THE ROADWAY SUBBASE ELEVATION. INLET PROTECTION MUST BE PROVIDED FOR ALL CITY OWNED INLETS LOCATED WITHIN ONE CITY BLOCK OF THE PROJECT SITE.
- SILT FENCE/COMPOST FILTER SOCKS SHALL BE INSTALLED DOWNSLOPE OF ALL AREAS TO BE DISTURBED BEFORE ANY WORK BEGINS. SILT FENCE/COMPOST FILTER SOCKS AND ROCK FILTERS SHALL BE INSTALLED AS NEAR AS POSSIBLE TO THE LOCATIONS SHOWN ON THE PLAN. FILTER FABRIC/COMPOST FILTER SOCKS SHOULD BE INSTALLED AT LEVEL GRADE. BOTH ENDS OF EACH FENCE SECTION SHOULD BE EXTENDED AT LEAST 8 FEET UPSLOPE AT 45 DEGREES TO THE MAIN BARRIER ALIGNMENT. SUPPORT STAKES AND/OR OBJECTS OF CONSIDERABLE MASS SHALL BE SPACED AT A MAXIMUM OF 15 FEET. SEDIMENT MUST BE REMOVED WHEN ACCUMULATIONS REACH 1/2 THE ABOVE GROUND HEIGHT OF THE FILTER FENCE/FILTER SOCK.
- STOCKPILED TOPSOIL MOUNDS SHALL BE STABILIZED BY APPLYING TEMPORARY SEED AND A PERMETER SILT FENCE/FILTER SOCK SHALL BE INSTALLED AROUND EACH MOUND. TEMPORARY SEEDING SHALL BE PER PENNDOT FORM 408, SECTION 804B.
- ALL STABILIZED CONSTRUCTION ENTRANCES SHALL BE INSTALLED AS NEAR AS POSSIBLE TO THE LOCATION SHOWN ON THE PLAN. THE ROCK CONSTRUCTION ENTRANCE THICKNESS SHALL BE CONSTANTLY MAINTAINED ON SITE. A STOCKPILE SHALL BE MAINTAINED ON SITE FOR THIS PURPOSE. AT THE END OF EACH CONSTRUCTION DAY, ALL SEDIMENT DEPOSITED ON PAVED ROADWAYS SHALL BE REMOVED AND RETURNED TO THE CONSTRUCTION SITE.
- FREQUENT INSPECTION SHALL BE MADE ON THE FILTER FABRIC FENCE/COMPOST FILTER SOCK. DAMAGED FENCE/STOCK SHALL BE IMMEDIATELY REPLACED. SEDIMENT MUST BE REMOVED WHERE ACCUMULATIONS REACH 1/2 THE ABOVE GROUND HEIGHTS OF FENCE/STOCK. SILT FENCE/COMPOST FILTER SOCK WHICH HAS BEEN TOPPED OR UNDERMINED IS TO BE REPLACED WITH A ROCK FILTER OUTLET. SEDIMENT MUST BE REMOVED WHEN ACCUMULATIONS REACH 1/3 THE HEIGHT OF THE OUTLET.
- PROCEDURE FOR STABILIZATION OF SOILS FOUND OR SUSPECTED TO BE CONTAMINATED:
- DISTURBED EARTH TO BE IMMEDIATELY STABILIZED WITH SEEDING THROUGH THE USE OF HYDROSEEDING TECHNIQUES AND HAY OR STRAW MULCHING TO CONTAIN CONTAMINATED SOILS.
- PLASTIC LINER TO BE INSTALLED BELOW TEMPORARY TOPSOIL STOCKPILES. TOPSOIL STOCKPILE TO BE STABILIZED WITH SEEDING AND TO BE COVERED BY A TEMPORARY DAILY COVER IMMEDIATELY PER THE REQUIREMENTS OF THE PA DEP E&S GUIDELINES.
- EXPOSED SOILS TO BE SPRINKLED WITH WATER ON A DAILY BASIS UNTIL MOIST TO PREVENT DUST GENERATION AND TRANSPORT.
- UTILIZE APPROVED PA DEP EROSION AND SEDIMENTATION CONTROL MEASURES TO PREVENT SOIL FROM LEAVING THE SITE. METHODS INCLUDE, BUT ARE NOT LIMITED TO WHEEL WASH, SILT FENCE, INLET FILTER PROTECTION, ROCK CONSTRUCTION ENTRANCE, ETC.

PWD STANDARD SEQUENCE OF CONSTRUCTION NOTES:

- AT LEAST SEVEN (7) DAYS PRIOR TO ANY EARTH DISTURBANCE, THE INSPECTIONS COORDINATOR OF PWD (OFFICE: 215-685-6387) MUST BE CALLED TO SCHEDULE A PRECONSTRUCTION MEETING.
- AT LEAST THREE (3) DAYS PRIOR TO RAIN GRADING AND SUBSURFACE DETENTION BASIN INSTALLATION, THE INSPECTIONS COORDINATOR OF PWD (OFFICE: 215-685-6387) MUST BE CALLED TO SCHEDULE AN INSPECTION (FOR EACH SMP).
- ALL STONE THAT MAKES UP THE (SUBSURFACE DETENTION BASIN) MUST REMAIN FREE OF SEDIMENT. IF SEDIMENT ENTERS THE STONE, THE CONTRACTOR MAY BE REQUIRED TO REMOVE THE SEDIMENT AND REPLACE IT WITH CLEAN WASHED STONE.
- UPON COMPLETION OF ALL EARTH DISTURBANCE ACTIVITIES AND PERMANENT STABILIZATION OF ALL DISTURBED AREAS, THE OWNER AND/OR OPERATOR SHALL CONTACT INSPECTIONS COORDINATOR OF PWD (OFFICE: 215-685-6387) FOR A FINAL INSPECTION PRIOR TO REMOVAL/CONVERSION OF THE E&S BMPs.
- AS SOON AS SLOPES, CHANNELS, DITCHES, AND OTHER DISTURBED AREAS REACH FINAL GRADE, THEY MUST BE STABILIZED. CESSATION OF ACTIVITY FOR FOUR (4) DAYS OR LONGER REQUIRES TEMPORARY STABILIZATION.
- THE NPDES NOTICE OF TERMINATION (N.O.T.) MUST BE SUBMITTED TO PA DEP UPON COMPLETION OF CONSTRUCTION (WHEN APPLICABLE).
- WATER PUMPED FROM WORK AREAS SHOULD BE TREATED FOR SEDIMENT REMOVAL PRIOR TO DISCHARGING TO A SURFACE WATER (WHEN APPLICABLE).

SEQUENCE OF CONSTRUCTION:

ALL EARTH DISTURBANCE ACTIVITIES SHALL PROCEED IN ACCORDANCE WITH THE FOLLOWING SEQUENCE AND SHALL BE COMPLETED IN COMPLIANCE WITH PHILADELPHIA WATER DEPARTMENT AND CHAPTER 102 REGULATIONS.

AT LEAST 7 DAYS BEFORE STARTING ANY EARTH DISTURBANCE ACTIVITIES, THE OPERATOR SHALL INVITE ALL CONTRACTORS INVOLVED IN THOSE ACTIVITIES, THE LAND OWNER, ALL APPROPRIATE MUNICIPAL OFFICIALS, THE EROSION AND SEDIMENT CONTROL PLAN PREPARER, AND A REPRESENTATIVE FROM THE PHILADELPHIA WATER DEPARTMENT TO SCHEDULE A PRE-CONSTRUCTION MEETING. ALSO, AT LEAST 3 WORKING DAYS BEFORE STARTING ANY EARTH DISTURBANCE ACTIVITIES, ALL CONTRACTORS INVOLVED SHALL NOTIFY THE PENNSYLVANIA ONE CALL SYSTEM, INC. AT 1-800-242-1776 FOR BURIED UTILITY LOCATIONS.

BEFORE IMPLEMENTING ANY REVISIONS TO THE APPROVED EROSION AND SEDIMENT CONTROL PLAN OR REVISIONS TO OTHER PLANS WHICH MAY AFFECT THE EFFECTIVENESS OF THE APPROVED E&S CONTROL PLAN, THE OPERATOR MUST RECEIVE APPROVAL OF THE REVISIONS FROM THE PHILADELPHIA WATER DEPARTMENT AND PA DEP. OF ENVIRONMENTAL PROTECTION (PADEP).

THE OPERATOR SHALL REMOVE FROM THE SITE, RECYCLE, OR DISPOSE OF ALL BUILDING MATERIALS AND WASTES IN ACCORDANCE WITH THE DEPARTMENT'S SOLID WASTE MANAGEMENT REGULATIONS AT 25 PA CODE 260.1 et seq. AND 287.1 et seq.

THE OPERATOR SHALL ASSURE THAT AN EROSION AND SEDIMENT CONTROL PLAN HAS BEEN PREPARED, APPROVED BY THE PHILADELPHIA WATER DEPARTMENT AND PADEP AND IS BEING IMPLEMENTED AND MAINTAINED FOR ALL PROPOSED SOIL, ROCK, STONE, AND BORROW AREAS ON OR OFF SITE.

- CONSTRUCTION WILL BEGIN UPON RECEIPT OF ALL REQUIRED PERMITS FROM CITY OF PHILADELPHIA AND PADEP.
- PRIOR TO PROCEEDING WITH CONSTRUCTION, CONFIRM THE LOCATION AND ELEVATION OF ALL EXISTING UTILITIES, MAINTAIN AND PROTECT ALL EXISTING UTILITIES TO REMAIN AT ALL TIMES.
- AS SOON AS SLOPES, CHANNELS, DITCHES, AND OTHER DISTURBED AREAS REACH FINAL GRADE, THEY MUST BE STABILIZED. CESSATION OF ACTIVITY FOR FOUR (4) DAYS OR LONGER REQUIRES TEMPORARY STABILIZATION.
- WATER PUMPED FROM WORK AREAS MUST BE TREATED FOR SEDIMENT REMOVAL PRIOR TO DISCHARGING TO A SURFACE WATER.
- DISTURBANCE OF THE PROJECT SITE MUST BE KEPT TO THE ABSOLUTE MINIMUM.
- ALL SOIL MOVEMENT SHALL BE IN ACCORDANCE WITH THE SPECIFICATIONS AND APPLICABLE MUNICIPAL, STATE, AND FEDERAL REGULATIONS.
- EXISTING PEDESTRIAN TRAFFIC SHALL BE MAINTAINED OR PROPERLY DIRECTED AROUND THE SITE THROUGHOUT THE DURATION OF THE PROJECT.
- DELINEATE LIMITS OF DISTURBANCE AS OUTLINED ON THE APPROVED EROSION AND SEDIMENTATION CONTROL PLANS WITH ORANGE CONSTRUCTION FENCING, LINEAR UTILITY TRENCHES, AS OUTLINED ON THE APPROVED EROSION AND SEDIMENTATION CONTROL PLANS. DO NOT NEED TO BE DELINEATED WITH FENCE. UTILITY TRENCHES SHALL BE BACKFILLED AT THE END OF EACH DAY AND TEMPORARILY STABILIZED. CONTRACTOR SHALL NOT PERFORM ANY OTHER WORK OUTSIDE OF THE APPROVED LIMITS OF DISTURBANCE.
- INSTALL INLET PROTECTION.
- ROCK CONSTRUCTION ENTRANCE AT THE SITE ENTRANCE AS SHOWN ON THE APPROVED PLAN. EXISTING SUBGRADE TO BE REMOVED WITHIN THE FOOTPRINT OF THE ROCK CONSTRUCTION ENTRANCE AREA FOR WHICH ROCK CONSTRUCTION ENTRANCE CAN BE INSTALLED BY THE END OF EACH WORK DAY. CONSTRUCTION VEHICLES SHALL ENTER AND EXIT THE SITE THROUGH THE ENTRANCE. ROCK CONSTRUCTION ENTRANCE TO BE RESTORED AND SEDIMENT IS TO BE REMOVED ON A DAILY BASIS.
- INSTALL COMPOST FILTER SOCK DOWN GRADE AND ON CONTOUR OF DISTURBED AREAS PER THE APPROVED PLAN. DISTURBED AREAS SHALL BE IMMEDIATELY STABILIZED AND GRADED TO SLOPE TOWARDS THE SITE AND/OR EXISTING UTILITY.
- STAKE OUT AND INSTALL ORANGE CONSTRUCTION FENCE AROUND THE LIMITS OF ANY PROPOSED, OR EXISTING TO BE PROTECTED, INFILTRATION AREA (TREE TRENCHES AND RAIN GARDENS). CONTRACTOR TO MINIMIZE THE CONSTRUCTION OF SOIL WITH THE INFILTRATION AREA. HEAVY MACHINERY IS PROHIBITED TO ENTER WITHIN THE LIMITS OF THE PROPOSED INFILTRATION AREA.
- THE STAGING AREA SHALL BE ESTABLISHED ON THE SITE AND SHALL NOT IMPED E RUNOFF FROM REACHING THE EXISTING INLETS. STAGING AREAS NOT CREATED ON EXISTING PAVEMENTS SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE APPROVED DETAIL.
- CONCRETE WASHOUT TO BE INSTALLED ADJACENT TO THE ROCK CONSTRUCTION ENTRANCE PER THE APPROVED PLANS.
- CONTRACTOR SHALL TEMPORARILY STABILIZE THE SITE AT THE END OF EACH WORK DAY (WHENEVER POSSIBLE) AND AT THE START OF A RAINFALL EVENT. CESSATION OF EARTH DISTURBANCE ACTIVITIES FOR AT LEAST 4 DAYS REQUIRES TEMPORARY STABILIZATION.
- CONTRACTOR TO HAVE PUMPED WATER FILTER BAG ON-SITE AND AVAILABLE FOR DEWATERING OF EXCAVATED AREAS WHEN REQUIRED. FILTER BAG TO BE DISCHARGED OVER UNDISTURBED VEGETATED AREAS IN ACCORDANCE WITH THE APPROVED PLANS.
- CONTRACTOR TO BEGIN DEMOLITION OF THE REMAINING EXISTING SITE FEATURES. REMOVE VEGETATION AND CONSTRUCTION OF THE FILL. DISTURBED AREAS SHALL BE STABILIZED WHENEVER POSSIBLE AND AS REQUIRED.
- PRIOR TO SMP INSTALLATION, THE INSPECTIONS COORDINATOR OF PWD (OFFICE: 215-685-6387) MUST BE CALLED TO SCHEDULE AN INSPECTION.

*INSTALL STAMPED BASIN (SEE SUBSURFACE BASIN SEQUENCE OF CONSTRUCTION) AND OUTLET CONTROL STRUCTURE.

*INSTALL MEDIA FILTER WATER QUALITY DEVICE IN ACCORDANCE WITH MANUFACTURERS SPECIFICATIONS.

- FOLLOWING THE CONSTRUCTION OF THE SMP, THE SITE IMPROVEMENTS INCLUDING THE SYNTHETIC TURF FIELD, CONCRETE PATHWAYS, PLAYGROUND SURFACING AND LANDSCAPING SHALL BE CONSTRUCTED, ENSURING ALL SMPs AND E&S BMPs ARE PROTECTED AND MAINTAINED THROUGHOUT THE CONSTRUCTION PROCESS.
- REMOVE ROCK CONSTRUCTION ENTRANCE AND CONCRETE WASHOUT WHEN CONCRETE WORK IS COMPLETE AND CONSTRUCTION EQUIPMENT NO LONGER NEEDS ACCESS.
- ONCE THE SITE AREA HAS ACHIEVED A MINIMUM OF 70% UNIFORM PERENNIAL VEGETATIVE COVER OR OTHER PERMANENT NON-VEGETATIVE COVER SUFFICIENT TO RESIST ACCELERATED SURFACE EROSION, REMOVE TEMPORARY EROSION AND SEDIMENTATION BMPs INCLUDING ROCK CONSTRUCTION ENTRANCES AND INLET PROTECTION. ANY AREA DISTURBED DURING THE REMOVAL OF A TEMPORARY BMP SHALL BE IMMEDIATELY STABILIZED WITH SEEDING AND STRAW MULCH.
- ENSURE THAT ALL POST-CONSTRUCTION STORMWATER MANAGEMENT BMPs ARE IN PLACE AND FUNCTIONING ACCORDING TO THE APPROVED PCSM PLANS, DETAILS, AND NARRATIVE. CONTACT THE PHILADELPHIA WATER DEPARTMENT FOR A FINAL INSPECTION AND SUBMIT RECORD DRAWINGS TO COMPLETE THE PROJECT.
- REMOVAL AND DISPOSAL OF BITUMINOUS MATERIAL SHALL BE IN COMPLETED IN ACCORDANCE WITH DETAILS AND REGULATIONS OF THE MUNICIPALITY, PADEP, AND PENNDOT, AS APPLICABLE AND IS SUBJECT TO INSPECTION AND APPROVAL AS APPROPRIATE.
- CONTRACTOR TO COLLECT AND REMOVE ALL TRASH/DEBRIS ON SITE.

*CRITICAL STATE - DURING THE BMP INSTALLATION, A LICENSED PROFESSIONAL ENGINEER, REGISTERED ARCHITECT/LANDSCAPE ARCHITECT, PROFESSIONAL LAND SURVEYOR, GEOLOGIST, OR LICENSED CONTRACTOR MUST DOCUMENT THE INFORMATION AND MEASUREMENTS REQUIRED ON THE BMP CONSTRUCTION CERTIFICATION FORMS WITHIN THE CERTIFICATION PACKAGE.

SEQUENCE OF CONSTRUCTION: SUBSURFACE BASIN

- AT LEAST 3 DAYS PRIOR TO THE CONSTRUCTION OF THE DETENTION BASIN, THE CONTRACTOR SHALL CONTACT INSPECTIONS COORDINATOR (OFFICE: 215-685-6387).
- CONTRACTOR TO NOTIFY DESIGN ENGINEER 3 DAYS PRIOR TO CONSTRUCTION OF BASIN.
- EXCAVATE FOOTPRINT TO SPECIFIED ELEVATION.
- INSTALL STORMWATER BASIN, INCLUDING LOW FLOW DEVICE, AS SPECIFIED BY MANUFACTURER IN THE AREA SPECIFIED ON PLANS.
- INSTALL MEDIA FILTER WATER QUALITY DEVICE IN ACCORDANCE WITH MANUFACTURERS SPECIFICATIONS.
- CONNECT PIPES TO STORMWATER BASIN IN LOCATIONS SPECIFIED ON PLANS.
- BACKFILL TO GRADE.

SEQUENCE OF CONSTRUCTION: MEDIA FILTER

- AT LEAST 3 DAYS PRIOR TO THE INSTALLATION OF THE MEDIA FILTER, THE CONTRACTOR SHALL CONTACT INSPECTIONS COORDINATOR (OFFICE: 215-685-6387).
- CONTRACTOR TO NOTIFY DESIGN ENGINEER 3 DAYS PRIOR TO INSTALLATION OF MEDIA FILTER.
- EXCAVATE FOOTPRINT TO SPECIFIED ELEVATION.
- INSTALL MEDIA FILTER WATER QUALITY DEVICE IN ACCORDANCE WITH MANUFACTURERS SPECIFICATIONS.
- CONNECT PIPES TO STORMWATER BASIN IN LOCATIONS SPECIFIED ON PLANS.
- BACKFILL TO GRADE.

EROSION AND SEDIMENTATION CONTROL NOTES:

- ALL EARTH DISTURBANCES, INCLUDING CLEARING AND GRUBBING AS WELL AS CUTS AND FILLS SHALL BE DONE IN ACCORDANCE WITH THE APPROVED E&S PLAN. A COPY OF THE APPROVED DRAWINGS (STAMPED, SIGNED AND DATED) BY THE REVIEWING AGENCY MUST BE AVAILABLE AT THE PROJECT SITE AT ALL TIMES. THE REVIEWING AGENCY SHALL BE NOTIFIED OF ANY CHANGES TO THE APPROVED PLAN PRIOR TO IMPLEMENTATION OF THOSE CHANGES. THE REVIEWING AGENCY MAY REQUIRE A WRITTEN SUBMITTAL OF THOSE CHANGES FOR REVIEW AND APPROVAL AT ITS DISCRETION.
- AT LEAST 7 DAYS PRIOR TO STARTING ANY EARTH DISTURBANCE ACTIVITIES, INCLUDING CLEARING AND GRUBBING, THE OWNER AND/OR OPERATOR SHALL INVITE ALL CONTRACTORS, THE LANDOWNER, APPROPRIATE MUNICIPAL OFFICIALS, THE E&S PLAN PREPARER, THE PCSM PLAN PREPARER, THE LICENSED PROFESSIONAL RESPONSIBLE FOR OVERSIGHT OF CRITICAL STAGES OF IMPLEMENTATION OF THE PCSM PLAN, AND A REPRESENTATIVE FROM THE LOCAL CONSERVATION DISTRICT TO AN ON-SITE PRECONSTRUCTION MEETING.
- AT LEAST 3 DAYS PRIOR TO STARTING ANY EARTH DISTURBANCE ACTIVITIES, OR EXPANDING INTO AN AREA PREVIOUSLY UNMARKED, THE PENNSYLVANIA ONE CALL SYSTEM, INC. SHALL BE NOTIFIED AT 1-800-242-1776 FOR THE LOCATION OF EXISTING UNDERGROUND UTILITIES.
- ALL EARTH DISTURBANCE ACTIVITIES SHALL PROCEED IN ACCORDANCE WITH THE SEQUENCE PROVIDED ON THE PLAN DRAWINGS. DEVIATION FROM THAT SEQUENCE MUST BE APPROVED IN WRITING FROM THE LOCAL CONSERVATION DISTRICT OR BY THE DEPARTMENT PRIOR TO IMPLEMENTATION.
- AREAS TO BE FILLED ARE TO BE CLEARED, GRUBBED, AND STRIPPED OF TOPSOIL TO REMOVE TREES, VEGETATION, ROOTS AND OTHER OBJECTIONABLE MATERIAL.
- CLEARING, GRUBBING, AND TOPSOIL STRIPPING SHALL BE LIMITED TO THOSE AREAS DESCRIBED IN EACH STAGE OF THE CONSTRUCTION SEQUENCE. GENERAL SITE CLEARING, GRUBBING AND TOPSOIL STRIPPING MAY NOT COMMENCE IN ANY STAGE OF THE PROJECT UNTIL THE E&S BMPs SPECIFIED BY THE BMP SEQUENCE FOR THAT STAGE OR PHASE HAVE BEEN INSTALLED AND ARE FUNCTIONING AS DESCRIBED IN THIS E&S PLAN.
- AT NO TIME SHALL CONSTRUCTION VEHICLES BE ALLOWED TO ENTER AREAS OUTSIDE THE LIMIT OF DISTURBANCE BOUNDARIES SHOWN ON THE PLAN MAPS. THESE AREAS MUST BE CLEARLY MARKED AND FENCED OFF BEFORE CLEARING AND GRUBBING OPERATIONS BEGIN.
- TOPSOIL REQUIRED FOR THE ESTABLISHMENT OF VEGETATION SHALL BE STOCKPILED, IF NECESSARY, AT THE LOCATION(S) SHOWN ON THE PLAN MAP(S) IN THE AMOUNT NECESSARY TO COMPLETE THE FINISHED GRADING OF ALL EXPOSED AREAS THAT ARE TO BE STABILIZED BY VEGETATION. EACH STOCKPILE SHALL BE PROTECTED IN THE MANNER SHOWN ON THE PLAN DRAWINGS. STOCKPILE HEIGHTS SHALL NOT EXCEED 20 FEET. STOCKPILE SLOPES SHALL BE 2H:1V OR FLATTER.
- IMMEDIATELY UPON DISCOVERING UNFORESEEN CIRCUMSTANCES POSING THE POTENTIAL FOR ACCELERATED EROSION AND/OR SEDIMENT POLLUTION, THE OPERATOR SHALL IMPLEMENT APPROPRIATE BEST MANAGEMENT PRACTICES TO MINIMIZE THE POTENTIAL FOR EROSION AND SEDIMENT POLLUTION AND NOTIFY THE LOCAL CONSERVATION DISTRICT AND/OR THE REGIONAL OFFICE OF THE DEPARTMENT.
- ALL BUILDING MATERIALS AND WASTES SHALL BE REMOVED FROM THE SITE AND RECYCLED OR DISPOSED OF IN ACCORDANCE WITH THE DEPARTMENT'S SOLID WASTE MANAGEMENT REGULATIONS AT 25 PA CODE 260.1 ET SEQ. 271.1, AND 287.1 ET SEQ. NO BUILDING MATERIALS OR WASTES OR UNRECYCLED BUILDING MATERIALS SHALL BE BURNED, BURIED, DUMPED, OR DISCHARGED AT THIS SITE.
- ALL OFF-SITE WASTE AND BORROW AREAS MUST HAVE AN E&S PLAN APPROVED BY THE LOCAL CONSERVATION DISTRICT OR THE DEPARTMENT FULLY IMPLEMENTED PRIOR TO BEING ACTIVATED.
- THE CONTRACTOR IS RESPONSIBLE FOR ENSURING THAT ANY MATERIAL BROUGHT ON SITE IS CLEAN FILL. FORM FP-001 MUST BE RETAINED BY THE PROPERTY OWNER FOR ANY FILL MATERIAL AFFECTED BY A SPILL OR RELEASE OF A REGULATED SUBSTANCE BUT QUALIFYING AS CLEAN FILL DUE TO ANALYTICAL TESTING.
- ALL PUMPING OF WATER FROM ANY WORK AREA SHALL BE DONE ACCORDING TO THE PROCEDURE DESCRIBED IN THIS PLAN, OVER UNDISTURBED VEGETATED AREAS. ALL PUMPING OF SEDIMENT LADEN WATER SHALL BE THROUGH A SEDIMENT CONTROL BMP. SUCH AS A PUMPED WATER FILTER BAG DISCHARGING OVER NON-DISTURBED AREAS.
- VEHICLES AND EQUIPMENT MAY NEVER ENTER DIRECTLY NOR EXIT DIRECTLY FROM LOTS. VEHICLES AND EQUIPMENT MAY ONLY ENTER AND EXIT THE CONSTRUCTION SITE VIA A STABILIZED ROCK CONSTRUCTION ENTRANCE OR HANDS STREET.

- UNTIL THE SITE IS STABILIZED, ALL EROSION AND SEDIMENT BMPs SHALL BE MAINTAINED PROPERLY. MAINTENANCE SHALL INCLUDE INSPECTIONS OF ALL EROSION AND SEDIMENT BMPs AFTER EACH RAINFALL EVENT AND ON A WEEKLY BASIS. THE OPERATOR WILL MAINTAIN AND MAKE AVAILABLE TO NORTHAMPTON COUNTY CONSERVATION DISTRICT COMPLETE, WRITTEN INSPECTION LOGS OF ALL THESE INSPECTIONS. ALL PREVENTATIVE AND REMEDIAL MAINTENANCE WORK, INCLUDING CLEAN OUT, REPAIR, REPLACEMENT, REGRADING, RESEEDING, REMULCHING, AND RENETTING, MUST BE PERFORMED IMMEDIATELY. IF THE E&S BMPs FAIL TO PERFORM AS EXPECTED, REPLACEMENT BMPs, OR MODIFICATIONS OF THOSE INSTALLED, WILL BE REQUIRED.
- A LOG SHOWING DATES THAT E&S BMPs WERE INSPECTED AS WELL AS ANY DEFICIENCIES FOUND AND THE DATE THEY WERE CORRECTED SHALL BE MAINTAINED ON THE SITE AND BE MADE AVAILABLE TO REGULATORY AGENCY OFFICIALS AT THE TIME OF INSPECTION.
- SEDIMENT TRACKED ONTO ANY PUBLIC ROADWAY OR SIDEWALK SHALL BE RETURNED TO THE CONSTRUCTION SITE BY THE END OF EACH WORK DAY AND DISPOSED IN THE MANNER DESCRIBED IN THIS PLAN. IN NO CASE SHALL THE SEDIMENT BE WASHED, SHOVELLED, OR SWEEP INTO ANY ROADSIDE DITCH, STORM SEWER, OR SURFACE WATER.
- ALL SEDIMENT REMOVED FROM BMPs SHALL BE DISPOSED OF IN THE MANNER DESCRIBED ON THE PLAN DRAWINGS. SEDIMENT REMOVED FROM BMPs SHALL BE DISPOSED OF IN LANDSCAPED AREAS OUTSIDE OF STEEP SLOPES, WETLANDS, FLOODPLAINS OR DRAINAGE SWALES AND IMMEDIATELY STABILIZED, OR PLACED IN TOPSOIL STOCKPILES.
- AREAS WHICH ARE TO BE TOPSOILED SHALL BE SCARIFIED TO A MINIMUM DEPTH OF 3 TO 5 INCHES - 6 TO 12 INCHES ON COMPACTED SOILS - PRIOR TO PLACEMENT OF TOPSOIL. AREAS TO BE VEGETATED SHALL HAVE A MINIMUM 4 INCHES OF TOPSOIL. IN PLACE PRIOR TO SEEDING AND MULCHING. FILL OUTSLOPES SHALL HAVE A MINIMUM OF 2 INCHES OF TOPSOIL.
- ALL FILLS SHALL BE COMPACTED AS REQUIRED TO REDUCE EROSION, SLIPPAGE, SETTLEMENT, SUBSIDENCE OR OTHER RELATED PROBLEMS. FILL INTENDED TO SUPPORT BUILDINGS, STRUCTURES, AND CONDUITS, ETC. SHALL BE COMPACTED IN ACCORDANCE WITH LOCAL REQUIREMENTS OR CODES.
- ALL EARTHEN FILLS SHALL BE PLACED IN COMPACTED LAYERS NOT TO EXCEED 9 INCHES IN THICKNESS, UNLESS DIRECTED OTHERWISE BY ENGINEER.
- FILL MATERIALS SHALL BE FREE OF FROZEN PARTICLES, BRUSH, ROOTS, SOO, OR OTHER FOREIGN OR OBJECTIONABLE MATERIALS THAT WOULD INTERFERE WITH OR PREVENT CONSTRUCTION OF SATISFACTORY FILLS.
- FROZEN MATERIALS OR SOFT, MUCKY, OR HIGHLY COMPRESSIBLE MATERIALS SHALL NOT BE INCORPORATED INTO FILLS.
- FILL SHALL NOT BE PLACED ON SATURATED OR FROZEN SURFACES.
- SEEPS OR SPRINGS ENCOUNTERED DURING CONSTRUCTION SHALL BE HANDLED IN ACCORDANCE WITH THE STANDARD AND SPECIFICATION FOR SUBSURFACE DRAIN OR OTHER APPROVED METHOD.
- ALL GRADED AREAS SHALL BE PERMANENTLY STABILIZED IMMEDIATELY UPON REACHING FINISHED GRADE. CUT SLOPES IN COMPETENT BEDROCK AND ROCK FILLS NEED NOT BE VEGETATED. SEEDED AREAS WITHIN 50 FEET OF A SURFACE WATER, OR AS OTHERWISE SHOWN ON THE PLAN DRAWINGS, SHALL BE BLANKETED ACCORDING TO THE STANDARDS OF THIS PLAN.
- IMMEDIATELY AFTER EARTH DISTURBANCE ACTIVITIES CEASE IN ANY AREA OR SUBAREA OF THE PROJECT, THE OPERATOR SHALL STABILIZE ALL DISTURBED AREAS. DURING NON-GERMINATING MONTHS, MULCH OR OTHER PROTECTIVE BLANKETING SHALL BE APPLIED AS DESCRIBED IN THE PLAN. AREAS NOT AT FINISHED GRADE, WHICH WILL BE REACTIVATED WITHIN 1 YEAR, MAY BE STABILIZED IN ACCORDANCE WITH THE TEMPORARY STABILIZATION SPECIFICATIONS. THOSE AREAS WHICH WILL NOT BE REACTIVATED WITHIN 1 YEAR SHALL BE STABILIZED IN ACCORDANCE WITH THE PERMANENT STABILIZATION SPECIFICATIONS.
- PERMANENT STABILIZATION IS DEFINED AS A MINIMUM UNIFORM, PERENNIAL, 70% VEGETATIVE COVER OR OTHER PERMANENT NON-VEGETATIVE COVER WITH A DENSITY SUFFICIENT TO RESIST ACCELERATED EROSION. CUT AND FILL SLOPES SHALL BE CAPABLE OF RESISTING FAILURE DUE TO SLUMPING, SLIDING, OR OTHER MOVEMENTS.
- EROSION AND SEDIMENT BMPs MUST BE CONSTRUCTED, STABILIZED, AND FUNCTIONAL BEFORE SITE DISTURBANCE BEGINS WITHIN THE TRIBUTARY AREAS OF THOSE BMPs. E&S BMPs SHALL REMAIN FUNCTIONAL AS SUCH UNTIL ALL AREAS TRIBUTARY TO THEM ARE PERMANENTLY STABILIZED OR UNTIL THEY ARE REPLACED BY ANOTHER BMP APPROVED BY THE LOCAL CONSERVATION DISTRICT OR THE DEPARTMENT.
- UPON COMPLETION OF ALL EARTH DISTURBANCE ACTIVITIES AND PERMANENT STABILIZATION OF ALL DISTURBED AREAS, THE OWNER AND/OR OPERATOR SHALL CONTACT THE LOCAL CONSERVATION DISTRICT FOR AN INSPECTION PRIOR TO REMOVAL/CONVERSION OF THE E&S BMPs.

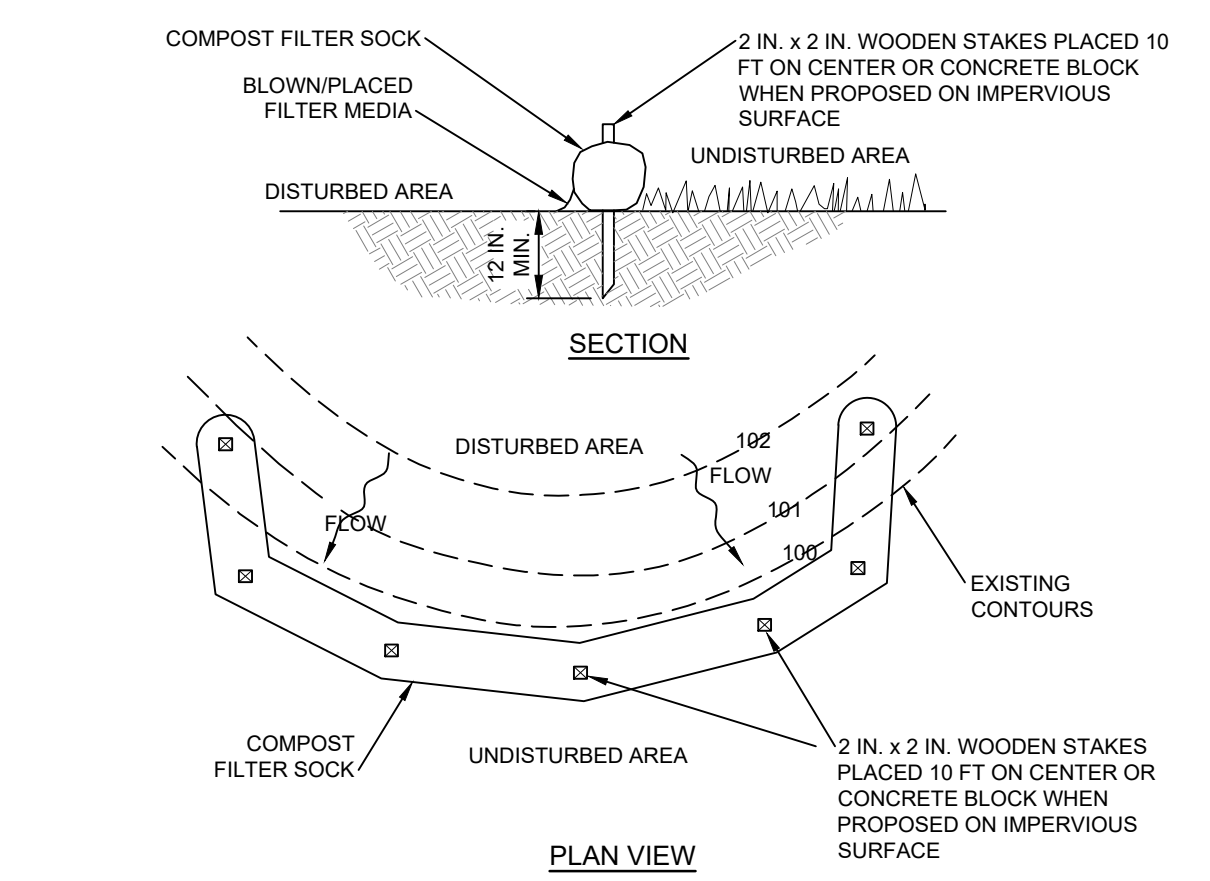
- AFTER FINAL SITE STABILIZATION HAS BEEN ACHIEVED, TEMPORARY EROSION AND SEDIMENT BMPs MUST BE REMOVED OR CONVERTED TO PERMANENT POST CONSTRUCTION STORMWATER MANAGEMENT BMPs. AREAS DISTURBED DURING REMOVAL OR CONVERSION OF THE BMPs SHALL BE STABILIZED IMMEDIATELY. IN ORDER TO ENSURE RAPID REVEGETATION OF DISTURBED AREAS, SUCH REMOVAL/CONVERSIONS ARE TO BE DONE ONLY DURING THE GERMINATING SEASON.
- UPON COMPLETION OF ALL EARTH DISTURBANCE ACTIVITIES AND PERMANENT STABILIZATION OF ALL DISTURBED AREAS, THE OWNER AND/OR OPERATOR SHALL CONTACT THE LOCAL CONSERVATION DISTRICT TO SCHEDULE A FINAL INSPECTION.
- FAILURE TO CORRECTLY INSTALL E&S BMPs, FAILURE TO PREVENT SEDIMENT-LADEN RUNOFF FROM LEAVING THE CONSTRUCTION SITE OR FAILURE TO TAKE IMMEDIATE CORRECTIVE ACTION TO RESOLVE FAILURE OF E&S BMPs MAY RESULT IN ADMINISTRATIVE, CIVIL, AND/OR CRIMINAL PENALTIES BEING INSTITUTED BY THE DEPARTMENT AS DEFINED IN SECTION 802 OF THE CLEAN STREAMS LAW. THE CLEAN STREAMS LAW PROVIDES FOR UP TO \$10,000 PER DAY IN CIVIL PENALTIES, UP TO \$10,000 IN SUMMARY CRIMINAL PENALTIES, AND UP TO \$25,000 IN MISDEMEANOR CRIMINAL PENALTIES FOR EACH VIOLATION.

ADDITIONAL EROSION AND SEDIMENTATION CONTROL NOTES:

- CONCRETE WASH WATER SHALL BE HANDLED IN THE MANNER DESCRIBED ON THE PLAN DRAWINGS. IN NO CASE SHALL IT BE ALLOWED TO ENTER ANY SURFACE WATERS OR GROUNDWATER SYSTEMS.
- EROSION CONTROL BLANKETING SHALL BE INSTALLED ON ALL SLOPES 3H:1V OR STEEPER WITHIN 50 FEET OF A SURFACE WATER AND ON ALL OTHER DISTURBED AREAS SPECIFIED ON THE PLAN MAPS AND/OR DETAIL SHEETS.
- THE OPERATOR SHALL ASSURE THAT THE APPROVED EROSION AND SEDIMENT CONTROL PLAN IS PROPERLY AND COMPLETELY IMPLEMENTED.
- THE CONTRACTOR IS ADVISED TO BECOME THOROUGHLY FAMILIAR WITH THE PROVISIONS OF THE APPENDIX 6A EROSION CONTROL RULES AND REGULATIONS, TITLE 25, PART 1, DEPARTMENT OF ENVIRONMENTAL PROTECTION, SUBPART C, PROTECTION OF NATURAL RESOURCES, ARTICLE III, WATER RESOURCES, CHAPTER 102, EROSION CONTROL.
- STRAW MULCH SHALL BE APPLIED IN LONG STRANDS, NOT CHOPPED OR FINELY BROKEN.

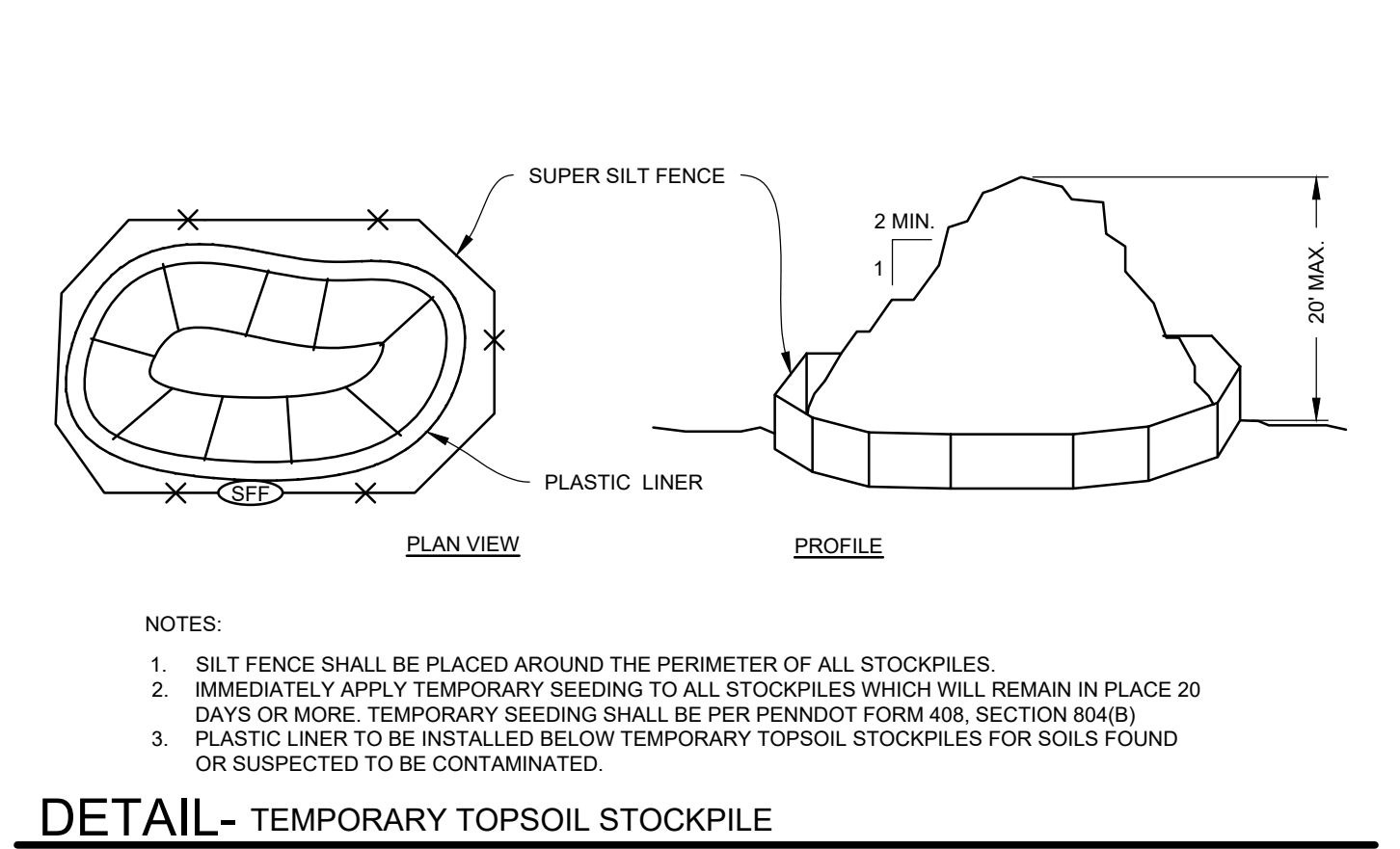
EROSION AND SEDIMENTATION MAINTENANCE:

- DURING THE LIFE OF THE PROJECT, ALL EROSION AND SEDIMENTATION CONTROL DEVICES MUST BE PROPERLY MAINTAINED. MAINTENANCE SHALL INCLUDE THE INSPECTION OF EROSION CONTROL FACILITIES AFTER EACH MEASURABLE RAINFALL EVENT (0-25 INCH) AND ON A WEEKLY BASIS, UNLESS MORE FREQUENT INSPECTION IS REQUIRED. IMMEDIATELY PERFORM CLEANOUT, REPAIR AND REPLACEMENT OF THE FACILITIES AS NEEDED. (REGRADING, RESEED AND MULCH WASHED OFF AREAS AS NEEDED.)
- UNTIL THE SITE IS STABILIZED, ALL EROSION AND SEDIMENTATION BMPs MUST BE MAINTAINED PROPERLY. MAINTENANCE MUST INCLUDE INSPECTIONS OF ALL EROSION AND SEDIMENTATION BMPs AFTER EACH RAINFALL EVENT AND ON A WEEKLY BASIS. ALL SITE INSPECTIONS WILL BE DOCUMENTED IN AN INSPECTION LOG KEPT FOR THIS PURPOSE. THE COMPLIANCE ACTIONS AND THE DATE, TIME AND NAME OF THE PERSON CONDUCTING THE INSPECTION. THE INSPECTION LOG WILL BE KEPT ON SITE AT ALL TIMES AND MADE AVAILABLE TO PWD/PEP UPON REQUEST.
- WHERE BMPs ARE FOUND TO FAIL TO ALLEVIATE EROSION OR SEDIMENT POLLUTION THE PERMITTEE OR CO-PERMITTEE NOTIFY THE PHILADELPHIA WATER DEPARTMENT OF THE FAILURE AND SHALL INCLUDE THE FOLLOWING INFORMATION:
 - THE LOCATION AND SEVERITY OF THE BMPs FAILURE AND ANY POLLUTION EVENTS.
 - ALL STEPS TAKEN TO REDUCE, ELIMINATE AND PREVENT THE REOCCURENCE OF THE NON-COMPLIANCE.
 - THE TIME FRAME TO CORRECT THE NONCOMPLIANCE, INCLUDING THE EXACT DATES WHEN THE ACTIVITY WILL RETURN TO COMPLIANCE.
- ALL PREVENTATIVE AND REMEDIAL MAINTENANCE WORK, INCLUDING CLEAN OUT, REPAIR, REPLACEMENT, REGRADING, RESEEDING, REMULCHING AND RENETTING MUST BE PERFORMED IMMEDIATELY IF EROSION AND SEDIMENTATION BMPs FAIL TO PERFORM AS EXPECTED, REPLACEMENT BMPs OR MODIFICATIONS OF THOSE INSTALLED WILL BE NEEDED.
- SEEDED AREAS THAT HAVE WASHED AWAY SHALL BE FILLED AND GRADED, AS NECESSARY, AND THEN RESEEDED. A STRAW COVER SHALL BE APPLIED TO RETAIN THE SEED ALONG WITH AN ANCHORING METHOD DESCRIBED ON THE ATTACHED MULCH ANCHORING GUIDE. UNTIL IT HAS A CHANCE TO ROOT PROPERLY.
- IMMEDIATELY AFTER EARTH DISTURBANCE ACTIVITIES CEASE, THE OPERATOR SHALL STABILIZE ANY AREAS DISTURBED BY THE ACTIVITIES. DURING NON-GERMINATING PERIODS, MULCH MUST BE APPLIED AT THE SPECIFIED RATES. DISTURBED AREAS WHICH ARE NOT AT FINISHED GRADE AND WHICH WILL BE REACTIVATED WITHIN 1 YEAR MUST BE STABILIZED IN ACCORDANCE WITH THE PERMANENT VEGETATION STABILIZATION SPECIFICATIONS.
- SEDIMENT REMOVED FROM BMPs SHALL BE DISPOSED OF IN LANDSCAPED AREAS OUTSIDE OF STEEP SLOPES, WETLANDS, FLOODPLAINS OR DRAINAGE SWALES AND IMMEDIATELY STABILIZED, OR PLACED IN TOPSOIL STOCKPILES.
- INLET FILTER BAGS SHALL BE CLEANED OUT OR REPLACED WHEN BAG IS HALF FULL.
- SEDIMENT SHALL BE REMOVED FROM COMPOST FILTER SOCKS WHEN REACHING ONE HALF THE HEIGHT OF THE SOCK, IF USED.
- THE CONTRACTOR SHALL INSPECT ALL ROCK CONSTRUCTION ENTRANCES ON A DAILY BASIS AND SHALL ENSURE THAT SEDIMENT IS NOT BEING TRACKED OUT PUBLIC STREETS. SEDIMENT THAT IS TRACKED ONTO PUBLIC STREETS SHALL BE COLLECTED AND RETURNED TO THE SITE OR OTHERWISE PROPERLY REMOVED BY A STREET SWEEPER.
- EROSION CONTROL BLANKETING SHALL BE INSTALLED ON ALL SLOPES 3H:1V OR STEEPER WITHIN 50 FEET OF SURFACE WATER AND ON ALL OTHER DISTURBED AREAS SPECIFIED ON THE PLAN MAPS AND/OR DETAIL SHEETS, IF USED.
- FILL MATERIAL FOR EMBANKMENTS SHALL BE FREE OF ROOTS, OR OTHER WOODY VEGETATION, ORGANIC MATERIAL, LARGE STONES, AND OTHER OBJECTIONABLE MATERIALS. THE EMBANKMENT SHALL BE COMPACTED TO A MAXIMUM 100% LAYERED LIFTS AT 90% DENSITY.
- AFTER FINAL SITE STABILIZATION HAS BEEN ACHIEVED, TEMPORARY EROSION AND SEDIMENT BMPs MUST BE REMOVED. ARE



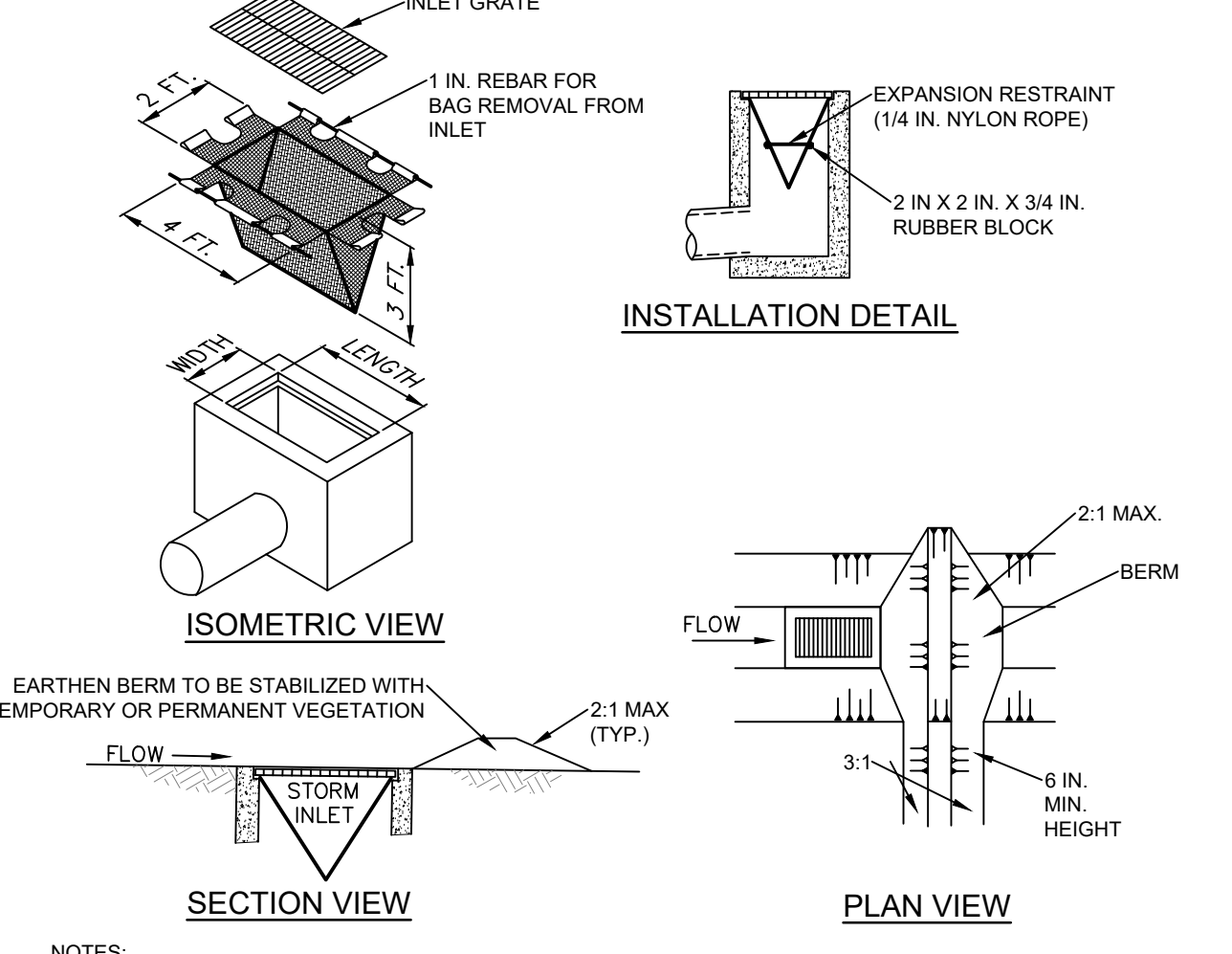
DETAIL - COMPOST FILTER SOCK
N.T.S.

NOTES:
SOCK FABRIC SHALL MEET STANDARDS OF TABLE 4.1 OF THE PA DEP EROSION CONTROL MANUAL. COMPOST SHALL MEET THE STANDARDS OF TABLE 4.2 OF THE PA DEP EROSION CONTROL MANUAL.
COMPOST FILTER SOCK SHALL BE PLACED AT EXISTING LEVEL GRADE. BOTH ENDS OF THE BARRIER SHALL BE EXTENDED AT LEAST 8 FEET UP SLOPE AT 45 DEGREES TO THE MAIN BARRIER ALIGNMENT. MAXIMUM SLOPE LENGTH ABOVE ANY BARRIER SHALL NOT EXCEED THAT SPECIFIED FOR THE SIZE OF THE SOCK AND THE SLOPE OF ITS TRIBUTARY AREA.
TRAFFIC SHALL NOT BE PERMITTED TO CROSS COMPOST FILTER SOCKS.
ACCUMULATED SEDIMENT SHALL BE REMOVED WHEN IT REACHES 1/2 THE ABOVE GROUND HEIGHT OF THE BARRIER AND DISPOSED IN THE MANNER DESCRIBED ELSEWHERE IN THE PLAN.
COMPOST FILTER SOCKS SHALL BE INSPECTED WEEKLY AND AFTER EACH RUNOFF EVENT. DAMAGED SOCKS SHALL BE REPAIRED ACCORDING TO MANUFACTURER'S SPECIFICATIONS OR REPLACED WITHIN 24 HOURS OF INSPECTION.
BIODEGRADABLE COMPOST FILTER SOCKS SHALL BE REPLACED AFTER 6 MONTHS. PHOTODEGRADABLE SOCKS AFTER 1 YEAR. POLYPROPYLENE SOCKS SHALL BE REPLACED ACCORDING TO MANUFACTURER'S RECOMMENDATIONS.
UPON STABILIZATION OF THE AREA TRIBUTARY TO THE SOCK, STAKES SHALL BE REMOVED. THE SOCK MAY BE LEFT IN PLACE AND VEGETATED OR REMOVED. IN THE LATTER CASE, THE MESH SHALL BE CUT OPEN AND THE MULCH SPREAD AS A SOIL SUPPLEMENT.



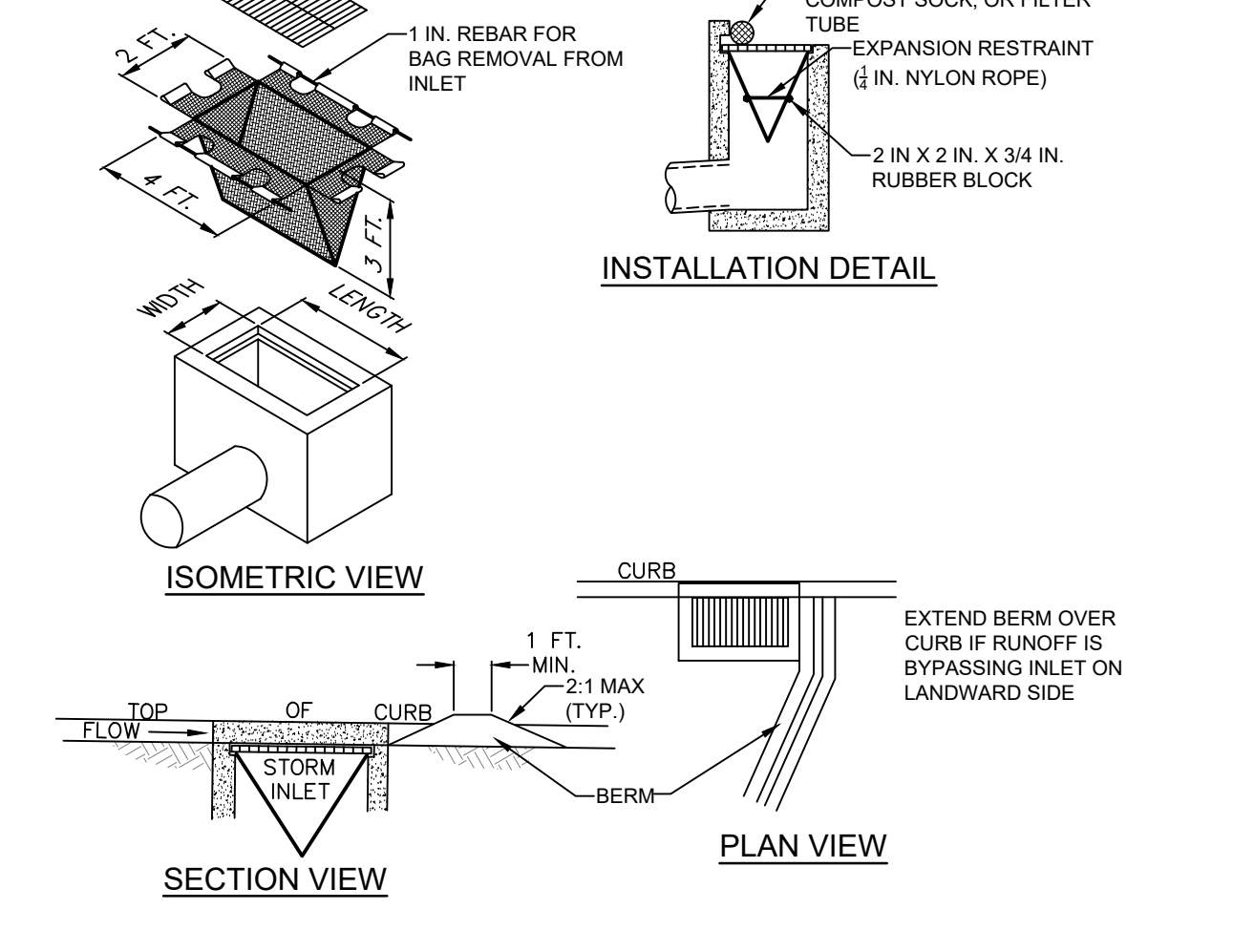
DETAIL - TEMPORARY TOPSOIL STOCKPILE
N.T.S.

NOTES:
1. SILT FENCE SHALL BE PLACED AROUND THE PERIMETER OF ALL STOCKPILES.
2. IMMEDIATELY APPLY TEMPORARY SEEDING TO ALL STOCKPILES WHICH WILL REMAIN IN PLACE 30 DAYS OR MORE. TEMPORARY SEEDING SHALL BE PER PENNDOT FORM 409, SECTION 804(B).
3. PLASTIC LINER TO BE INSTALLED BELOW TEMPORARY TOPSOIL STOCKPILES FOR SOILS FOUND OR SUSPECTED TO BE CONTAMINATED.



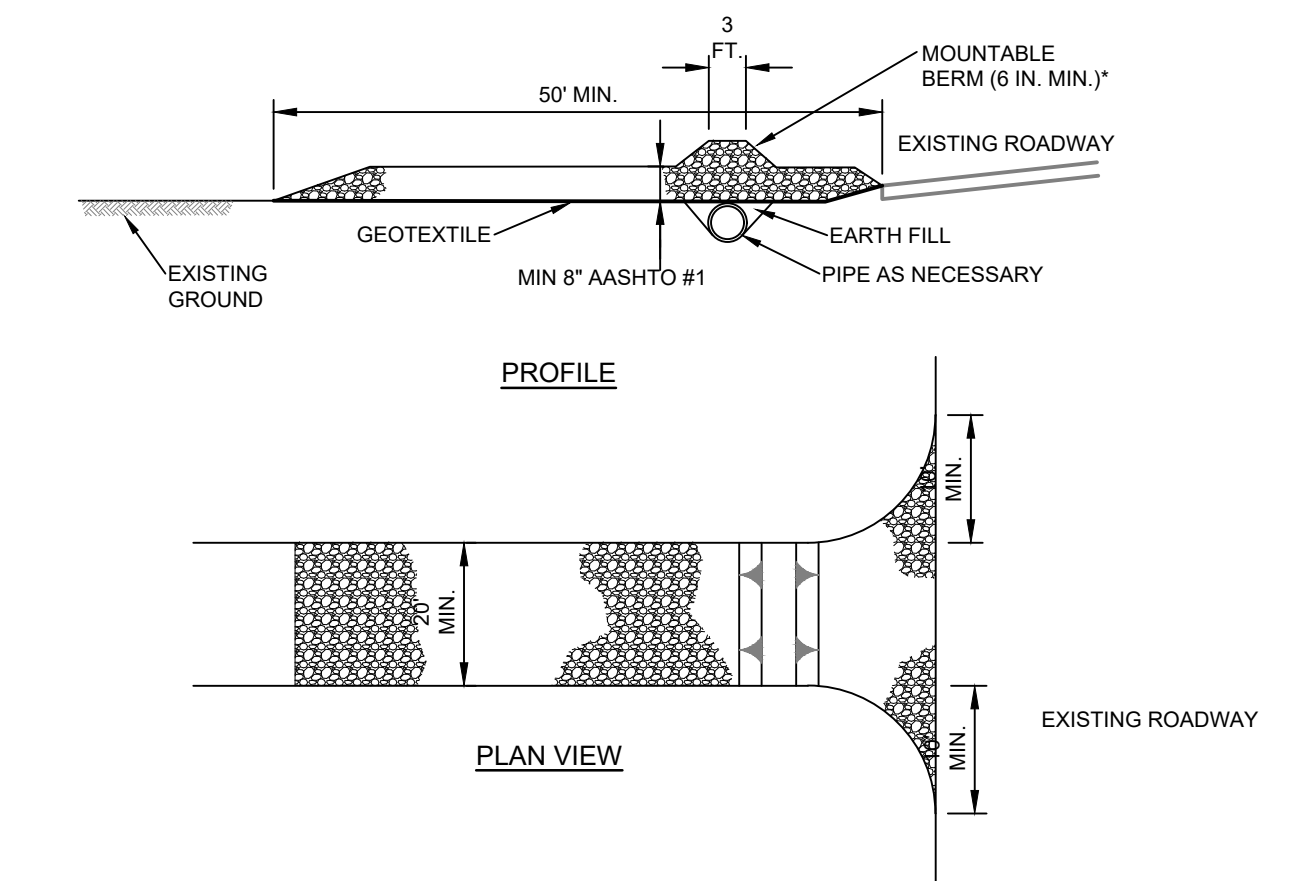
DETAIL - FILTER BAG INLET PROTECTION - TYPE M INLET
N.T.S.

NOTES:
MAXIMUM DRAINAGE AREA = 1/2 ACRE.
INLET PROTECTION SHALL NOT BE REQUIRED FOR INLET TRIBUTARY TO SEDIMENT BASIN OR TRAP. BERMS SHALL BE REQUIRED FOR ALL INSTALLATIONS.
ROLLED EARTHEN BERM IN ROADWAY SHALL BE MAINTAINED UNTIL ROADWAY IS STONED. ROAD SUBBASE BERM OR ROADWAY SHALL BE MAINTAINED UNTIL ROADWAY IS PAVED. EARTHEN BERM IN CHANNEL SHALL BE MAINTAINED UNTIL PERMANENT STABILIZATION IS COMPLETED OR REMAIN PERMANENTLY.
AT A MINIMUM, THE FABRIC SHALL HAVE A MINIMUM GRAB TENSILE STRENGTH OF 120 LBS. A MINIMUM BURST STRENGTH OF 200 PSI, AND A MINIMUM TRAPEZOIDAL TEAR STRENGTH OF 50 LBS. FILTER BAGS SHALL BE CAPABLE OF TRAPPING ALL PARTICLES NOT PASSING A NO. 40 SIEVE.
INLET FILTER BAGS SHALL BE INSPECTED ON A WEEKLY BASIS AND AFTER EACH RUNOFF EVENT. BAGS SHALL BE EMPTIED AND RINSED OR REPLACED WHEN HALF FULL OR WHEN FLOW CAPACITY HAS BEEN REDUCED SO AS TO CAUSE FLOODING OR BYPASSING OF THE INLET. DAMAGED OR CLOGGED BAGS SHALL BE REPLACED. A SUPPLY SHALL BE MAINTAINED ON SITE FOR REPLACEMENT OF BAGS. ALL NEEDED REPAIRS SHALL BE INITIATED IMMEDIATELY AFTER THE INSPECTION. DISPOSE ACCUMULATED SEDIMENT AS WELL AS ALL USED BAGS ACCORDING TO THE PLAN NOTES.
DO NOT USE ON MAJOR PAVED ROADWAYS WHERE PONDING MAY CAUSE TRAFFIC HAZARDS.



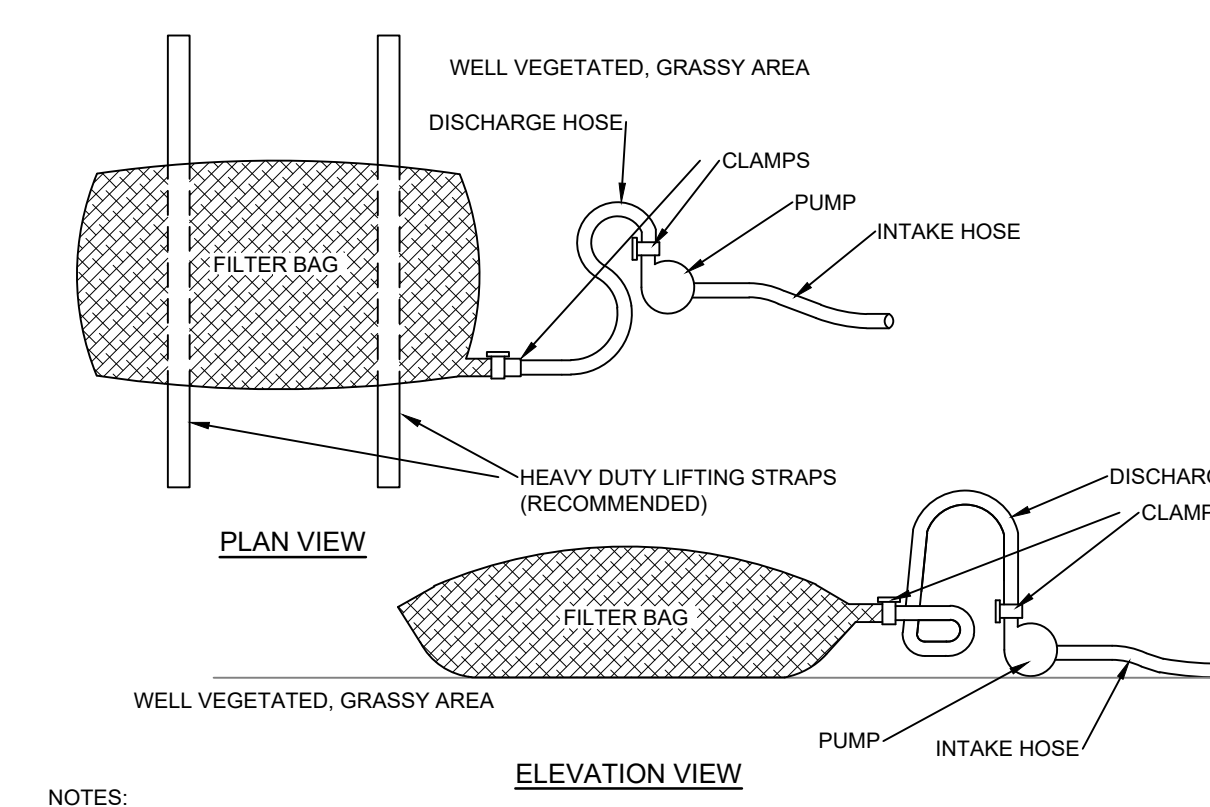
DETAIL - FILTER BAG INLET PROTECTION - TYPE C INLET
N.T.S.

NOTES:
MAXIMUM DRAINAGE AREA = 1/2 ACRE.
INLET PROTECTION SHALL NOT BE REQUIRED FOR INLET TRIBUTARY TO SEDIMENT BASIN OR TRAP. BERMS SHALL BE REQUIRED FOR ALL INSTALLATIONS.
ROLLED EARTHEN BERM SHALL BE MAINTAINED UNTIL ROADWAY IS STONED. ROAD SUBBASE BERM SHALL BE MAINTAINED UNTIL ROADWAY IS PAVED. EARTHEN BERM IN CHANNEL SHALL BE MAINTAINED UNTIL ROADWAY SURFACE RECEIVES FINAL COAT.
AT A MINIMUM, THE FABRIC SHALL HAVE A MINIMUM GRAB TENSILE STRENGTH OF 120 LBS. A MINIMUM BURST STRENGTH OF 200 PSI, AND A MINIMUM TRAPEZOIDAL TEAR STRENGTH OF 50 LBS. FILTER BAGS SHALL BE CAPABLE OF TRAPPING ALL PARTICLES NOT PASSING A NO. 40 SIEVE.
INLET FILTER BAGS SHALL BE INSPECTED ON A WEEKLY BASIS AND AFTER EACH RUNOFF EVENT. BAGS SHALL BE EMPTIED AND RINSED OR REPLACED WHEN HALF FULL OR WHEN FLOW CAPACITY HAS BEEN REDUCED SO AS TO CAUSE FLOODING OR BYPASSING OF THE INLET. DAMAGED OR CLOGGED BAGS SHALL BE REPLACED. A SUPPLY SHALL BE MAINTAINED ON SITE FOR REPLACEMENT OF BAGS. ALL NEEDED REPAIRS SHALL BE INITIATED IMMEDIATELY AFTER THE INSPECTION. DISPOSE OF ACCUMULATED SEDIMENT AS WELL AS ALL USED BAGS ACCORDING TO THE PLAN NOTES.
DO NOT USE ON MAJOR PAVED ROADWAYS WHERE PONDING MAY CAUSE TRAFFIC HAZARDS.



DETAIL - ROCK CONSTRUCTION ENTRANCE
N.T.S.

NOTES:
REMOVE TOPSOIL PRIOR TO INSTALLATION OF ROCK CONSTRUCTION ENTRANCE. EXTEND ROCK OVER FULL WIDTH OF ENTRANCE.
RUNOFF SHALL BE DIVERTED FROM ROADWAY TO A SUITABLE SEDIMENT REMOVAL AND PROPER TO ENTERING ROCK CONSTRUCTION ENTRANCE.
MOUNTABLE BERM SHALL BE INSTALLED WHEREVER OPTIONAL DELIVERT PIPE IS USED AND PRIOR PIPE COVER AS SPECIFIED BY MANUFACTURER IS NOT OTHERWISE PROVIDED. PIPE SHALL BE SIZED APPROPRIATELY FOR SIZE OF DITCH BEING CROSSED.
GEOTEXTILE FOR ROCK CONSTRUCTION ENTRANCE TO MEET THE REQUIREMENTS OF PENNDOT PUBLICATION 408, SECTION 735, CLASS 4, TYPE A GEOTEXTILE.
MAINTENANCE: ROCK CONSTRUCTION ENTRANCE THICKNESS SHALL BE CONSTANTLY MAINTAINED TO THE SPECIFIED DIMENSIONS BY ADDING ROCK. A STOCKPILE SHALL BE MAINTAINED ON SITE FOR THIS PURPOSE. ALL SEDIMENT DEPOSITED ON PAVED ROADWAYS SHALL BE REMOVED AND RETURNED TO THE CONSTRUCTION SITE IMMEDIATELY. IF EXCESSIVE AMOUNTS OF SEDIMENT ARE BEING DEPOSITED ON ROADWAY, EXTEND LENGTH OF ROCK CONSTRUCTION ENTRANCE BY 50 FOOT INCREMENTS UNTIL CONDITION IS ALLEVIATED OR INSTALL WASH RACK, WASHING THE ROADWAY OR SWEEPING THE DEPOSITS INTO ROADWAY DITCHES, SEWERS, CULVERTS, OR OTHER DRAINAGE COURSES IS NOT ACCEPTABLE.

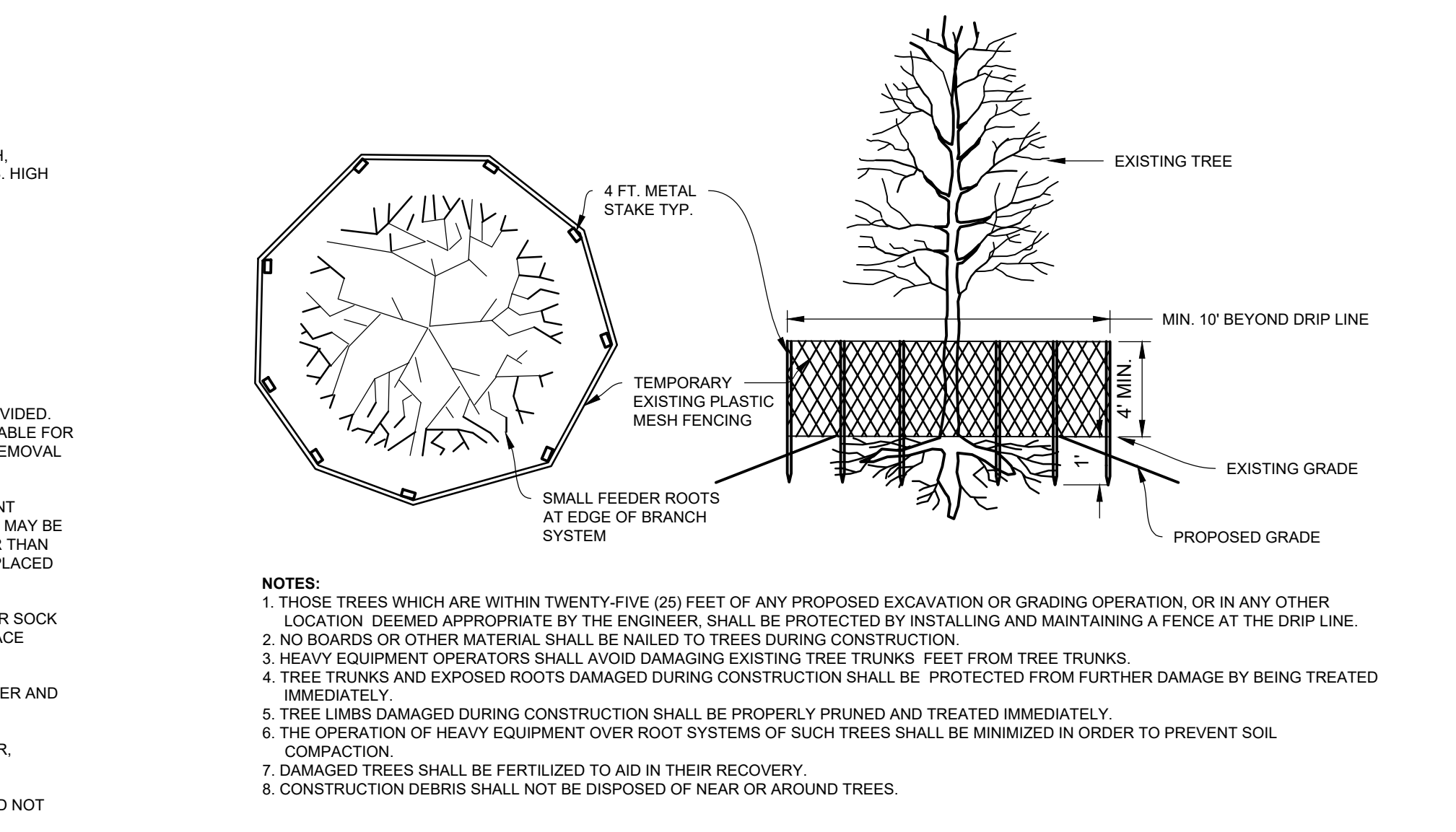


DETAIL - PUMPED WATER FILTER BAG
N.T.S.

NOTES:
LOW VOLUME FILTER BAGS SHALL BE MADE FROM NON-WOVEN GEOTEXTILE MATERIAL SEWN WITH HIGH STRENGTH, DOUBLE STITCHED 2" TYPE SEAMS. THEY SHALL BE CAPABLE OF TRAPPING PARTICLES LARGER THAN 150 MICRONS. HIGH VOLUME FILTER BAGS SHALL BE MADE FROM WOVEN GEOTEXTILES THAT MEET THE FOLLOWING STANDARDS:

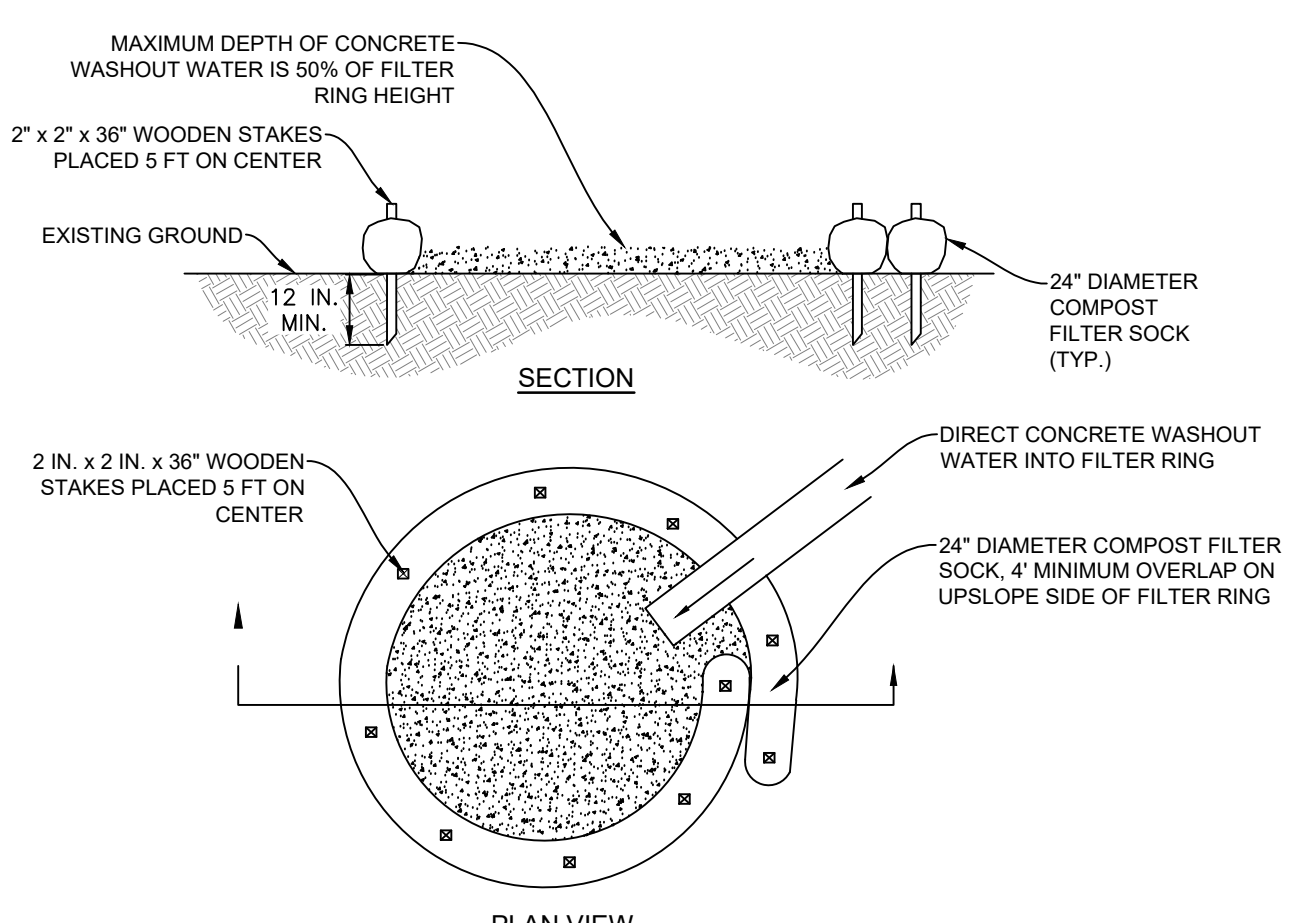
PROPERTY	TEST METHOD	MINIMUM STANDARD
AVG. WIDTH	ASTM D-4884	60 LBS
GRAB TENSILE	ASTM D-4632	205 LB
PUNCTURE	ASTM D-4833	110 LB
MULLER BURST	ASTM D-3786	350 PSI
UV RESISTANCE	ASTM D-4355	70%
AOS % RETAINED	ASTM D-4751	80 SIEVE

A SUITABLE MEANS OF ACCESSING THE BAG WITH MACHINERY REQUIRED FOR DISPOSAL PURPOSES SHALL BE PROVIDED. FILTER BAGS SHALL BE REPLACED WHEN THEY BECOME 1/2 FULL OF SEDIMENT. SPARE BAGS SHALL BE KEPT AVAILABLE FOR REPLACEMENT OF THOSE THAT HAVE FAILED OR ARE FILLED. BAGS SHALL BE PLACED ON STRAPS TO FACILITATE REMOVAL, UNLESS BAGS COME WITH LIFTING STRAPS ALREADY ATTACHED.
BAGS SHALL BE LOCATED IN WELL-VEGETATED (GRASSY) AREA, AND DISCHARGE ONTO STABLE, EROSION RESISTANT AREAS. WHERE THIS IS NOT POSSIBLE, A GEOTEXTILE UNDERLAYMENT AND FLOW PATH SHALL BE PROVIDED. BAGS MAY BE PLACED ON FILTER STONE TO INCREASE DISCHARGE CAPACITY. BAGS SHALL NOT BE PLACED ON SLOPES GREATER THAN 5% FOR SLOPES EXCEEDING 5%. CLEAN ROCK OR OTHER NON-ERODIBLE AND NON-POLLUTING MATERIAL MAY BE PLACED UNDER THE BAG TO REDUCE SLOPE STEEPNESS.
NO DOWNSLOPE SEDIMENT BARRIER IS REQUIRED FOR MOST INSTALLATIONS. COMPOST BERM OR COMPOST FILTER SOCK SHALL BE INSTALLED BELOW BAGS LOCATED IN HO OR EV WATERSHEDS, WITHIN 5 FEET OF ANY RECEIVING SURFACE. WATER OR WHERE GRASSY AREA IS NOT AVAILABLE.
THE PUMP DISCHARGE HOSE SHALL BE INSERTED INTO THE BAGS IN THE MANNER SPECIFIED BY THE MANUFACTURER AND SECURELY CLAIMED. A PIECE OF PVC PIPE IS RECOMMENDED FOR THIS PURPOSE.
THE PUMPING RATE SHALL BE NO GREATER THAN 700 GPM OR 1/2 THE MAXIMUM SPECIFIED BY THE MANUFACTURER, WHICHEVER IS LESS. PUMP INTAKES SHALL BE FLOATING AND SCREENED.
FILTER BAGS SHALL BE INSPECTED DAILY. IF ANY PROBLEM IS DETECTED, PUMPING SHALL CEASE IMMEDIATELY AND NOT RESUME UNTIL THE PROBLEM IS CORRECTED.



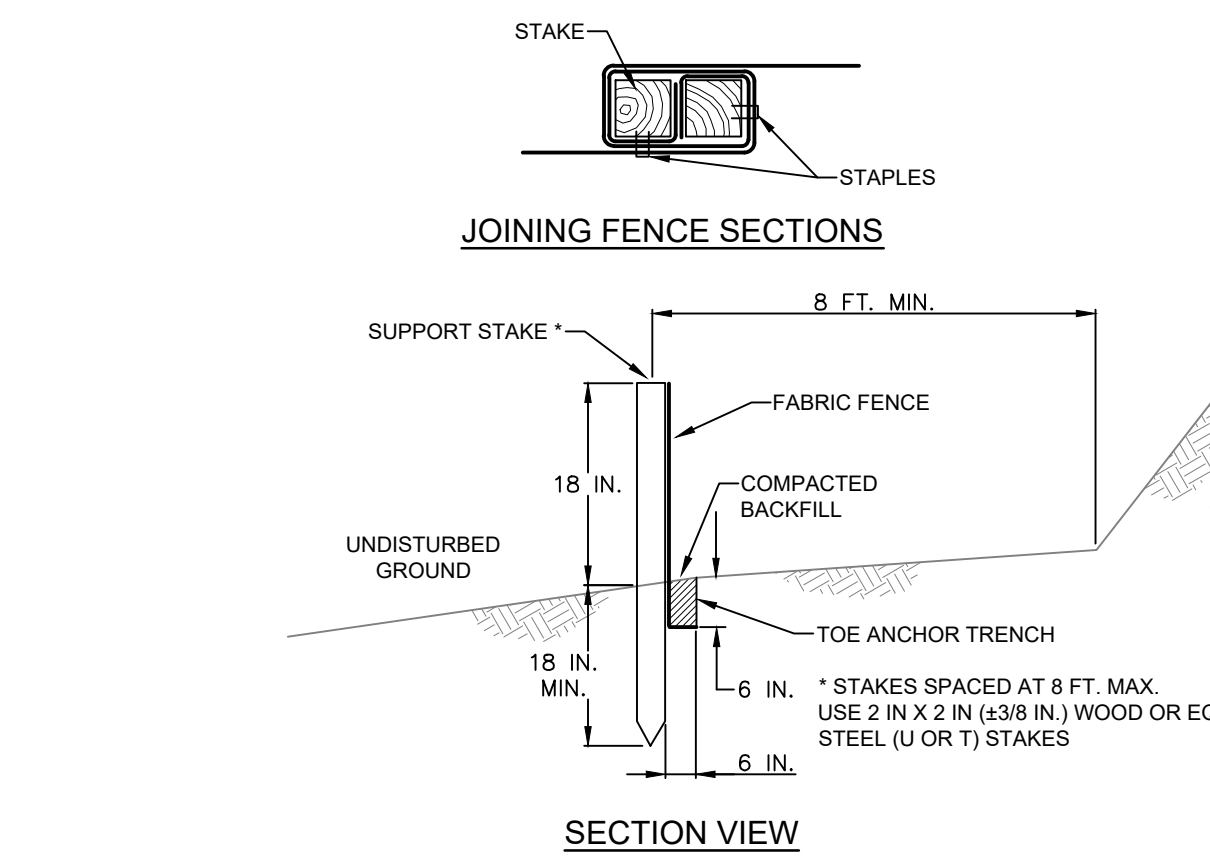
DETAIL - TREE PROTECTION FENCE
N.T.S.

NOTES:
1. THOSE TREES WHICH ARE WITHIN TWENTY-FIVE (25) FEET OF ANY PROPOSED EXCAVATION OR GRADING OPERATION, OR IN ANY OTHER LOCATION DEEMED APPROPRIATE BY THE ENGINEER, SHALL BE PROTECTED BY INSTALLING AND MAINTAINING A FENCE AT THE DRIP LINE.
2. NO BOARDS OR OTHER MATERIAL SHALL BE NAILED TO TREES DURING CONSTRUCTION.
3. HEAVY EQUIPMENT OPERATORS SHALL AVOID DAMAGING EXISTING TREE TRUNKS FEET FROM TREE TRUNKS.
4. TREE TRUNKS AND EXPOSED ROOTS DAMAGED DURING CONSTRUCTION SHALL BE PROTECTED FROM FURTHER DAMAGE BY BEING TREATED IMMEDIATELY.
5. TREE LIMBS DAMAGED DURING CONSTRUCTION SHALL BE PROPERLY PRUNED AND TREATED IMMEDIATELY.
6. THE OPERATION OF HEAVY EQUIPMENT OVER ROOT SYSTEMS OF SUCH TREES SHALL BE MINIMIZED IN ORDER TO PREVENT SOIL COMPACTION.
7. DAMAGED TREES SHALL BE FERTILIZED TO AID IN THEIR RECOVERY.
8. CONSTRUCTION DEBRIS SHALL NOT BE DISPOSED OF NEAR OR AROUND TREES.



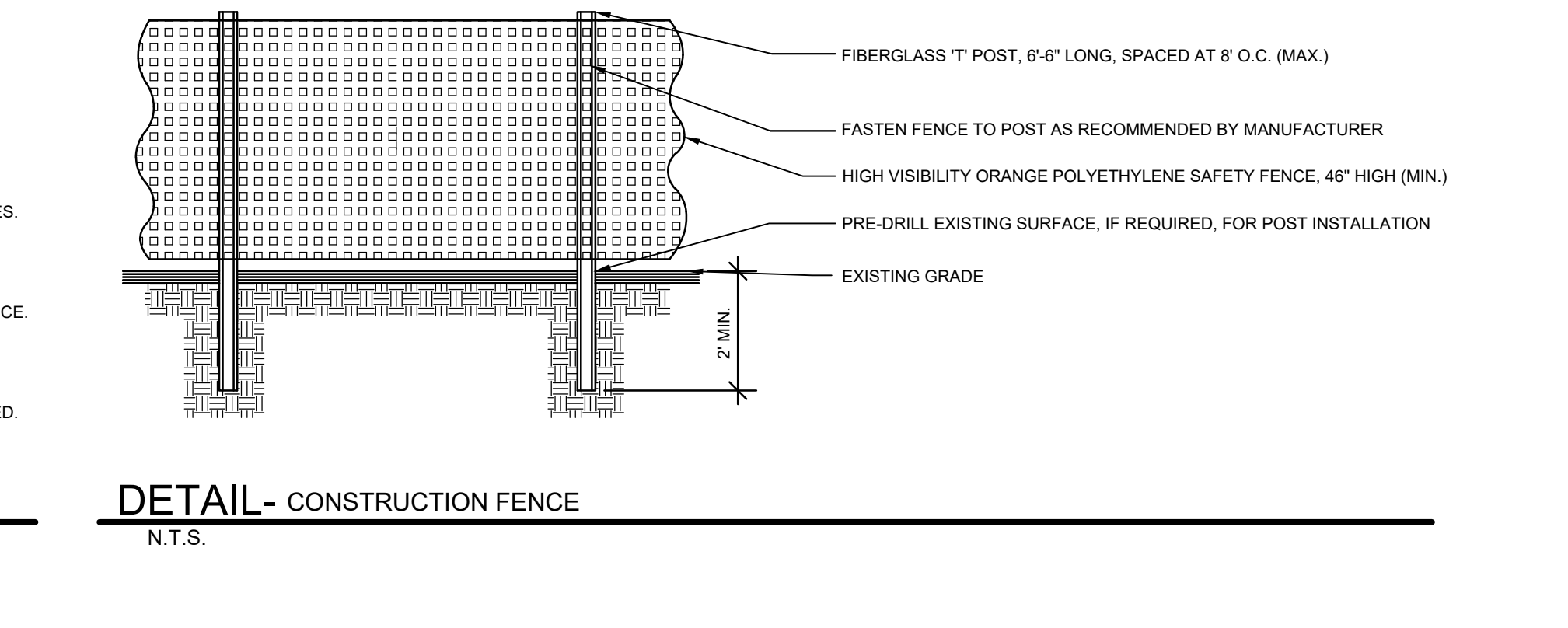
DETAIL - CONCRETE WASHOUT
N.T.S.

NOTES:
A SUITABLE IMPERVIOUS GEOMEMBRANE SHALL BE PLACED AT THE LOCATION OF THE WASHOUT PRIOR TO INSTALLING THE SOCKS.
INSTALL ON FLAT GRADE FOR OPTIMUM PERFORMANCE.
COMPOST SOCKS SHALL BE STAKED IN THE MANNER RECOMMENDED BY THE MANUFACTURER AROUND THE PERIMETER OF THE GEOMEMBRANE SO AS TO FORM A RING WITH THE ENDS OF THE SOCK LOCATED AT THE UPSLOPE CORNER.
CARE SHALL BE TAKEN TO ENSURE CONTINUOUS CONTACT OF THE SOCK WITH THE GEOMEMBRANE AT ALL LOCATIONS.
18" DIAMETER FILTER SOCK MAY BE STACKED ONTO DOUBLE 24" DIAMETER FILTER SOCKS IN PYRAMIDAL CONFIGURATION FOR ADDED HEIGHT.
MAINTENANCE: ALL CONCRETE WASHOUT FACILITIES SHOULD BE INSPECTED DAILY. DAMAGED OR LEAKING WASHOUTS SHOULD BE DEACTIVATED AND REPAIRED OR REPLACED IMMEDIATELY. ACCUMULATED MATERIALS SHALL BE REMOVED WHEN THEY REACH 50% CAPACITY. GEOMEMBRANE SHALL BE REPLACED WITH EACH CLEANING OF THE WASHOUT FACILITY.



DETAIL - SILT FENCE
N.T.S.

NOTES:
FABRIC SHALL HAVE THE MINIMUM PROPERTIES AS SHOWN IN TABLE 4.3 OF THE PA DEP EROSION CONTROL MANUAL.
FABRIC WIDTH SHALL BE 30 IN. MINIMUM. STAKES SHALL BE HARDWOOD OR EQUIVALENT STEEL (U OR T) STAKES.
SILT FENCE SHALL BE PLACED AT LEVEL EXISTING GRADE. BOTH ENDS OF THE FENCE SHALL BE EXTENDED AT LEAST 8 FEET UP SLOPE AT 45 DEGREES TO THE MAIN FENCE ALIGNMENT.
SEDIMENT SHALL BE REMOVED WHEN ACCUMULATIONS REACH HALF THE ABOVE GROUND HEIGHT OF THE FENCE.
ANY SECTION OF SILT FENCE WHICH HAS BEEN UNDERMINED OR TOPPED SHALL BE IMMEDIATELY REPLACED WITH A ROCK FILTER OUTLET (STANDARD CONSTRUCTION DETAIL # 4-6).
FENCE SHALL BE REMOVED AND PROPERLY DISPOSED OF WHEN TRIBUTARY AREA IS PERMANENTLY STABILIZED.



DETAIL - CONSTRUCTION FENCE
N.T.S.

REVISIONS

ISSUE	DATE	DESCRIPTION
1	01/09/23	PWD PCSM/NPDES SUBMISSION
2	02/14/23	PWD PCSM RESUBMISSION
3	05/24/23	PWD PCSM RESUBMISSION



REVIEWED BY:
PROJECT COORDINATOR:
REALS:



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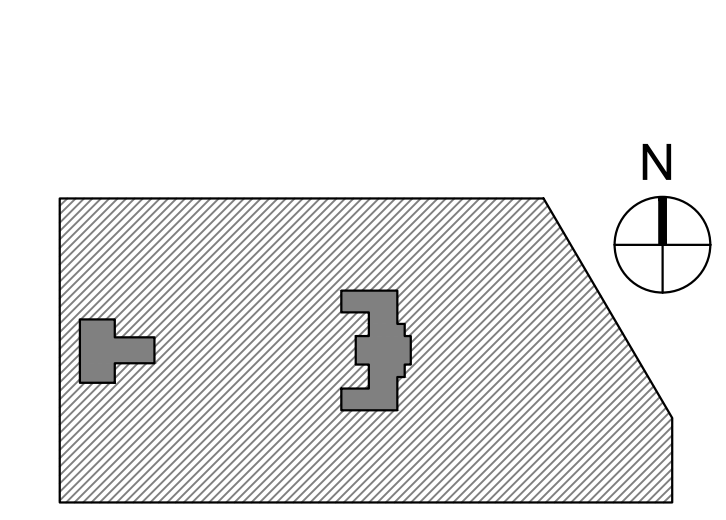
LANDSCAPE ARCHITECT:
Salt Design Studio
4100 Main Street Suite 200
Philadelphia PA 19127
www.saltdesignstudio.com

M.E.P./P./SITE CIVIL ENGINEERS:
Pennoni Associates
1900 Market Street Suite 300
Philadelphia PA 19103
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LEED CONSULTANT:
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1635 Market Street Suite 1600
Philadelphia PA 19103

CITY OF PHILADELPHIA
DEPARTMENT OF PUBLIC PROPERTY
1515 ARCH STREET
11TH FLOOR, ONE PARKWAY BUILDING
PHILADELPHIA, PENNSYLVANIA

PROJECT TITLE
KINGSESSING RECREATION CENTER

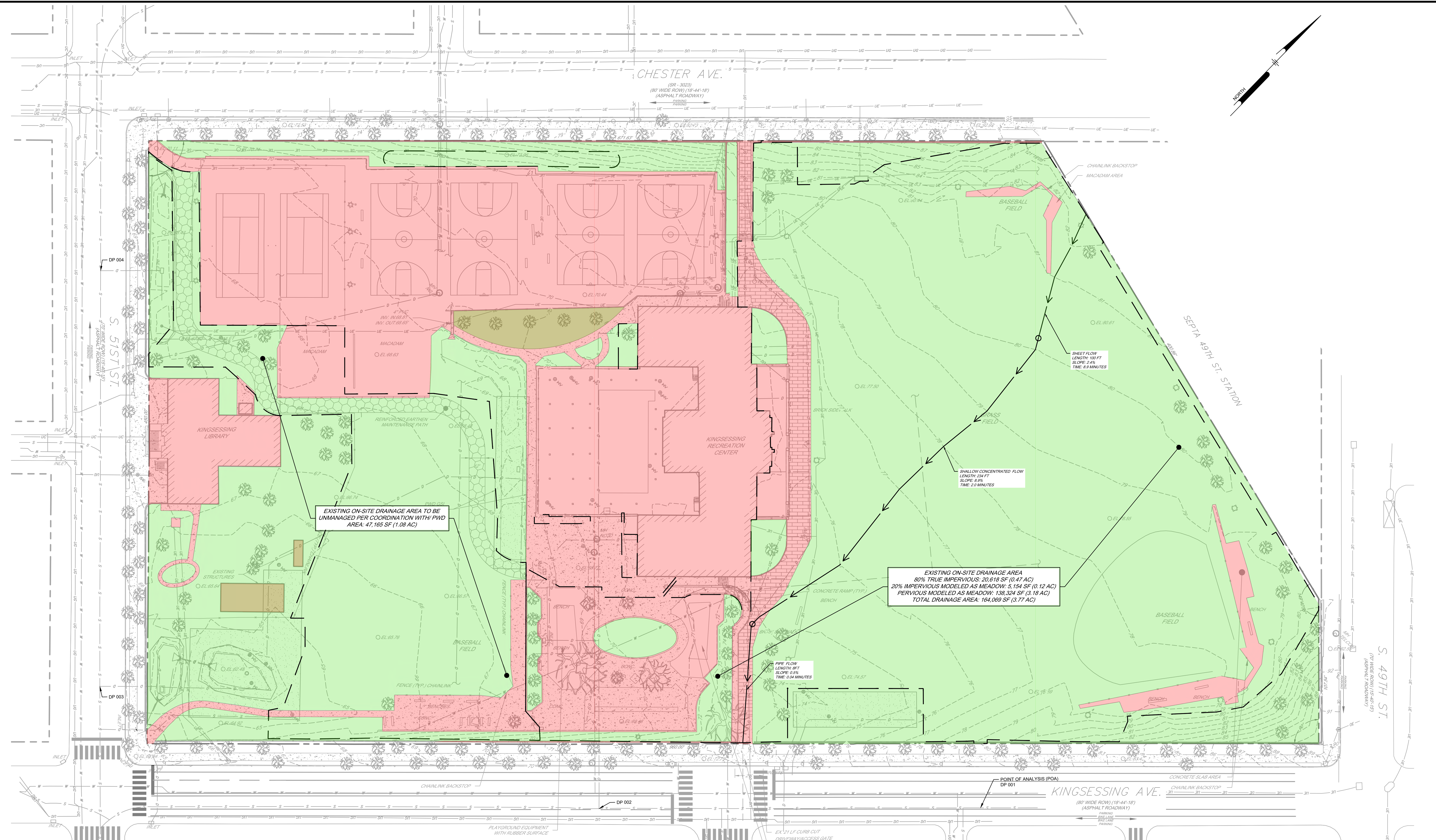


EROSION & SEDIMENTATION CONTROL DETAILS

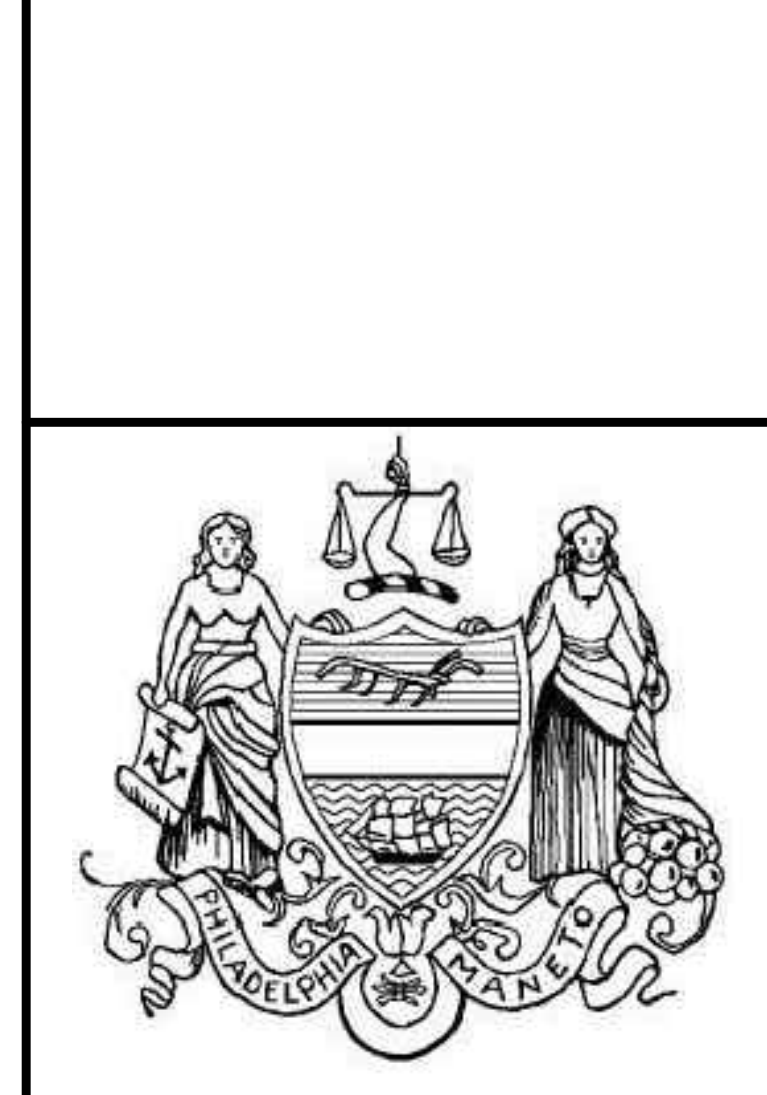
PROJECT NO. KLMLX21003	DRAWING NO. C-852-R.2
DATE 08/28/23	SCALE
DRAWN BY SDL	CHECKED BY MJM

NOTE: ALL DIMENSIONS AND CONDITIONS SHALL BE VERIFIED BY THE CONTRACTOR AT THE SITE BEFORE PROCEEDING WITH THE WORK.

PACKAGE 2 - IFB
NOT FOR CONSTRUCTION
08/28/23
NPDES PERMIT #PAC510302
PWD TRACKING #FY22-KING-6800-01



REVISIONS		
ISSUE	DATE	DESCRIPTION
1	01/09/23	PWD PCSM/NPDES SUBMISSION
2	02/14/23	PWD PCSM RESUBMISSION
3	05/24/23	PWD PCSM RESUBMISSION



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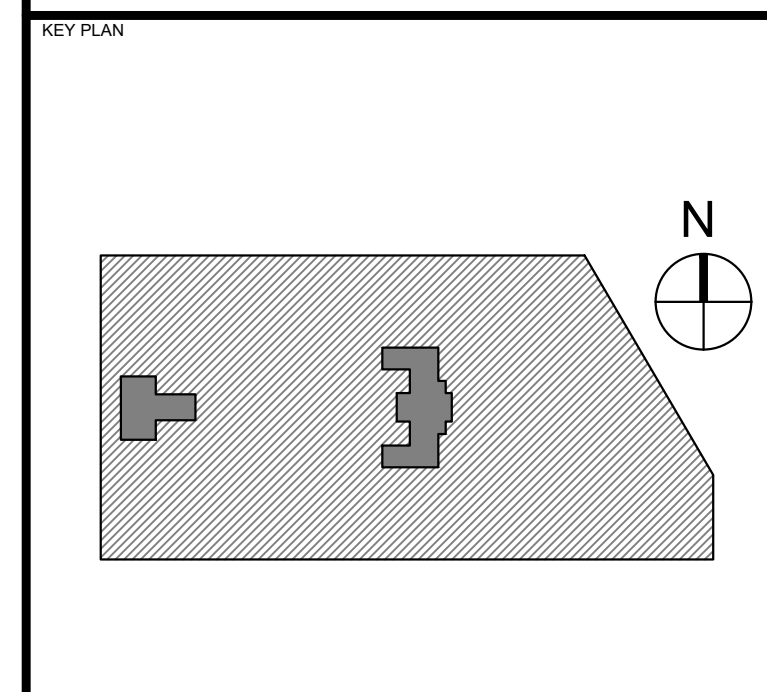
LANDSCAPE ARCHITECT:
Salt Design Studio
4100 Main Street Suite 200
Philadelphia PA 19127
www.saltdesignstudio.com

M.E.P./F.P./SITE CIVIL ENGINEERS:
Pennoni Associates
1900 Market Street Suite 300
Philadelphia PA 19103
www.pennoni.com

LEED CONSULTANT:
Verde Architecture Consulting
1635 Market Street Suite 1600
Philadelphia PA 19103

CITY OF PHILADELPHIA
DEPARTMENT OF PUBLIC PROPERTY
1515 ARCH STREET
11TH FLOOR, ONE PARKWAY BUILDING
PHILADELPHIA PENNSYLVANIA

PROJECT TITLE
KINGESSING RECREATION CENTER



DRAWING TITLE
PRE-CONSTRUCTION DRAINAGE AREA MAP

PROJECT NO. KLMLX21003	DRAWING NO. C-900-R.2
DATE 08/28/23	SCALE 1" = 30'
DRAWN BY SDL	CHECKED BY MJM

PRE-CONSTRUCTION DRAINAGE AREA (ON-SITE)			
	AREA (SF)	AREA (AC)	CN
PRE-IMPERVIOUS (80% OF TRUE IMPERVIOUS)	20,618	0.47	98
PRE-MEADOW (20% OF TRUE IMPERVIOUS MODELED AS MEADOW)	5,154	0.12	58
GRASS (MODELED AS MEADOW)	138,324	3.18	58
TOTAL	164,099	3.77	

GENERAL INFORMATION:

SITE:
4901 KINGESSING AVE
PHILADELPHIA, PA 19143
OP#A783249500

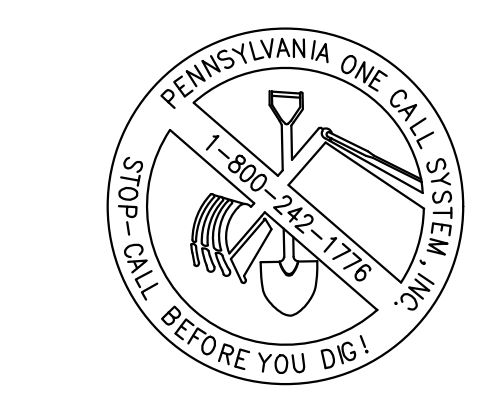
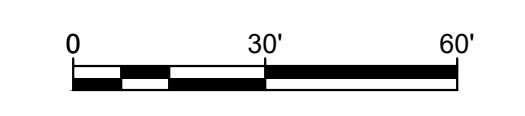
CLIENT:
CITY OF PHILADELPHIA
PARKS AND RECREATION
1515 ARCH STREET, 10TH FLOOR
PHILADELPHIA, PA 19102

ENGINEER:
PENNONI ASSOCIATES
1900 MARKET STREET, SUITE 300
PHILADELPHIA, PA 19103

REFERENCES:

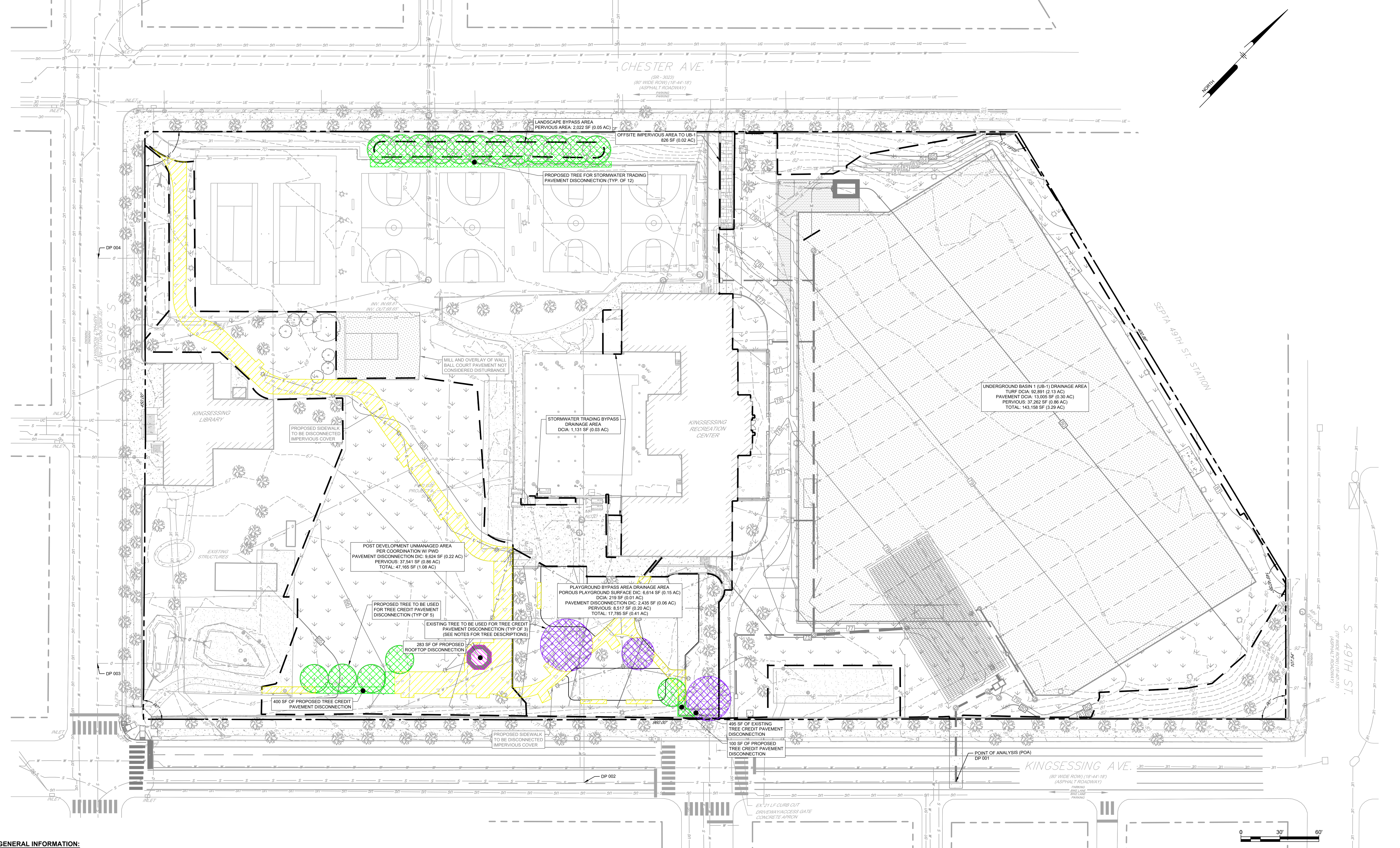
1. BOUNDARY AND TOPOGRAPHIC INFORMATION TAKEN FROM A PLAN BY AMERICAN ENGINEERS GROUP, LLC ENTITLED "KINGESSING RECREATION CENTER TOPOGRAPHIC SURVEY", SHEET NO. 1, DATED 03/12/2021.

- EXISTING LEGEND**
- EXISTING PROPERTY LINE (APPROXIMATE)
 - EXISTING RIGHT OF WAY LINE (APPROXIMATE)
 - EXISTING BUILDING
 - EXISTING CURB
 - EXISTING SIDEWALK
 - EXISTING EDGE OF MACADAM/GRAVEL
 - EXISTING TRAFFIC MARKING
 - EXISTING FENCE
 - EXISTING TREE
 - EXISTING MAJOR CONTOUR
 - EXISTING MINOR CONTOUR
 - EXISTING SIGN
 - EXISTING BOLLARD
 - EXISTING UTILITY POLE
 - EXISTING LIGHT
 - EXISTING INLET
 - EXISTING SEWER
 - EXISTING STORM SEWER
 - EXISTING WATER LINE
 - EXISTING UNDERGROUND TELEPHONE LINE
 - EXISTING GAS LINE
 - EXISTING UNDERGROUND ELECTRIC
 - EXISTING OVERHEAD WIRES
 - EXISTING UTILITY STRUCTURES
 - EXISTING ON-SITE DRAINAGE AREA BOUNDARY
 - EXISTING IMPERVIOUS AREA
 - EXISTING PERVIOUS AREA



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CALL 1-800-242-1776
PA. ACT 287 OF 1974 REQUIRES THREE WORKING DAYS NOTICE TO UTILITIES BEFORE YOU EXCAVATE, DRILL OR BLAST PENNSYLVANIA ONE-CALL SYSTEM, INC. SERIAL NUMBER(S): 20212583952

PACKAGE 2 - IFB
NOT FOR CONSTRUCTION
08/28/23
NPDES PERMIT #PAC510302
PWD TRACKING #FY22-KING-6800-01



REVISIONS		
ISSUE	DATE	DESCRIPTION
1	01/09/23	PWD PCSM/NPDES SUBMISSION
2	02/14/23	PWD PCSM RESUBMISSION
3	05/24/23	PWD PCSM RESUBMISSION



REVIEWED BY:
PROJECT COORDINATOR:
REALS:



KELLY MAIELLO ARCHITECTS
1420 Walnut Street, 15th Floor
Philadelphia, PA 19102
www.kmarchitects.com

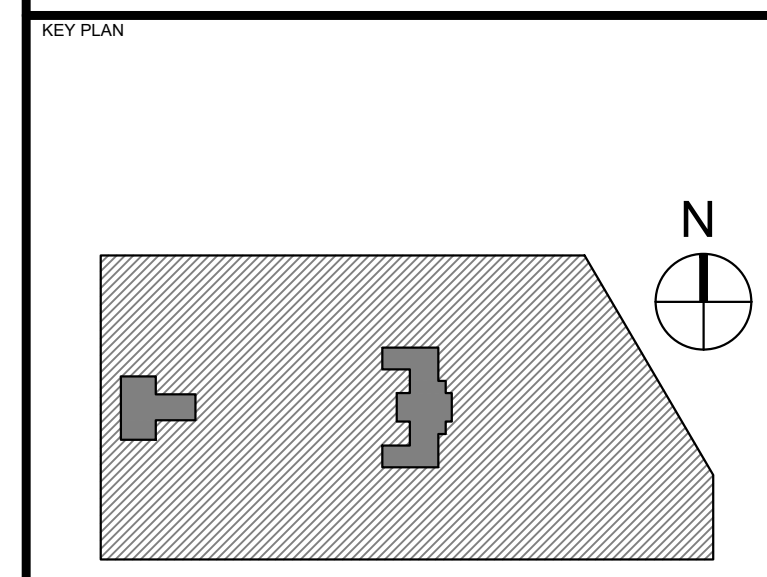
LANDSCAPE ARCHITECT:
Salt Design Studio
4100 Main Street Suite 200
Philadelphia PA 19127
www.saltdesignstudio.com

M.E.P./F.P./SITE CIVIL ENGINEERS:
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1635 Market Street Suite 1600
Philadelphia PA 19103

CITY OF PHILADELPHIA
DEPARTMENT OF PUBLIC PROPERTY
1515 ARCH STREET
11TH FLOOR, ONE PARKWAY BUILDING
PHILADELPHIA, PENNSYLVANIA

PROJECT TITLE
KINGSESSING RECREATION CENTER



GENERAL INFORMATION:

SITE:
4901 KINGSESSING AVE
PHILADELPHIA, PA 19143
OPA#783249500

CLIENT:
CITY OF PHILADELPHIA
PARKS AND RECREATION
1515 ARCH STREET, 10TH FLOOR
PHILADELPHIA, PA 19102

ENGINEER:
PENNONI ASSOCIATES
1900 MARKET STREET, SUITE 300
PHILADELPHIA, PA 19103

REFERENCES:
1. BOUNDARY AND TOPOGRAPHIC INFORMATION TAKEN FROM A PLAN BY AMERICAN ENGINEERS GROUP, LLC ENTITLED "KINGSESSING RECREATION CENTER TOPOGRAPHIC SURVEY", SHEET NO. 1, DATED 03/12/2021.

	AREA (SF)	AREA (AC)	CN
PERVIOUS	37,262	0.86	61
SYNTHETIC TURF DCIA	92,891	2.13	98
DCIA	13,005	0.30	98
TOTAL	143,158	3.29	

	AREA (SF)	AREA (AC)	CN
DCIA	1,131	0.03	98
TOTAL	1,131	0.03	

	AREA (SF)	AREA (AC)	CN
PERVIOUS	8,517	0.20	61
POROUS PLAYGROUND SURFACE (DIC)	6,614	0.15	70
DCIA	219	0.01	98
PAVEMENT DIC	2,435	0.06	98
TOTAL	17,785	0.41	

	AREA (SF)	AREA (AC)	CN
PERVIOUS	2,022	0.05	61
TOTAL	2,022	0.05	

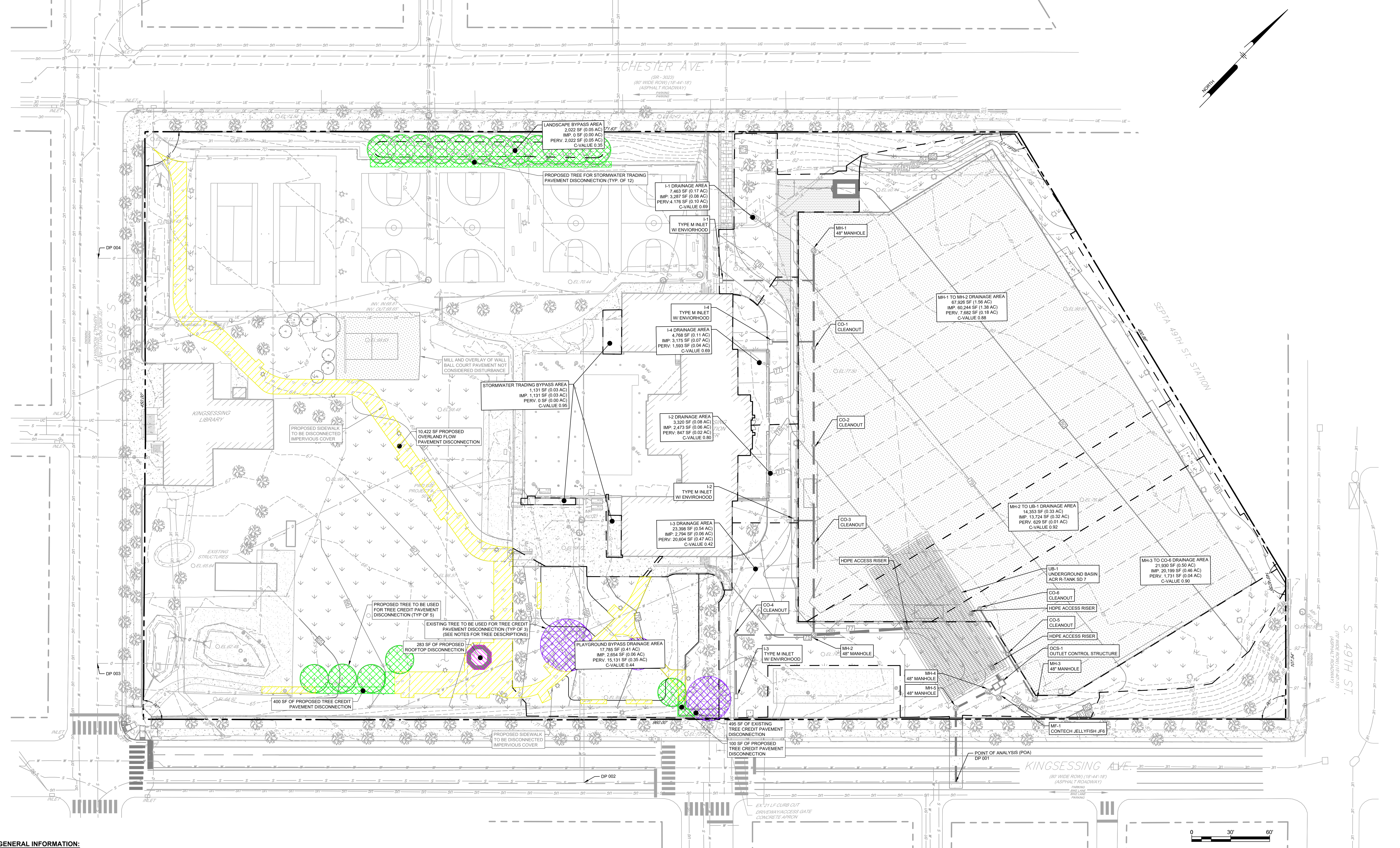
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- EXISTING PROPERTY LINE (APPROXIMATE)
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 - EXISTING GAS LINE
 - EXISTING UNDERGROUND ELECTRIC
 - EXISTING OVERHEAD WIRES
 - EXISTING UTILITY STRUCTURES

- PROPOSED LEGEND**
- PROPOSED BUILDING
 - PROPOSED DOOR
 - PROPOSED CONCRETE PAVEMENT
 - PROPOSED ASPHALT PAVEMENT
 - PROPOSED LANDSCAPED AREA
 - PROPOSED UNIT PAVERS
 - PROPOSED POROUS PLAYGROUND SAFETY SURFACE (DIC)
 - PROPOSED SYNTHETIC TURF FIELD
 - PROPOSED ADA RAMP
 - PROPOSED CURB
 - PROPOSED DEPRESSED CURB
 - PROPOSED WALL
 - PROPOSED STOP BAR/PAVEMENT MARKING
 - PROPOSED FENCE
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 - PROPOSED NATURAL GAS LINE
 - PROPOSED CABLE/TELEPHONE LINE
 - PROPOSED ELECTRIC LINE

- PROPOSED DRAINAGE AREA BOUNDARY TO PWD INFRASTRUCTURE
- EXISTING TREE DISCONNECTION
- PROPOSED TREE DISCONNECTION
- PAVEMENT DISCONNECTION
- ROOFTOP DISCONNECTION

PACKAGE 2 - IFB
CALL BEFORE YOU DIG
NOT FOR CONSTRUCTION
08/28/23
PA ACT 287 THREE WORKING DAYS
NOTICE TO UTILITIES BEFORE YOU EXCAVATE, DRILL OR
INSTALL ANYTHING BELOW GROUND SURFACE
NPDES PERMIT #PA05410302
PWD TRACKING #FY22-KING-6800-01

DRAWING TITLE	
POST CONSTRUCTION DRAINAGE AREA MAP	
PROJECT NO. KLMXL21003	DRAWING NO. C-901-R.2
DATE 08/28/23	SCALE 1" = 30'
DRAWN BY SDL	CHECKED BY MJM
NOTE: ALL DIMENSIONS AND CONDITIONS SHALL BE VERIFIED BY THE CONTRACTOR AT THE SITE BEFORE PROCEEDING WITH THE WORK.	



REVISIONS		
ISSUE	DATE	DESCRIPTION
1	01/09/23	PWD PCSM/NPDES SUBMISSION
2	02/14/23	PWD PCSM RESUBMISSION
3	05/24/23	PWD PCSM RESUBMISSION



REVIEWED BY:
PROJECT COORDINATOR:
REALS:



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www.kmarchitects.com

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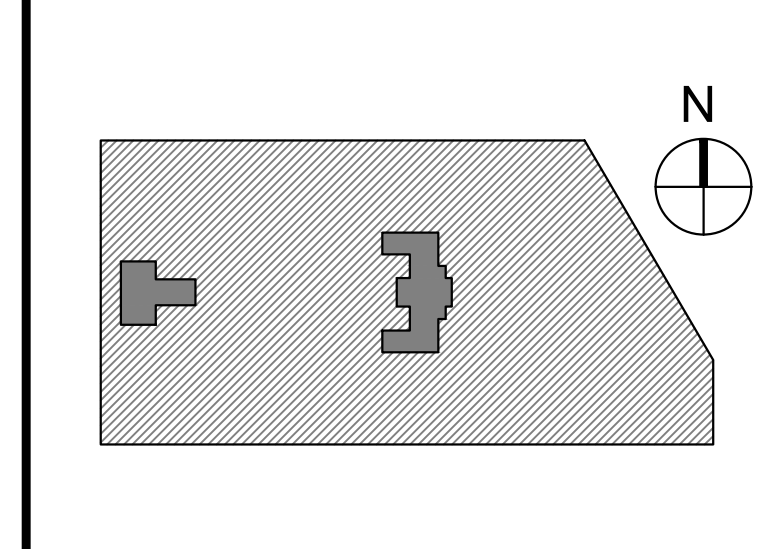
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Philadelphia PA 19103
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LEED CONSULTANT:
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CITY OF PHILADELPHIA
DEPARTMENT OF PUBLIC PROPERTY
1515 ARCH STREET
PHILADELPHIA, PENNSYLVANIA
11TH FLOOR, ONE PARKWAY BUILDING

PROJECT TITLE
KINGESSING RECREATION CENTER

KEY PLAN



DRAWING TITLE
INLET DRAINAGE AREA MAP

PROJECT NO. KMLX21003	DRAWING NO.
DATE 08/28/23	C-902-R.2
SCALE 1" = 30'	
DRAWN BY SDL	
CHECKED BY MJM	

GENERAL INFORMATION:
SITE:
4901 KINGESSING AVE
PHILADELPHIA, PA 19143
OPA#783249500

CLIENT:
CITY OF PHILADELPHIA
PARKS AND RECREATION
1515 ARCH STREET, 10TH FLOOR
PHILADELPHIA, PA 19102

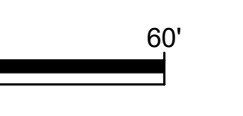
ENGINEER:
PENNONI ASSOCIATES
1900 MARKET STREET, SUITE 300
PHILADELPHIA, PA 19103

INLET DRAINAGE AREA SUMMARY				
DRAINAGE AREA	IMPERVIOUS AREA SF (AC.)	PERVIOUS AREA SF (AC.)	TOTAL AREA SF (AC.)	C VALUE
I-1	3,287 (0.08)	4,176 (0.10)	7,463 (0.17)	0.61
I-2	2,473 (0.06)	847 (0.02)	3,320 (0.08)	0.80
I-3	2,794 (0.06)	20,604 (0.47)	23,398 (0.54)	0.42
I-4	3,175 (0.07)	1,593 (0.04)	4,768 (0.11)	0.75
MH-1 TO MH-2	60,244 (1.38)	7,682 (0.18)	67,926 (1.56)	0.88
MH-2 TO UB-1	13,724 (0.32)	629 (0.01)	14,353 (0.33)	0.92
MH-3 TO CO-6	20,199 (0.46)	1,731(0.04)	21,930 (0.50)	0.90
PLAYGROUND BYPASS	2,654 (0.06)	15,131 (0.35)	17,785 (0.41)	0.44
STORMWATER TRADING BYPASS	1,131 (0.03)	0 (0.00)	1,131 (0.03)	0.95
LANDSCAPE BYPASS	0 (0.00)	2,022 (0.05)	2,022 (0.05)	0.35
TOTAL	109,681 (2.52)	54,415 (1.25)	164,096 (3.77)	

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PACKAGE 2 - IFB
NOT FOR CONSTRUCTION
08/28/23
NPDES PERMIT #PAC510302
PWD TRACKING #FY22-KING-6800-01

PENNONI ASSOCIATES, INC. 1900 MARKET STREET, SUITE 300 PHILADELPHIA, PA 19103
 PROJECT NO. KMLX21003 DRAWING NO. C-902-R.2
 DATE 08/28/23 SCALE 1" = 30'
 DRAWN BY SDL CHECKED BY MJM
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